

**PUBLIC WORKS MEMORANDUM**  
**#13-2023**

**DATE:** March 20, 2023

**TO:** Honorable Mayor Meredith Leighty and City Council Members

**THROUGH:** Heather Geyer, City Manager *hmg*  
Jason Loveland, Interim Deputy City Manager *AL2*

**FROM:** Kent Kisselman PE, Director of Public Works *KHK*

**SUBJECT:** 2022 Pavement Condition Assessment

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**PURPOSE**

To present information from the 2022 Pavement Condition Assessment, including current total street network conditions and recommendations for future roadway asset management.

**BACKGROUND**

The City is responsible for the pavement condition assessment of its roadway network every three years and to maintain an average Pavement Condition Index (PCI) rating of 70 or better throughout its entire roadway network pursuant to Resolution No. 06-92, Series 2006. The City is responsible for approximately 106 center line miles of roadway. The roadway network is classified into arterial, collector, and local roadways.

The City has adopted the PCI as a measure of pavement condition for asset management purposes. The PCI was first developed by the Army Corps of Engineers and then further standardized by the American Society for Testing and Materials (ASTM International). ASTM D6433 Standard Practice for Roads and Parking Lots Pavement Condition Index Survey describes a method for the determination of road and parking lot pavement conditions through visual surveys using the PCI method of quantifying pavement condition.

In 2016, the City contracted with IMS Infrastructure Maintenance Services, LLC to complete the survey and provide recommendations for roadway asset management. In order to keep a consistent rating, the City contracted IMS again in 2019 and 2022. IMS Infrastructure completes field inspections using laser technology. The laser technology is used to accurately identify distresses within the pavement area based on the ASTM Standard. For local roadways, the assessment is completed over the main driving surface. For arterial and collectors, the data is collected over the main travel lane. This provides an unbiased, consistent, and accurate measure of the roadway pavement condition.

IMS Infrastructure completed its field survey and data analysis, see Attachment 1 – Final Report. Highlights from the final report are discussed below:

The City average PCI is 67 with a backlog of 8.3%. The City's average PCI falls within the "Good" ranking. Backlog is defined as the percentage of streets at or below a PCI of 40 (poor and very poor).

- a. 58.2% of the City's roadways were found to be in good condition or better. The breakdown in PCI based on percentage is shown in Attachment 2 – Current Pavement Condition Using Descriptive Terms.
- b. A backlog below 15% is recommended to be maintained as those streets that fall below this threshold will require very expensive treatments to repair.

Staff uses Streetlogix, a highly customizable, web-based asset management software that enables municipalities to optimize their road budget within a GIS environment. The system provides information on the state of their infrastructure and makes maintenance and repair recommendations, including prioritizing roadway and sidewalk projects.

**BUDGET/TIME IMPLICATIONS**

The current and future PCI average of the roadway network is directly correlated to the City budget allocation within the Residential Street Program. The program is funded with Adams County Transportation Tax (ADCOT) dollars in the Capital Projects Fund. The 2023 Budget includes \$950,000 for the Residential Street Program, and \$650,000 for Arterial Rehabilitation on E. 112<sup>th</sup> Avenue.

**STAFF RECOMMENDATION**

This item is for informational purposes only.

**STAFF REFERENCE**

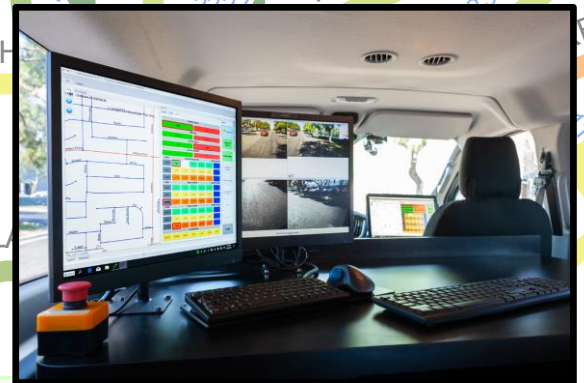
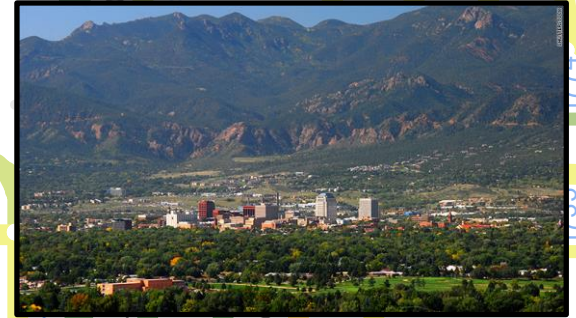
If Council members have any questions, please contact Kent Kisselman, Director of Public Works, at [kkisselman@northglenn.org](mailto:kkisselman@northglenn.org) or 303.450.4005.

**ATTACHMENTS**

1. Final Report
2. Current Pavement Condition Using Descriptive Terms
3. Presentation
4. List of Streets for Future Rehabilitation

# Northglenn, CO

## Pavement Management Report November 2022



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**APPENDED REPORTS**

**Appendix A**

**Appendix B**

**Appendix C**

**Following Page 35**

**Street Inventory and Condition Summary**

**\$1.5M/Yr. Street Rehabilitation Program by Segment**

**Full-Sized Maps**

**APPENDED MAPS**

**Functional Classification by Segment**

**Pavement Condition Index by Segment**

**Assembled Projects**

**\$1.5M/Yr Rehabilitation Plan Year 1-5 by Segment**

**\$1.5M/Yr Post Rehabilitation PCI Map Year 5 by Segment**

# 1.0 EXECUTIVE SUMMARY

## 1.1. Project Overview

In April of 2022, IMS deployed a state-of-the-art Road Surface Tester (RST) (shown in **Figure 1**) to Northglenn (City) to capture continuous, high-resolution pavement cracking, rutting, and roughness data on 117 centerline miles of asphalt and concrete roadways. Collected data were entered into the Easy Street Analysis™ (ESA) pavement management system, and representative pavement condition scores were determined for each roadway segment. The ESA pavement management system was used to develop multi-year pavement maintenance and rehabilitation (M&R) recommendations, which are included in this report.

The Pavement Condition Index (PCI) method was used in accordance with the American Society for Testing and Materials (ASTM) D6433 to assess the condition of the City’s pavements. Widely used in the industry, the PCI method is a standard, objective, and repeatable approach to assess pavement condition. Based on the PCI results, ESA prioritizes funding using a cost-of-deferral approach, recommending M&R activities that optimize funding by selecting rehabilitation candidates only when they approach the critical point where a heavier maintenance activity will soon be needed to restore the roadway to full service.

The analysis and data presented in this report are based on the inspections performed by IMS in April 2022 on the City’s pavement network, using available work history and other assumptions that are elaborated on later in this report. The information presented in the Executive Summary is condensed from various sections of this report. It is essential that reviewers familiarize themselves with the detailed information provided in subsequent sections of this report prior to making any specific decisions based on these results.



**Figure 1 - IMS Road Surface Tester (RST)**

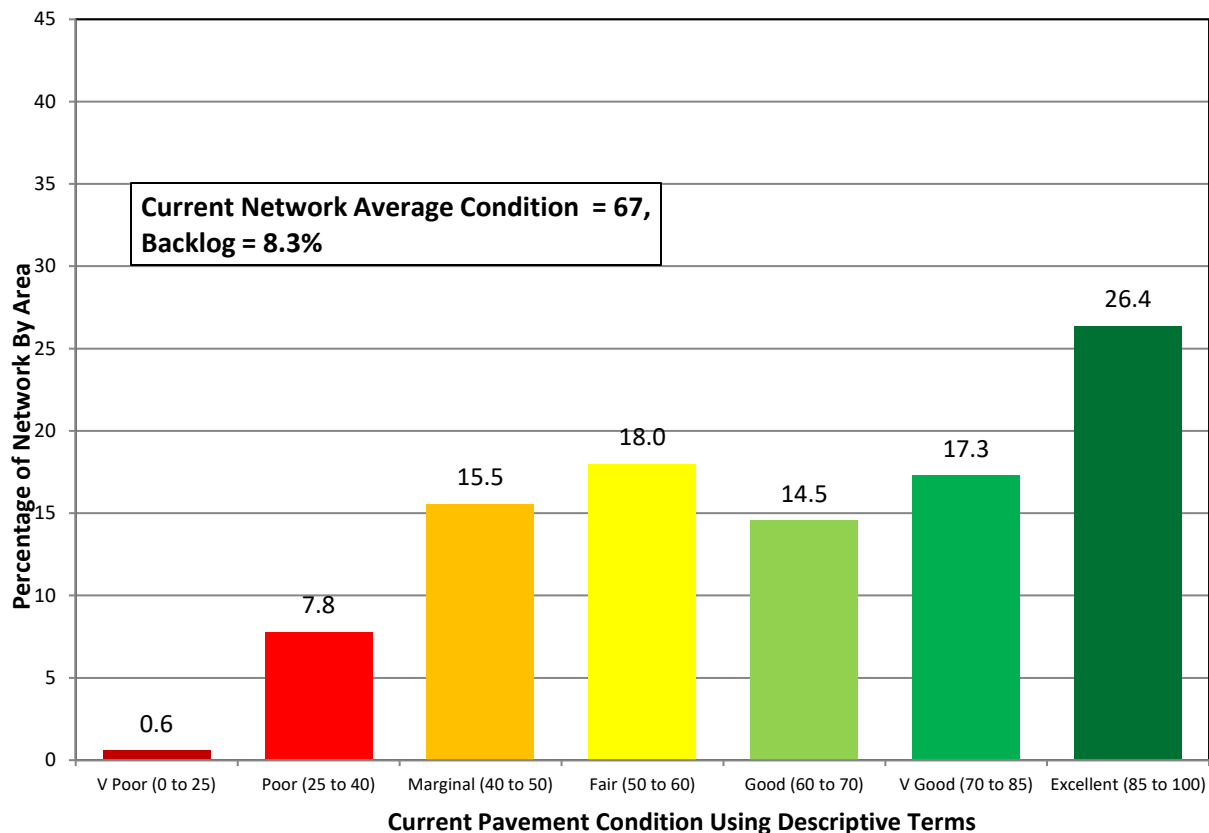
## 1.2. Results

PCI values provide an indication of the surface conditions and structural integrity of a pavement. The 0 to 100 PCI range is commonly divided into various categories using descriptive terms. Divisions between the terms are not fixed but are meant to reflect common perceptions of pavement conditions. **Table 1** shows the categories chosen to represent the City’s PCI assessment criteria, and it includes typical pavement distresses and M&R needs within each category.

**Table 1. City of Northglenn’s pavement condition categories.**

Category	Typical Distresses and M&R Recommendations	PCI Range
Excellent	Like new condition – little to no maintenance required  Monitor condition or preventive maintenance.	(85-100]
Very Good	Minor cracking, raveling, and other non-load associated distress  Routine or preventive maintenance. <i>E.g., Crack sealing, surface treatment</i>	(70-85]
Good	Minor to moderate cracking and low severity load associated distresses such as alligator cracking and rutting.  Surface treatments with localized repairs and overlays <i>E.g., Surface treatments, localized surface patching, thin overlay</i>	(60-70]
Fair	More extensive and severe longitudinal and transverse cracking, as well as moderate severity load associated distresses  Localized repairs or major rehabilitation. <i>E.g., Localized surface and/or full-depth patching, moderate overlays</i>	(50-60]
Marginal	Localized high-severity alligator cracking, and rutting.  Major rehabilitation. <i>E.g., Localized full-depth patching, mill and overlay, traditional overlay</i>	(40-50]
Poor	A greater extent of severe alligator cracking, rutting  Major rehabilitation. <i>E.g., More extensive full-depth patching, mill and overlay, traditional overlay</i>	(25-40]
Very Poor	Extensive and severe alligator cracking, more extensive and deeper rutting, and potholes.  Major rehabilitation. <i>E.g., Full-depth reclamation, reconstruction</i>	[0-25]

The City’s roadways were found to be in good condition **with an average PCI of 67**. Approximately 44% of the City’s roadways were found to be in excellent or very good condition. These pavements are candidates for more cost-effective pavement preservation treatments. Pavements with a PCI below 40 (i.e., pavements in poor or very poor condition) comprise the City’s “backlog” of M&R. The **City’s backlog was found to be approximately 8%**. A breakdown of the condition distribution of the City’s pavement network is shown in **Figure 2**. Detailed information on pavement conditions can be found in section 3.



**Figure 2 - Pavement Condition Overview**

### 1.3. Findings and Recommendations

For the City to get the most return on investment from the ESA Pavement Management System, IMS presents the City with the following findings and recommendations:

- The City’s network-level PCI is 67, and the backlog represents 8% of the network.
- Maintain a low backlog, thereby freeing up funds for inexpensive pavement preservation activities, such as slurry and chip seals.
- Focus on the restoration of roads in the “Marginal” PCI range through the use of strategic overlays to achieve a low backlog.
- Concentrate rehabs on critical PCI candidates. ESA recommendations produce optimal results when maintenance work is focused on roads that are at their “Critical PCI” point.
- Current \$1.5M/Yr. budget will achieve a PCI of 64 and a backlog of 17% after the 5-year budget horizon. (Detailed funding projections are presented in section 4)



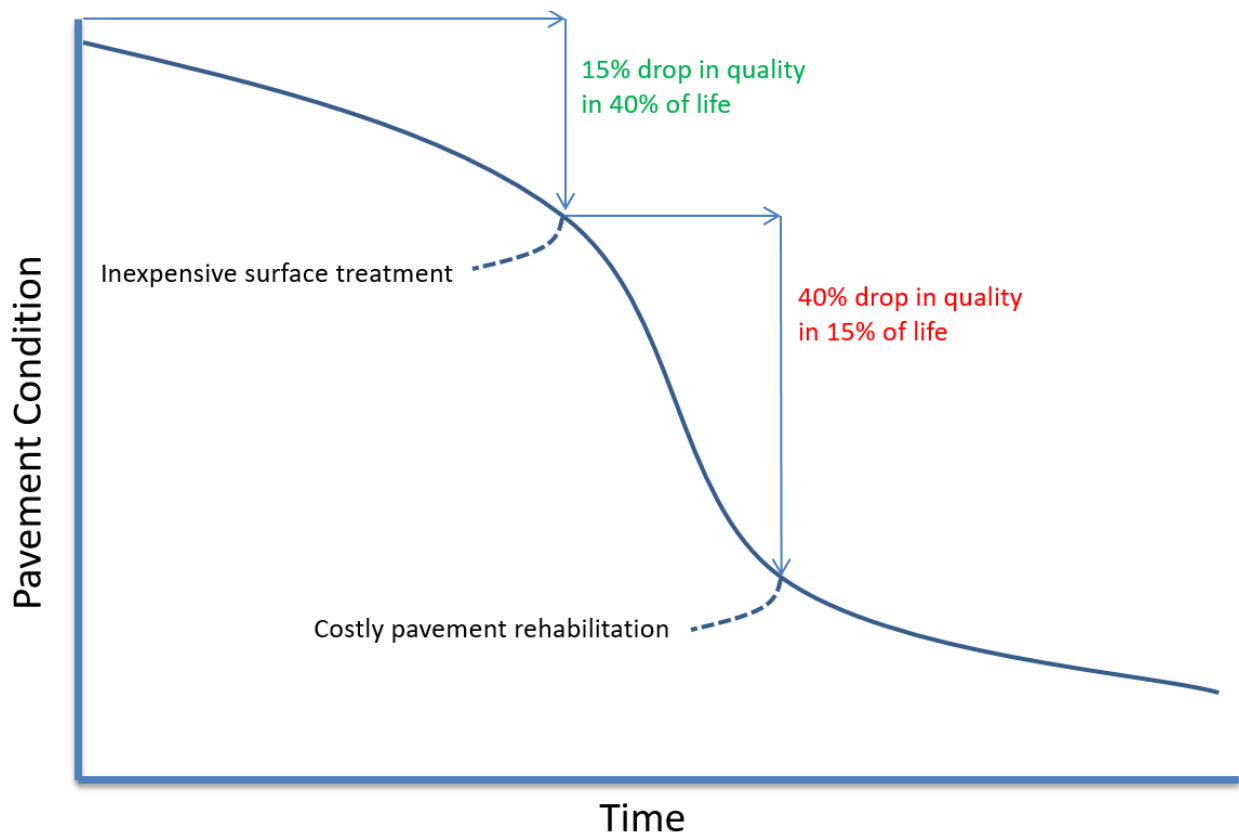
## 2.0 PRINCIPLES OF PAVEMENT MANAGEMENT

### 2.1. Foreword

This section covers the basics of pavement management, discussing its purpose of as well as best practices for M&R planning as they relate to the lifecycle of a pavement. It explains how these concepts interact within the ESA pavement management system that was implemented and will provide context for the findings and recommendations within the report.

### 2.2. Pavement Management Principles

Pavement management is the process of assessing, prioritizing, and preserving or rehabilitating pavements through a logical system that attempts to use available funds in the most cost-effective manner possible. It is generally an iterative process that grows in accuracy as more data becomes available to better refine prediction models. **Figure 3** illustrates that, typically, pavements start deteriorating rapidly once they hit a specific threshold. A nominal investment in cheaper surface treatments at 40% lifespan is much more cost-effective than deferring maintenance until heavier overlays or possibly reconstruction is required just a few years later. Streets that are repaired while in good condition will have an extended lifetime and will cost less over their lifetime than those left to deteriorate to a poor condition. Without an adequate routine pavement maintenance program, streets will require more frequent reconstruction, thereby requiring significantly greater funding.

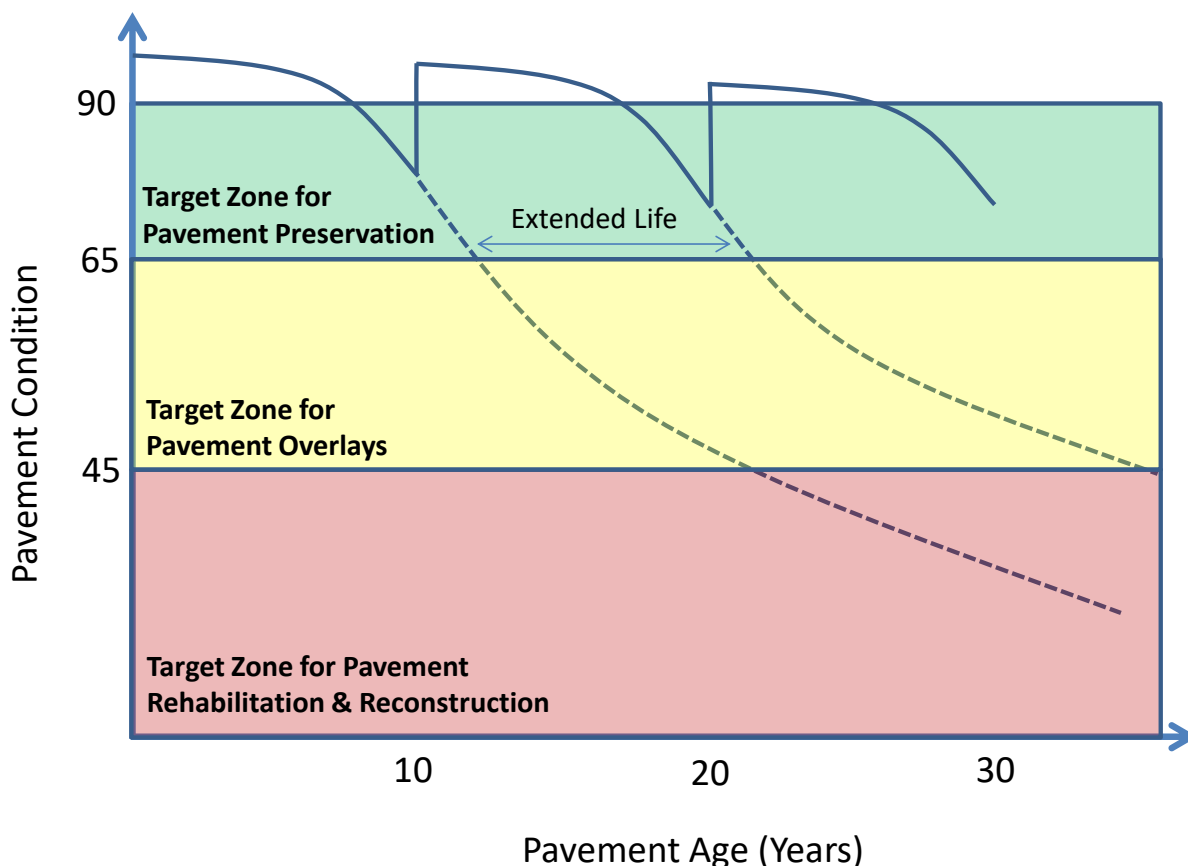


**Figure 3 - Pavement Deterioration and Life Cycle Costs**

The types of rehabilitation activities that the City chooses to deploy can have a significant effect on the longevity of a pavement. Depending on the zone in which a pavement falls, a detailed rehabilitation strategy set needs to be formed. Common rehabilitation types include Stop Gap, Rehabilitation, Reconstruction, and Preventive Maintenance. It is the proper incorporation and application of M&R activities within the Preventative Maintenance category that a pavement management program leverages. Popular examples of cost-effective preventive activities include:

- Crack and Joint Sealing
- Microsurfacing
- Rejuvenating agents
- Patching
- Fog, Slurry, and Chip Seals
- Thin Overlays

These activities help maintain and repair the surface integrity which can slow deterioration and depending on the treatment, also extend the life of a pavement. The outcome of this exercise is the long-term cost savings and an increase in network-level pavement quality over time. **Figure 4** illustrates the concept of extending pavement life through the application of timely M&R activities.



**Figure 4 - Pavement Life Cycle Curve**

The best method to obtain the most optimal usage of available funds or to determine the required funding to achieve a predetermined level of service is through the use of a pavement management system. An effective pavement management system can assist agencies in developing an organized catalog of pavement assets, store periodic condition assessments, track spending and costs, compare trends in data, and assess the effectiveness of maintenance activities and new technologies.

## 2.3. Pavement Management Program

The practical implementation of a pavement management program requires that an agency have an accurate understating of the assets under its management. To fulfill this requirement IMS was contracted by Northglenn, CO to conduct a comprehensive pavement condition assessment and pavement management analysis on the City's roadway network.

## 2.4. ESA Pavement Management System

The City primarily utilizes Easy Street Analysis™ (ESA) for its pavement management program. ESA is a proprietary spreadsheet-based software alternative developed by IMS. It is intended to be easy to use while still offering all of the capabilities of a standalone software package. ESA will allow the city to catalog, classify, assess, track, and analyze condition data to aid in the processes of budget planning and pavement rehabilitation.

More specifically the program will help the City through the process of pavement management by giving structure to the basic information required for a management system:



- Pavement Section Inventory
- Pavement Deterioration Modeling
- Prioritization
- Funding Analysis
- Inspection Data
- Rehabilitation Selections & History
- Work Planning
- Reporting

The following pages will briefly detail the reasoning behind why this information is collected and used within the management program.

### Pavement Section Inventory

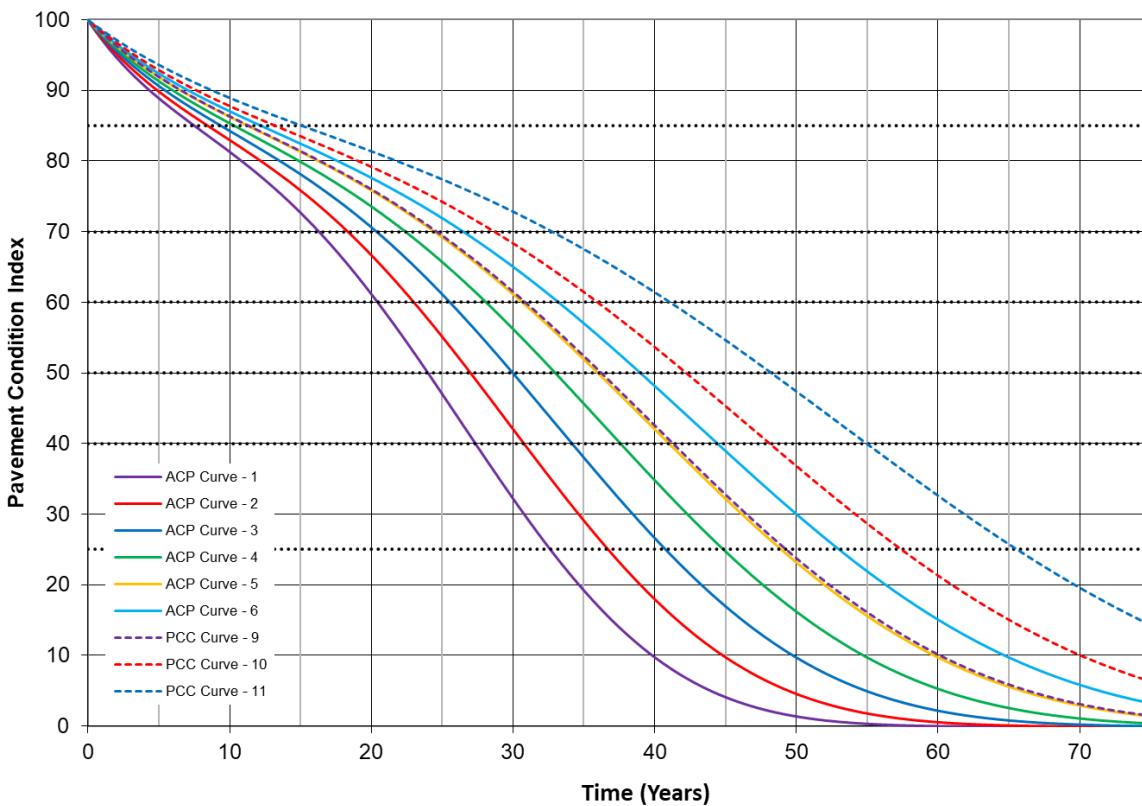
An accurate inventory of all City-owned streets is necessary to make any determinations, assumptions, or projections within a management system. The individual attributes about length, width, location, traffic use, surface type, condition, and other factors that may be tracked are all tied back to a single management segment within ESA and given a unique ID within the program. The attributes stored here determine what types of rehabilitation activities can be applied, how the section is prioritized within the system, and allow for placement and sorting during reporting.

### Inspection Data

ESA has the ability to use a blended condition index depending on the goals and requirements of the City which allows a custom reporting value to be built that is representative of aspects being considered to rank the condition of a pavement. The inputs for this index rely on inspection data from the field survey. In addition to the data collected for PCI, International Roughness Index (IRI) data was also used in ESA. Details on the individual components of the inspection are available in section 2.5

## Pavement Deterioration Modeling

Inspection data by itself is only able to represent conditions at the time of collection. Within ESA there are customizable curves that predict the rate of pavement deterioration based on a streets functional class, pavement type, and strength rating. The guiding concept is that pavements with like uses and attributes will deteriorate at similar rates. As such, high volume asphalt arterials in already poor conditions would be expected to deteriorate faster and are represented in **Figure 5** below with a purple line. On the opposite end of the spectrum, low volume concrete local streets would be expected to deteriorate slowly and are represented with a blue dashed line.



**Figure 5 - Pavement Deterioration Curves**

## Rehabilitation Work Selections & History

ESA uses a set of protocols that allow for activities to be assigned to PCI ranges based on filter criteria that give the City the ability to create detailed rehabilitation strategy sets for each functional class and pavement type according to the best practices determined for that pavement.

As planned rehabilitation work is completed a record of the work should be added to the pavement management system. This ensures that conditions are up to date for future selections and creates a repository of information to aid in planning.

## Prioritization

Within ESA the option is available to prioritize pavement projects for rehabilitation based on six main criteria: PCI, Cost of Deferral, Pavement Strength, Pavement Type, Functional Class, and the Area of a segment. Depending on the goals set forth at the beginning of the project these criteria can be weighted

differently based on their definition to create an overall priority factor for a project. Additional details on these factors are available in section 4.2.

### Project planning

The ability to plan work as needed allows the management program to better reflect the realities of a paving program. Certain constraints may be applied to funds that require their use within a certain year and activities relating to other assets may dictate the time and type of work to be performed. ESA allows for predefined projects to be entered into the management plan to account for work that is known. This ensures that the final outcome is consistent with overall city planning and accurately reflects current funding allocations.

In terms of pavement management efficiency, a program based on worst-first, that is starting at the lowest rated street and working up towards the highest, does not achieve an optimal expenditure of funds. Generally, under this scenario, agencies can not sufficiently fund pavement rehabilitation and lose ground despite injecting large amounts of capital into the network.

The preferred basis of rehabilitation candidate selection is to examine the cost of deferral of a street, against increased life expectancy.

### Funding analysis

The actual process of determining where and when to spend funds is a function of inputs mentioned in the section. Information from the street section inventory, condition survey, deterioration modeling, rehabilitation activity protocols, prioritization, and project planner are all assessed to predict the outcomes of funding scenarios. These can either be goal-based or budget-based. A more detailed description is available in Section 5.

### Reporting

ESA has the ability to generate basic reports for common data requests through a set of predefined layouts. This allows for quick access to section condition summaries, inspection data, budget scenario summaries, and data charts. The GIS data used to generate this report is also linked to the section summary information to allow for quick and easy visualizations of the data if imported into a GIS utility. An example of data, as presented in ESA, can be seen in **Figure 6**.

GISID	Agency ID	Street Number	Block Number	Street Prefix	On Street	From Street	To Street	FuncL Code	PaveType Code	Pavement Width (ft)	Pavement Length (ft)	Adft Area (sqft)	Pavement Area (sqft)	Pavement Condn Index (PCI)	Project ID	Project Length (ft)	Project Current PCI	Year of First Rehab	Segment Rehab Results	Rehab Activity Code	Rehab Activity	Avg Unit Rate (\$/yr)	Peripheral Concrete Costs (\$)	Segment Pavement Cost (\$)	Segment Total Cost (\$)	Whole Project Cost (\$)	P Year Post Rehab PCI
1295	1010	17			04TH ST	PICO BL	BAY ST	2	1	62	368	127,266	43	1780	738	42	1	Selected Yr 1	50	FWM + Thick Overlay (> 2.0 - 3.0)		60.50	0	160,930	160,930	223,669	89
1300	1010	18			04TH ST	BAY ST	BICKNELL AV	2	1	24	370	49,103	45	1780	738	42	1	Selected Yr 1	50	FWM + Thick Overlay (> 2.0 - 3.0)		60.50	0	62,739	62,739	223,669	89
1655	1025	1			07TH ST	CITY LIMIT - LA	AELAIDE PL	2	1	36	141	28,592	81	2900	141	80	1	Selected Yr 1	10	Slurry Seal / Seal Coat		2.40	0	1,421	1,421	1,421	80
1810	1025	32			07TH ST	OZONE ST	CITY LIMIT - LA	2	1	36	67	13,282	73	3620	67	72	1	Selected Yr 1	20	Slurry Seal / Seal Coat		7.75	0	2,186	2,186	2,186	84
3610	1115	9			26TH ST	MARGUERITA AV	ALTA AV	1	1	46	782	200,419	60	8060	1,391	62	1	Selected Yr 1	26	Slurry Seal / Seal Coat - Strctri Pch		9.75	0	40,892	40,892	73,447	83
3615	1115	10			26TH ST	ALTA AV	MONTANA AV	1	1	47	609	159,339	67	8060	1,391	62	1	Selected Yr 1	26	Slurry Seal / Seal Coat - Strctri Pch		9.75	0	32,555	32,555	73,447	83
4915	1200	1			CLOVER ST	ASHLAND AV	PER AV	5	1	40	360	80,187	28	9160	369	27	1	Selected Yr 1	56	FWM + Thick Overlay (> 2.0 - 3.0) + Strctri Pch		56.50	0	94,907	94,907	94,907	81
4515	1240	4			BROADWAY	5TH ST	6TH ST	2	1	47	399	104,218	30	10140	399	29	1	Selected Yr 1	56	FWM + Thick Overlay (> 2.0 - 3.0) + Strctri Pch		65.00	0	142,155	142,155	142,155	80
4700	1250	4			CALIFORNIA AV	4TH ST	5TH ST	5	1	24	399	53,118	42	10580	399	41	1	Selected Yr 1	50	FWM + Thick Overlay (> 2.0 - 3.0)		52.50	0	58,695	58,695	58,695	91
4975	1285	10			CLOVERFIELD BL	DELAWARE AV	VIRGINIA AV	2	1	60	607	202,429	61	11060	607	60	1	Selected Yr 1	26	Slurry Seal / Seal Coat - Strctri Pch		9.50	0	40,366	40,366	40,366	83

Figure 6 - Example of ESA City Data

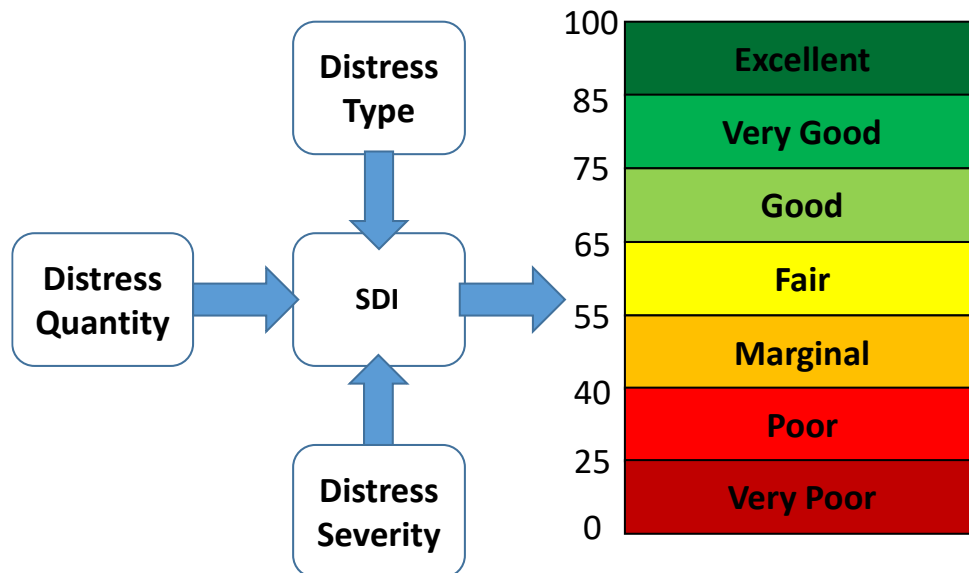
## 2.5. Pavement Condition Survey

The goal of the pavement condition survey is to determine an accurate rating for each pavement section. The process of collecting and assessing data involves both automated and manual observations that originate from the data collected with the Road Surface Tester using Pavemetrics Laser Crack Measuring System (LCMS-2) downward imaging lasers, an array of 4k cameras, and trained rating personnel.

Within the “Network Analysis” tab in ESA, IMS has populated values for a SDI, Ride score, and Strength Rating. These three indices form the foundation on which ESA operates.

### Surface Distress Index (SDI)

ASTM D6433 categorizes surface distress observations based on the extent and severity of distresses encountered along the length of the roadway for asphalt and concrete pavements. Presented on a 0 to 100 scale, the Surface Distress Index (SDI) is an aggregation of the observed pavement defects. However, not all surface distresses are weighted equally. Certain load-associated distresses (LAD) (caused by traffic loading), such as rutting or alligator cracking on asphalt streets, or divided slabs on concrete streets, have a much higher impact on the SDI than non-load associated distresses (NLAD) such as raveling or longitudinal and transverse cracking. Even at low extents and moderate severity (less than 10% of the total area), LAD can drop the SDI considerably. The rating systems also incorporates algorithms to correct for multiple or overlapping distresses within a segment to account for multiple distresses that may arise from a single cause. The SDI inputs are shown in **Figure 7**.



**Figure 7 - SDI Inputs and Detailed Scale**

ASTM D6433 covers nearly forty unique distress types that may or may not be present in an agency’s road network. For that reason, IMS uses a modified approach that collects the most common and relevant distresses.

The descriptions on the following page outline some of the distresses collected for the City:

**Table 2 - Distress Descriptions**

**Alligator Cracking** – Quantified by the severity of the failure and square footage. This cracking is caused by the repeated bending a pavement experiences as vehicles pass over it. The cracks propagate from the bottom, meaning that structural failure has occurred. As a load-associated distress, it has a significant impact on the condition score, even at low extents.



**Rutting** – Starting at a minimum depth of ¼ inch, ruts are quantified by their depth and square footage. Rutting is caused by the permanent deformation of the pavement and/or subgrade layers. Low densities of rutting can have a large impact on the final condition score due to their implication of possible structural failure.



**Longitudinal & Transverse Cracking** – Quantified by their length and width. These cracks can be the result of pavement shrinkage due to natural daily and seasonal temperature cycles, construction issues, or other factors.



**Block Cracking** – Quantified by their width and square footage, these cracks form interconnected longitudinal and transverse cracks that divide the pavement into approximately rectangular pieces. Block cracking is the result of aging and environmental factors.



**Patching** – Quantified by the square footage and severity of patches. Even a good quality patch is considered a surface defect and affects the ride quality and condition of a pavement.



**Raveling** – This is the loss of coarse aggregate on the pavement surface and is measured by the severity and square footage affected.



**Bleeding** – This is the presence of free asphalt binder on the roadway surface, which is caused by either an excess of asphalt in the pavement or insufficient voids in the matrix. The result is a pavement surface with reduced skid resistance. This distress is measured by severity and square footage.



**Edge Cracking** – Running parallel to the road and usually within 1 to 2 feet of the outer edge of the pavement, this distress is caused by traffic loading and weakened base conditions resulting from poor drainage. It is measured in linear feet.



**Distortion** – This includes various localized unevenness in the surface of the pavement including bumps and sags, depressions, swell, corrugation, or shoving. This distress can be caused by a number of factors, including construction issues, subgrade failure, mixture failure, environmental influence, etc.



**Weathering** – This is the wearing away of asphalt binder and fine aggregate matrix, which is quantified by severity and square footage.



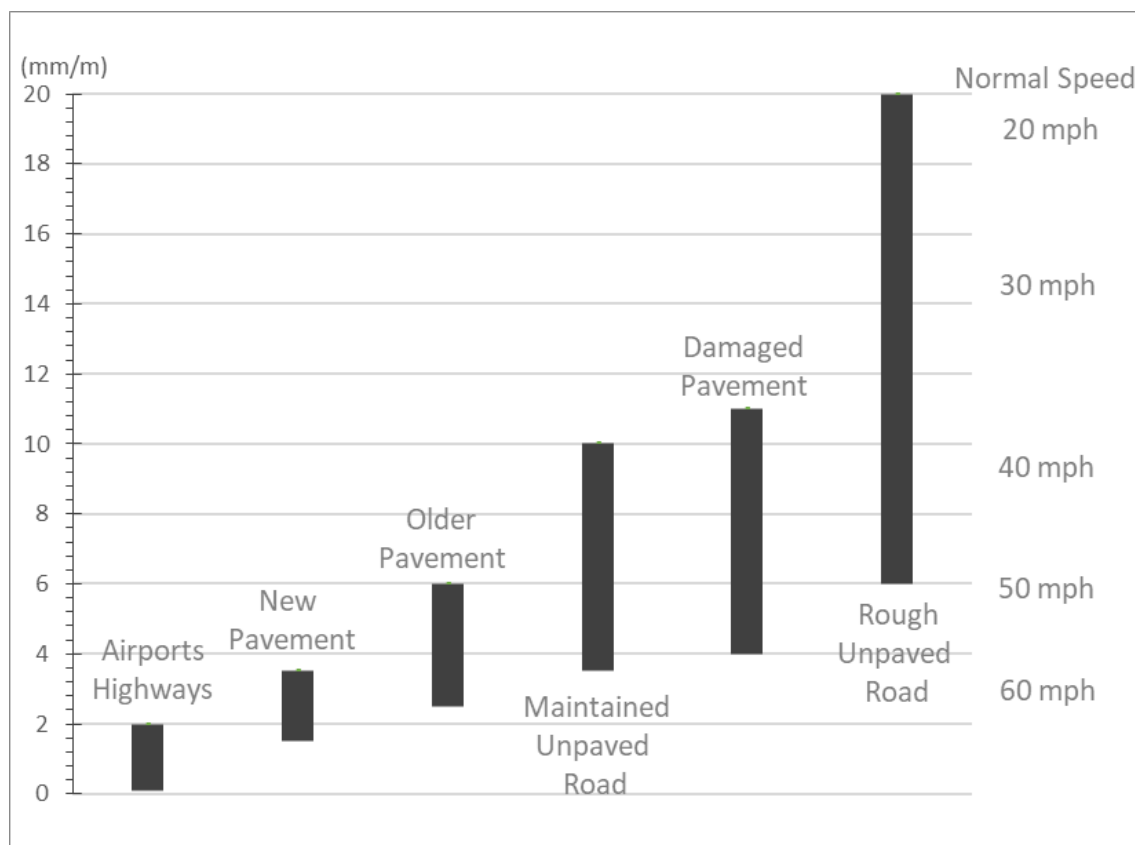


Similar distresses were collected for concrete streets, including **divided slabs, corner breaks, joint spalling, faulting, polished aggregate, and scaling**, which can be seen in the images below.



### Roughness Index (RI)

The Roughness Index (RI) gives a measure of ride quality and is recorded following the industry-standard ASTM E1926 for determination of International Roughness Index (IRI). This value is generated from the longitudinal profile measured by the LCMS as it records the change in elevation over a distance. Once calculated it is expressed as a slope and reported in millimeters/meter (mm/m). Common IRI levels for new, older, and damaged pavements are displayed in **Figure 8**.



**Figure 8 - IRI Scale Definitions**

In order to align with the PCI scale so that a blended condition score can be formed, the IRI value is converted to a 0 to 100 score and reported as the Ride Index (RI) as follows:

$$RI = (11 - 3.5 \times \ln(IRI)) \times 10$$

$\ln(IRI)$  is the natural logarithm of IRI.

For some context, a newer street would generally have an RI above 85, while one in need of an overlay would be in the range of 40 to 70. Roadways in poor condition typically have RI values below 40 but can achieve higher blended scores if the distresses causing the low RI score are not the result of structural failure or another severe cause. For example, hastened construction can result in a pavement surface with less than desirable smoothness, which would translate to a low RI value. But since the distress/imperfection is not due to severe failures within the pavement structure, the blended PCI value may not suffer greatly.

**Structural Index** – Per the wishes of the City, the network of streets was not subjected to conventional structural testing, which is typically performed using a Falling Weight Deflectometer (FWD). However, the structural performance and capacity of the roadways still needed to be quantified to run pavement analysis. So, the relationship between the PCI and the amount of load-associated structural distresses was analyzed, and each pavement section was determined to have a Weak, Moderate, or Strong strength rating. The strength rating then translated to a Structural Index (30, 60, or 90 for weak, moderate, and strong respectively). It is important to note that these SI values were not used in determining the overall pavement condition score, as they were not calculated from true structural testing. These SI values were simply used to classify the pavement strength and aid in selecting appropriate rehabilitation strategies.

**Pavement Condition Index (PCI)** – Following the field surveys, the condition data was imported to ESA for calculating the overall PCI. The PCI for each network was calculated using the following weighing factors:

$$\text{PCI} = 67\% \text{ SDI} + 33\% \text{ RI}$$

## 2.6. Field Survey Methodology

IMS deployed one of its LCMS2 road surface testing vehicles to perform continuous sampling data collection activities as part of the semi-automated pavement condition survey. The LCMS2 equipment provides three-dimensional high-speed, mm-level scanning and pattern recognition analysis from two downward lasers mounted at the rear of the vehicle. This provides a higher level of detail in determining crack length and width measurement over the sample area. The LCMS2 device also operates as a Class I profile device that collects longitudinal profile (in the form of IRI) and transverse profile (rutting) using the advanced 3D profile laser scanning technology. The vehicle is also equipped with a high accuracy GPS device and several high definition 4k cameras that collect forward, downward, and rearward images.

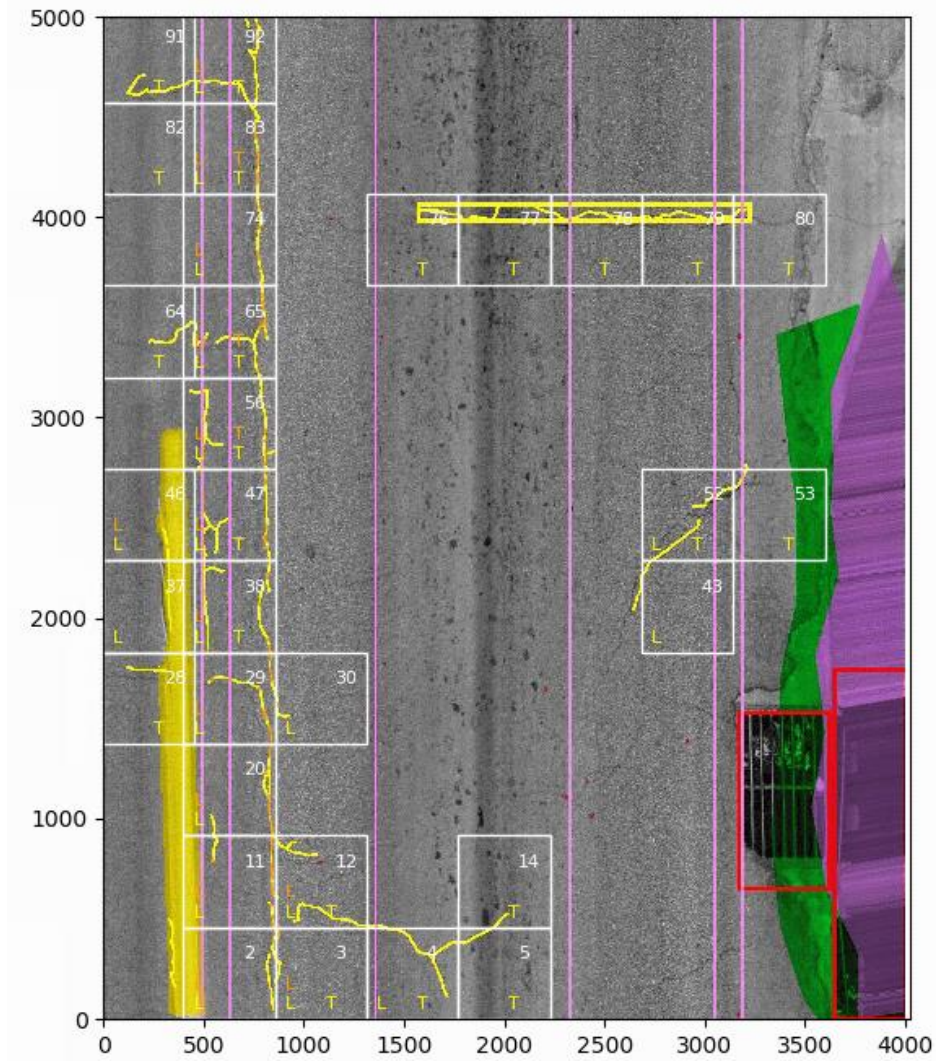


**Figure 9 - LCMS2 data collection vehicle**

While the LCMS2 automatically collects the majority of visible distresses, the RST platform integrates highly trained IMS technicians to input additional changes in observed distress severities and extents or identify specific roadway assets or attributes such as curb reveal or lip of gutter information by means of a touchscreen-based tablet computer connected to the data flow through time code, GPS, DMI distance and inventory control. The data is then processed in the IMS office to generate extent quantities for each observed distress severity level which are then used to calculate a PCI.

## **2.7. Data Quality Assurance**

The final step in determining PCI scores requires the field data to be subject to multiple stages of quality checks. The first phase is a rule-based check that flags roads based on expected outcomes from the automatic crack detection and processing parameters used to convert the LCMS2 data into severity and extent data. Part of this output is the identification of areas that are within the sample area but should be excluded from the assessment. This can include the edge of pavement or curb (purple and green highlight), manhole covers and inlets (red box), and pavement markings (yellow highlight) as seen in **Figure 10**. This is combined with information from our field raters about distresses that may not be present in a traditional manner or may be outside the guidelines set forth in ASTM D6433.



**Figure 10 - Processed LCMS2 Image**

A second review is then performed by a team of raters who are well-versed in both the distress standards and the data in its digital format. Using the downward laser scans, field notes, and 4K images collected from the survey the Quality Control (QC) team is able to verify the accuracy of what the LCMS has collected and what was flagged in the field.



Figure 11 - QC Image

To further ensure accurate condition data, spot checks are conducted on a network-wide basis by the QC team and the engineering staff. Once confidence in the integrity of the data has been achieved, an initial condition sheet is submitted for review by City staff.

## 2.8. Summary

This section outlined the fundamental concepts of pavement management and the process by which the pavement management system was implemented for the City. The operating parameters of ESA were reviewed, and the inputs provided by the LCMS-2 technology were explained to provide context for PCI, Roughness Index, and Pavement Strength.

## 3.0 PAVEMENT CONDITION SURVEY RESULTS

### 3.1. Foreword

This section will review the results of the pavement condition survey performed in April 2022. First is a summary of conditions in the four functional classes used in the City’s analysis. Next, this section will review photos of the network taken from the RST. Finally, a series of charts will summarize the findings of the condition survey and the overall PCI distribution of the City pavement network.

### 3.2. City Street Inventory and Condition Summary

The City of Northglenn is currently responsible for approximately 117 centerline miles of pavement with an overall PCI of 67 and a backlog of 8%. **Table 3** presents the City’s inventory and pavement condition breakdown between different functional classes. Detailed information for each management section is available in **Appendix A**.

**Table 3 - Network Inventory Summary by Functional Class and Pavement Type**

	Pavetype	Network	Arterial	Collector	Local
Segment (Block) Count	All Streets	1155	100	84	971
	Asphalt	1138	87	84	967
	Concrete	17	13	0	4
Network Length (ft):	All Streets	595,540	93,941	34,924	466,674
	Asphalt	582,020	84,105	34,924	462,991
	Concrete	13,520	9,836	0	3,683
Network Length (mi):	All Streets	112.8	17.8	6.6	88.4
	Asphalt	110.2	15.9	6.6	87.7
	Concrete	2.6	1.9	0.0	0.7
Average Width (ft):	All Streets	42.1	44.3	45.1	40.4
	Asphalt	41.6	41.9	45.1	40.2
	Concrete	62.5	64.5	0.0	57.0
Network Area (yd2):	All Streets	2,787,278	462,505	174,840	2,093,177
	Asphalt	2,693,455	392,005	174,840	2,069,854
	Concrete	93,823	70,500	0	23,323
Current Pavement Condition Index (CPCI) 9/2/22	All Streets	67	72	69	66
	Asphalt	67	70	69	66
	Concrete	80	84	0	68
Pavement Condition Index (Surveyed PCI)	All Streets	63	71	68	61
	Asphalt	62	69	68	61
	Concrete	81	85	0	69
Current Backlog (%)	All Streets	8	Percentage of Network with a PCI < 40		

### 3.3. City Network Condition Imagery

The images presented below provide a sampling of the City's streets that fall into the various condition categories with a discussion of potential rehabilitation strategies.

#### Very Poor (PCI = 0 to 24) – Complete Reconstruction



**Huron Street from West 98<sup>th</sup> Avenue to Melody Drive (GISID 1037, PCI = 20)** – Rated as Very Poor, this street displays a large quantity of alligator cracking that is severe enough to suggest that the pavement structure is inadequate for current traffic loads. The rehabilitation of roads in this condition through a mill and overlay is generally ineffective, as the failures usually extend to the bottom of the pavement layer. Streets in this condition require rehabilitation that involves removal and replacement of the asphalt layer, base stabilization, or complete reconstruction based on design requirements.

Deferral of reconstruction of streets rated as Very Poor will not cause a substantial decrease in overall pavement quality. The streets have passed the opportunity for overlay-based strategies, meaning that reconstruction, which is expensive, is the most suitable solution. So, Very Poor streets are often deferred in favor of rehabilitating more streets at lower costs, resulting in a greater net benefit to the City. This strategy however must be sensitive to citizen complaints forcing the street to be selected earlier. In addition, this type of street can pose a safety hazard for motorists since severe potholes and distortions may develop. It is important to consistently monitor these streets and check for potholes or other structural deficiencies until the street is eventually rebuilt.

**Poor (PCI = 25 to 39) – Last Opportunity for Surface/Base Rehabilitation**



**Huron Street from West 102<sup>nd</sup> Avenue to West 102<sup>nd</sup> Place (GISID 1106, PCI = 30)** – Rated as Poor, this segment still has some remaining life before it becomes a critical reconstruction need. As evident in the imagery, a fair amount of the segment is severely cracked. If left untreated, within a short period of time, a partial to full reconstruction would be required.

On heavily trafficked roadways, Poor streets often require partial to full reconstruction. On local roadways, they require removal of the pavement surface through grinding or excavation, base repairs, restoration of the curb line and drainage, and then placement of a new surface.



### Marginal (PCI = 40 to 49) – Thicker Overlays



**Pinnacle Creek from West 112<sup>th</sup> Avenue to Huron Street (GISID 1463, PCI = 45)** – On this street, the primary cause of deterioration is the alligator and longitudinal cracking.

Marginal streets that display high amounts of load associated distresses (LAD) are selected as a high priority for rehabilitation as they generally provide the best cost/benefit ratio to the City. If left untreated, Marginal streets with high amounts of LAD will deteriorate to become partial reconstruction candidates. Marginal streets that are failing due to materials issues or non-load associated failures may become suitable candidates for thick overlays if deferred, without a significant cost increase.

**Fair (PCI = 50 to 59) – Thin to Moderate Overlays**



**Gilpin Street from East 115<sup>th</sup> Avenue to Irma Drive (GISID 1899, PCI = 55)** – Fair streets have similar characteristics to Marginal streets in that the distresses present tend to be localized and moderate in severity. However, the distresses will predominately be non-load related (i.e., caused by environmental or other factors).

Like Marginal streets, Fair streets can provide a good cost/benefit ratio to an agency if addressed with an adequate rehabilitation technique. Stretching the application for surface treatments into this range can pose a cheap alternative to overlays but does not provide the appropriate renewal to the structural capacity of the pavement and may allow load related deterioration to continue unabated.

## Good (PCI = 60 to 69) – Surface Treatments



**Humboldt Street from East 112<sup>th</sup> Place to Franklin Street (GISID 1826, PCI = 66)** – Rated as Good, the primary cause of deterioration is the longitudinal and transverse cracking which has been mostly sealed. The pavement surface could be restored further with spot patching to fully waterproof the pavement.

Preventive measures on streets considered “Good” can have a positive impact of the City’s funding needs. While the expected life of a slurry seal is not as long as that of an overlay, its ability to slow deterioration and relative low cost can free up funding for streets in worse condition.

**Very Good (PCI = 70 to 79) – Surface Treatments and Localized Rehabilitation**



**Washington Street from East 117<sup>th</sup> Avenue to East 117<sup>th</sup> Avenue (GISID 1925, PCI = 74)** – Rated as Very Good, this road displays minor amounts of transverse cracking. This street is an example of a candidate for preventative maintenance, such as crack sealing, to extend the life of the roadway.

Routine maintenance prevents water intrusion by sealing and slowing crack growth. By keeping water out of the base layers, the pavement life is extended without the need for heavier rehabilitations.

**Excellent (PCI = 85 to 100)**



**Race Street to East 123<sup>rd</sup> Avenue (GISID 2041, PCI = 95)** – Rated as Excellent, this pavement displays little to no surface distresses. The ride is smooth, and the surface and the base are intact. Excellent roads should be periodically assessed for crack development that would trigger routine maintenance activities.

### 3.4. City Network Condition Distribution

Figure 12 shows the distribution of pavement condition for the roadway network in Northglenn.

- Twenty-six percent (26%) of the network can be considered in “Excellent” condition and should be closely monitored to ensure timely application of early localized preventive measures.
- Seventeen percent (17%) of the network falls into the PCI range considered “Very Good”. These are roads that benefit most from preventative maintenance techniques such as spot patching and slurry seals.
- Fifteen percent (15%) of the streets are rated as “Good” and may still be candidates for slurry seals or thin overlays.
- Thirty-four percent (34%) of network can be considered “Fair” to “Marginal” condition and representing candidates for progressively thicker overlay-based rehabilitation. If left untreated, they will decline rapidly into reconstruction candidates.
- The remaining eight percent (8%) of the network is rated as “Poor” or “Very Poor”, meaning these roadways have deteriorated to the point where surface rehabilitation can no longer restore the pavement to a point of structural adequacy. Rehabilitation of the entire pavement structure is required for these segments.

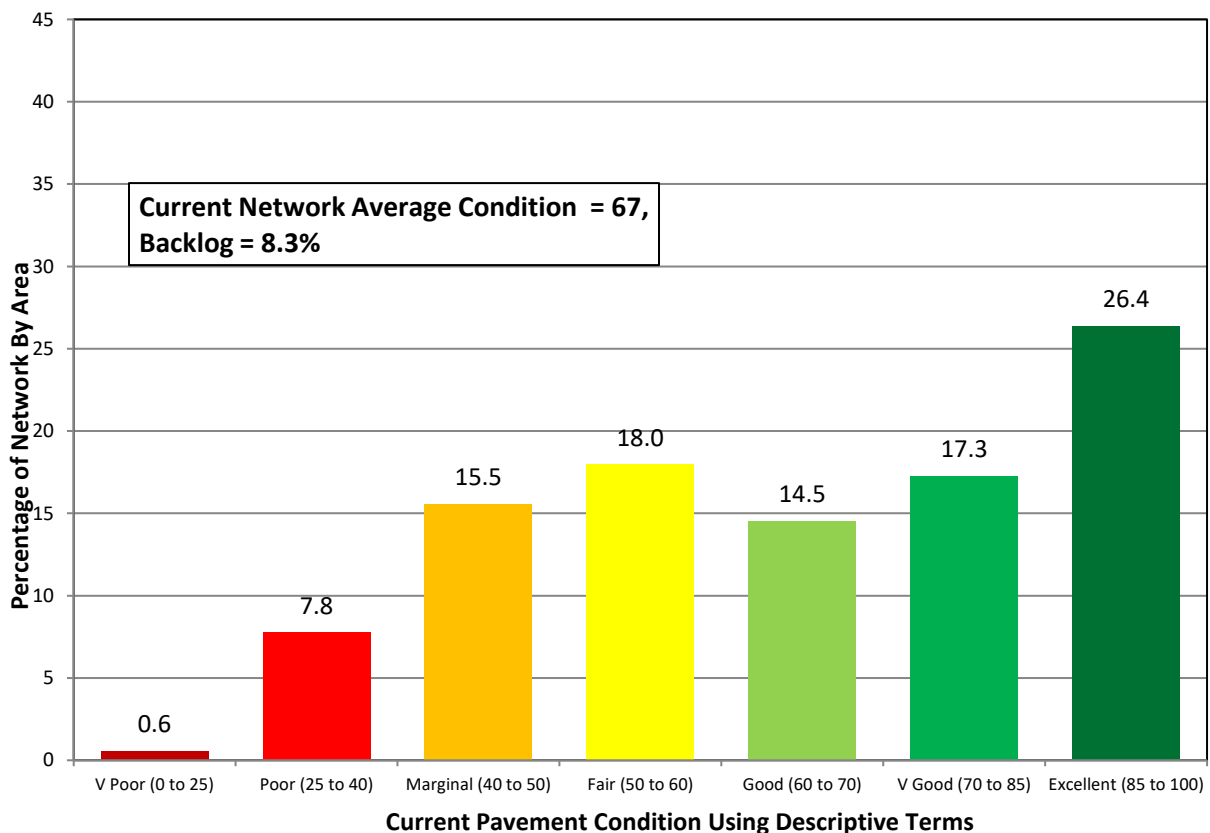
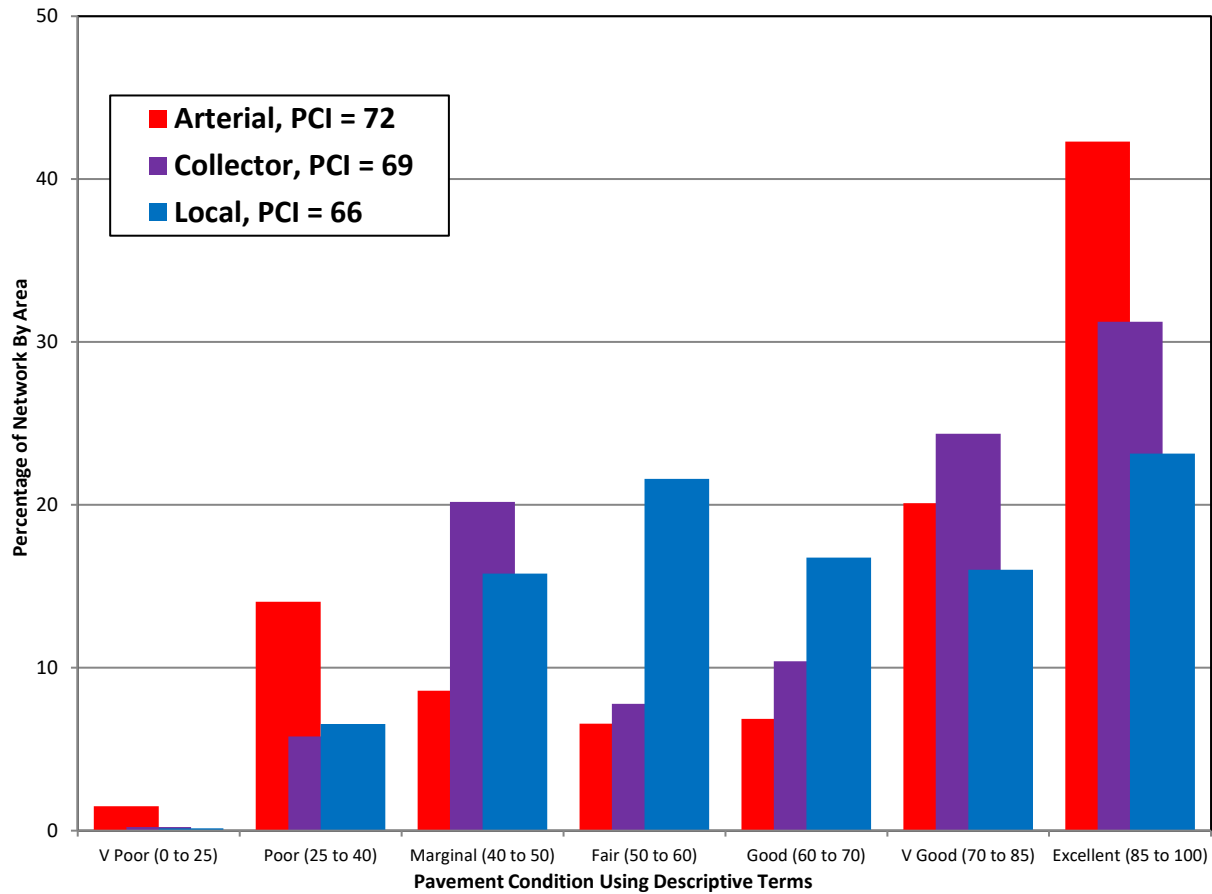


Figure 12 - Roadway Network Present Status Using Descriptive Terms

### 3.5. Condition By Functional Classification

When assessing the overall condition of the network there are additional subsets of data that can give a deeper insight into where an agency may want to focus resources. **Figure 13** highlights the pavement condition distribution for each functional class. It is important to note that arterial roadways, which are the streets that support the greatest traffic volumes in a given timeframe, should receive additional consideration when selecting rehabilitation candidates.



**Figure 13 - Condition Rating by Functional Classification**

### 3.6. Structural and Load Associated Distress Analysis

As mentioned in section 2, pavement distresses may be categorized into two classifications – load associated distresses (LAD) and non-load associated distresses (NLAD). Load associated distresses, such as rutting and alligator cracking, are those that are directly caused by traffic loading and lead to decreased structural capacity. Non-load associated distresses are those that result from material or environmental issues, including shrinkage (transverse) cracking, bleeding, and raveling. Generally, load associated distresses affect the overall condition score more than non-load associated distresses due to their implications of structural failure. The roadways were classified as Weak, Moderate, or Strong depending on the type of distresses found on their surfaces. The strength rating then translated to a Structural Index (30, 60, or 90 for weak, moderate, and strong respectively).

Weak pavements are those with a high ratio of load associated distresses compared to their PCI score and generally require increased pavement thickness to achieve long-term pavement life. Strong pavements are those that have a low load associated distress to PCI ratio and have suitable structural capacity. Surface treatments are acceptable rehabilitation solutions. Moderate pavements are those that require localized rehabilitation and/or increased thickness to achieve full pavement life. These are pavements that are starting to display structural failures, such as rutting or alligator cracking.

The purpose of the structural analysis is twofold:

- The Structural Index provides input into which performance curve each segment is to use.
- Structural analysis assists in rehabilitation selection by constraining inadequate pavement sections from receiving too light of rehabilitation and conversely, identifying segments suitable for lighter treatment.

The primary area of concern is the street segments that have a high pavement condition score yet exhibit low structural characteristics (weak to moderate pavement strengths), meaning that the pavement is determined to be in good condition at the surface level but is more than likely in less than ideal condition structurally. When an overlay is to be applied, project-level testing and design should be completed to ensure that structural integrity is restored.

### 3.7. Summary

Section 3 reviewed the results of the condition survey in the City. The section described each of the four functional classifications in the City and outlined their respective conditions on the PCI scale. The PCI scale was explained further through a series of pavement photographs that were taken during the 2022 survey. The section concluded with a discussion on the overall pavement condition distribution in the City and some useful charts that help put the survey results into perspective. **The network average PCI in Northglenn is 67 with a backlog of 8%.**



## 4.0 REHABILITATION PLAN AND BUDGET DEVELOPMENT

### 4.1. Foreword

This section discusses the results of the pavement management analysis that was performed using the ESA pavement management system. First is an overview of the assumptions that were used when implementing the system. Some of these include the development of accurate unit rates, and the selection methodology for rehabilitation candidates. Next, in 4.3, the results of each of the various budget runs is detailed, along with their predicted conditions. This is highlighted further through a series of charts that are used to demonstrate the advantages and disadvantages of various funding models.

### 4.2. Key Analysis Set Points and Assumptions

Pavement management analysis requires user inputs to complete its condition forecasting and prioritization. A series of operating parameters were developed to create an efficient program that is tailored to the City's needs.

#### Selecting Segments for Rehabilitation

The selection of rehabilitation candidates through a worst first approach or subjective committee input is neither efficient nor cost effective. A series of criteria must be established and their importance in the selection process determined. ESA has defined commonly used criteria within the program that allows different weighting factors to be applied depending on the City's goals.

- **PCI** – As mentioned earlier in this section the results of the pavement condition survey are used to generate a Pavement Condition Index that ranges from 0-100 where 0 is considered the worst and 100 the best. This factor can be given a higher weight to give greater priority to poor condition streets.
- **Cost of Deferral** – As time passes a pavement will deteriorate and require more costly repairs as it ages. ESA can be configured to prioritize streets nearing the point where this cost increase occurs.
- **Pavement Strength** – Through the use of deflection testing or the prevalence of load associated distresses the relative strength of a pavement can be determined. A prioritization factor can be applied that gives preference to streets that may deteriorate faster in order to apply more cost-effective rehabilitation early in the life cycle.
- **Pavement Type** – Depending on costs, design life, and the City's goals, a weighting factor can be applied based on the materials used to construct the pavement
- **Functional Class** – Generally higher volume streets are given the greatest priority within a program since they serve the most vehicles.
- **Area** –Project selection can consider the size of a project when determining rehabilitation priority. The City can decide to select large groups of streets over small ones if that approach suits the goals set forth at the beginning of the project.

For the City, weighting factors for these categories were set to maximize the savings from the concept of cost of deferral and address lower PCI 'Weak' streets in order to minimize backlog growth.

## Rehabilitation Strategies and Unit Rates

The rehabilitation strategies and unit rates used in the budget analysis are the driving factors in determining funding requirements for the City. A detailed listing of how each rehabilitation activity is applied and its associated cost can be found in **Table 4**. Some important parameters include:

- **Pavement Type** – The City of Northglenn has a majority asphalt network with a small fraction of concrete. The lists of appropriate rehabilitation activities for these two pavement types are unique and vary in cost.
- **Rehab Activity** – The assigned identifier and name to each rehabilitation strategy. Various degrees of slurry sealing are outlined to highlight the increasing cost associated with additional patching requirements for lower PCI streets.
- **Min, Max, and Critical PCI** – These values compose the PCI range for which a particular rehabilitation activity can be applied where the Min and Max values define the upper and lower limit, and the Critical PCI is the point at which the rehabilitation becomes a higher priority to take advantage of the cost of deferral factor. Overlap in the PCI range allows for additional rehabilitation differentiation based on pavement strength.
- **Unit Rates** – The rehab costs are presented on a per square yard basis for each pavement type, functional class, and rehabilitation activity combination. A base unit rate is established for the lowest assumed cost for a work type and adjusted for each functional class depending on the additional work that may be required for traffic control, intersection improvements, landscaping, utility adjustments, and right-of-way (ROW) infrastructure. IMS worked closely with the City to determine rates that accurately represent the cost of work.

**Table 4 – Rehabilitation Rates**

Pavetype	Rehab Activity	PCI			Base Unit Rate (\$/yd2)	Arterial Unit Rate (\$/yd2)	Collector Unit Rate (\$/yd2)	Local Unit Rate (\$/yd2)	Parking Lot Unit Rate (\$/yd2)
		Min PCI	Critical PCI (Need Year)	Max PCI					
All	Routine Maintenance	85	100	100	0.00	0.00	0.00	0.00	0.00
Asphalt	Slurry Seal / Seal Coat	80	82	85	<b>2.50</b>	2.80	2.70	2.60	2.60
Asphalt	MicroSurface / Chip Seal	70	73	80	<b>3.50</b>	3.90	3.80	3.70	3.60
Asphalt	Edge Mill + Thin Overlay (1.5 - 2.0)	60	63	70	<b>19.00</b>	21.00	20.50	20.00	19.50
Asphalt	EM/FWM + Moderate Overlay (2.0 - 3.0)	50	54	60	<b>19.00</b>	22.00	21.25	20.50	19.75
Asphalt	FWM + Thick Overlay (> 2.0 - 3.0)	40	44	50	<b>25.00</b>	30.50	29.00	27.50	26.50
Asphalt	Surf Recon + Base Rehab / FWM + Strctrl Ptch + Olay	25	30	40	<b>48.00</b>	58.00	55.50	53.00	50.50
Composite	Surf Recon + PCC to Base/FWM + Strctrl Ptch + Olay	25	30	40	<b>52.00</b>	63.00	60.00	57.50	54.50
Asphalt	ACP Full Depth Reconstruction	0	15	25	<b>65.00</b>	71.50	70.00	68.50	66.50
Composite	Full Depth Recon + PCC to Base	0	15	25	<b>73.00</b>	80.50	78.50	76.50	75.00
Concrete	PCC Jnt Rehab & Crk Seal	80	82	100	<b>8.00</b>	8.75	8.50	8.50	8.25
Concrete	PCC Localized Rehab	70	73	80	<b>12.00</b>	13.75	13.50	13.00	12.50
Concrete	PCC Slight Pnl Rplcmnt (<10%)	60	63	70	<b>24.00</b>	29.00	27.75	26.50	25.25
Concrete	PCC Moderate Pnl Rplcmnt (< 20%)	50	54	60	<b>37.00</b>	47.00	44.50	42.00	39.50
Concrete	PCC Extensive Pnl Rplcmnt (<33%)	40	44	50	<b>54.00</b>	72.00	67.00	62.50	58.00
Concrete	PCC Partial Reconstruction	25	30	40	<b>68.00</b>	86.50	81.50	77.00	72.50
Concrete	PCC Full Depth Reconstruction	0	15	25	<b>99.50</b>	132.50	123.50	115.00	107.00

### 4.3. Network Budget Analysis Models

By combining the condition assessment, deterioration model, prioritization factors, and rehabilitation assignments, the ESA program can determine the outcomes of various funding levels or suggest the funds required to attain a goal. IMS ran a series of budgets to model network trends and estimate the funding levels needed to reach certain condition and distribution targets. The results of this analysis are outlined in this section.

#### Budget Targets

The following scenarios were generated to forecast the outcomes of the current estimated City budget over the next five years and determine what level of funding may be appropriate going forward. The values for backlog and PCI have been rounded to the nearest whole number in order for the figures to be more legible. Varying budget figures will have slightly different outcomes that are visible in the charts but may not be completely represented in the legend text.

- **Northglenn Budget** (Green Line) – This represents the City’s current average annual budget of **\$1.5M/Yr.** dedicated to pavement preservation and rehabilitation. This level of funding will result in a network average PCI score of **64** and a backlog of **17%** after five years. Additionally, this budget was extrapolated out in a 5-year model that can be seen in **Figure 17**.
- **Steady State PCI** (Red Line) – This is simply the funds required to maintain the current network average PCI at a **67**. The annual budget required to do so is approximately **\$2.5M/Yr.** Backlog will grow to **12%** of the network after five years.
- **Backlog Control** (Dotted Purple Line) – A backlog control budget was developed in order to cap backlog growth at a maximum level of **11%**. The required level of funding to do so is estimated at **\$2.8M/Yr.** and lowers the PCI slightly to **69** after five years.
- **PCI Control** (Backlog Control Line)– A PCI control budget was developed in order to maintain a minimum PCI of **60**. This results in a budget value of **\$480K/Yr.** and a backlog increase to **23%** after five years.

The results of the analysis are summarized in **Figure 14**. The X-axis highlights the annual budget, while the Y-axis plots the 5 Year Post Rehab Network Average PCI value. The diagonal blue line is the network trend model developed to show estimated PCI along with a funding range up to \$4M/Yr.

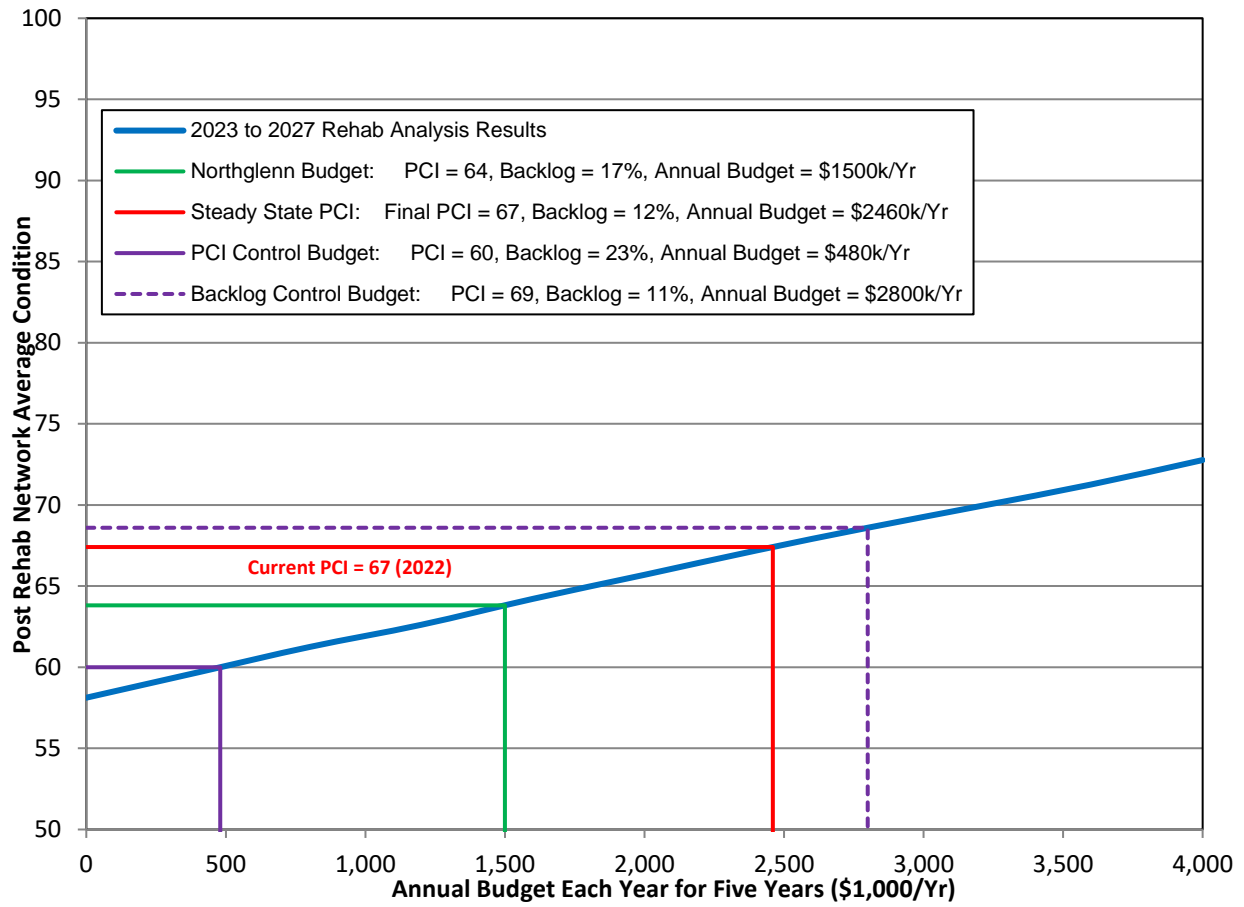


Figure 14 – 5-Year Post Rehab Network PCI Analysis Results

Figure 15 presents the resultant network backlog against the annual budget. It is similar to Figure 14, but instead of plotting the average PCI score, the blue diagonal line represents the total backlog after 5 years. The City currently maintains a backlog of 8%. While some growth is acceptable, every effort should be made to keep that value as low as possible. As the backlog grows, the funding required to return to the current level will increase considerably.

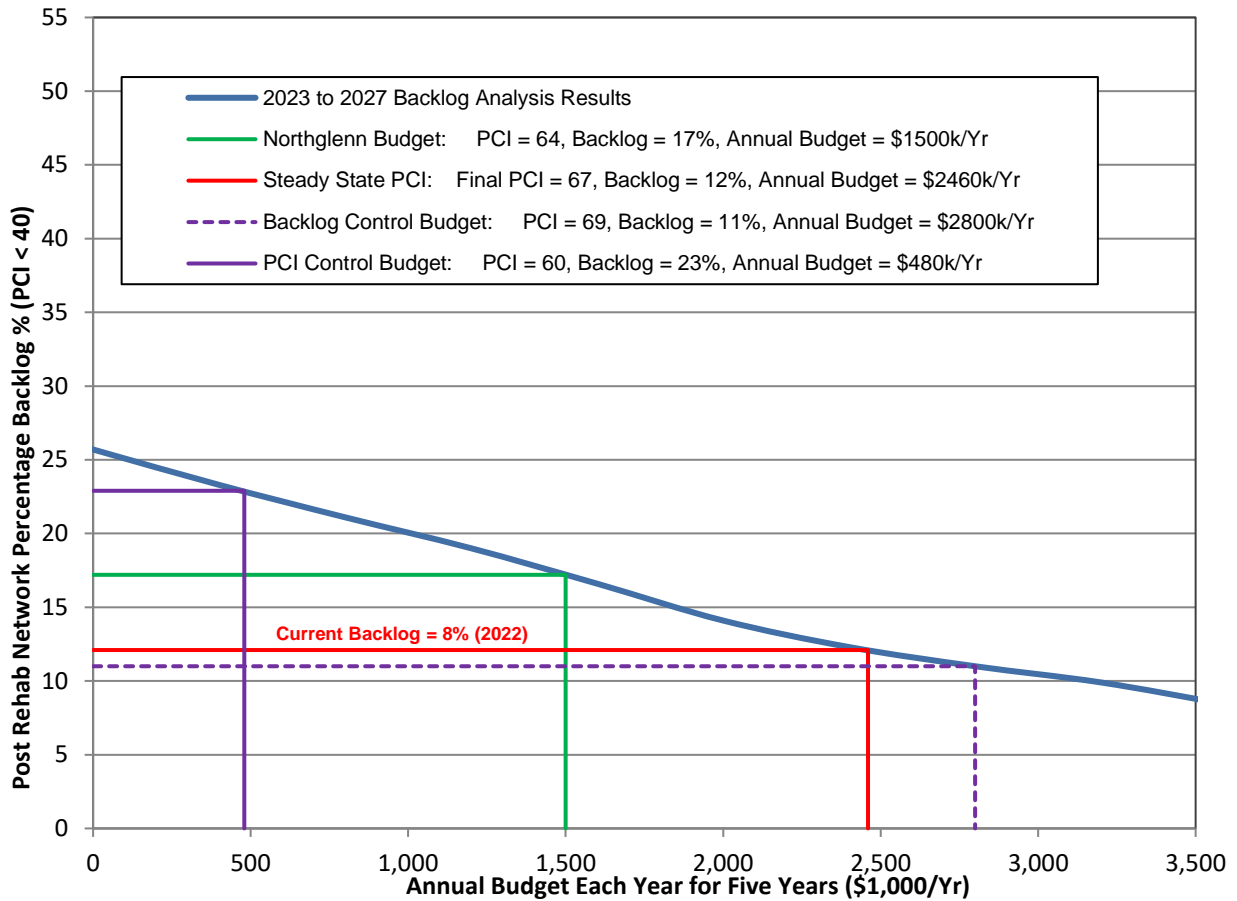


Figure 15 – 5-Year Post Rehab Network Backlog Results

Figure 16 presents the analysis results on an annual basis. This shows that if the budget falls below \$2.5M/Yr. (Steady State PCI Budget), over time the overall condition of the roads will deteriorate as the backlog continues to grow.

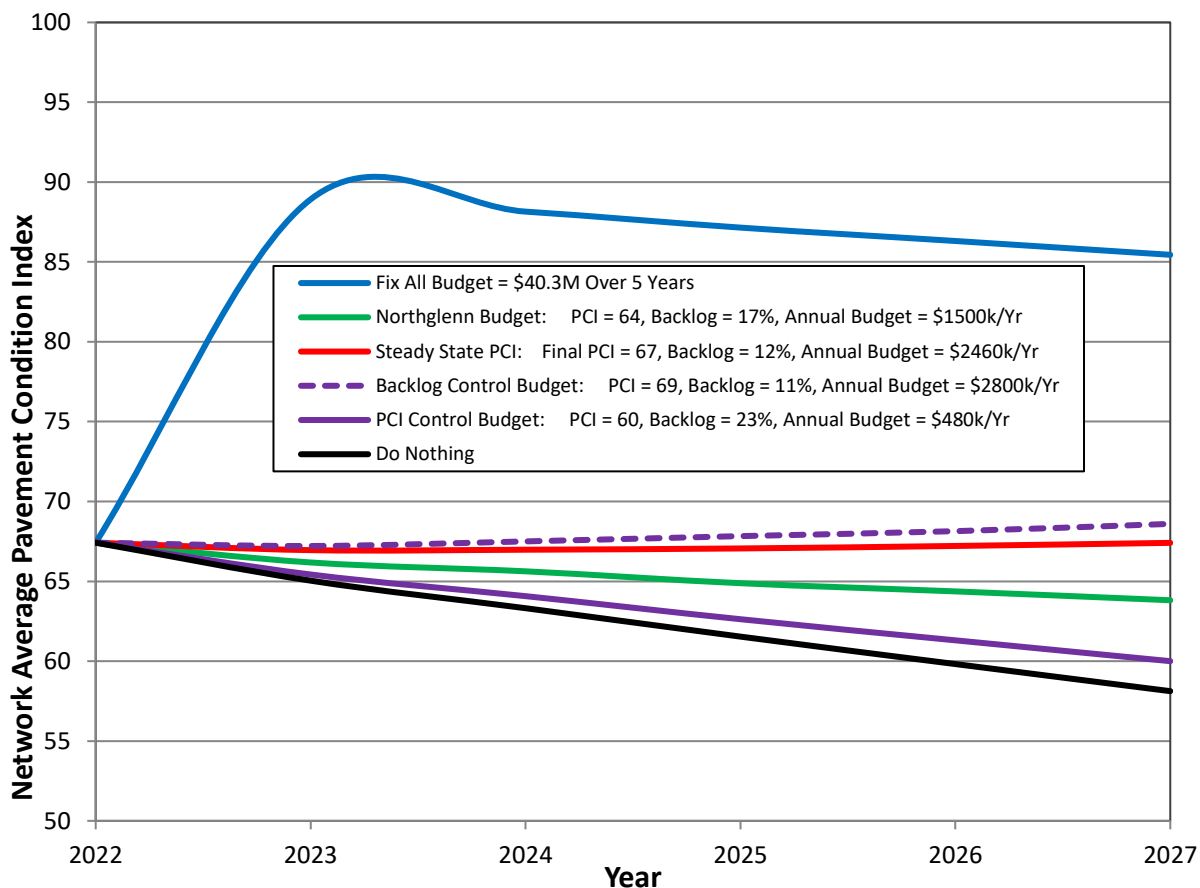
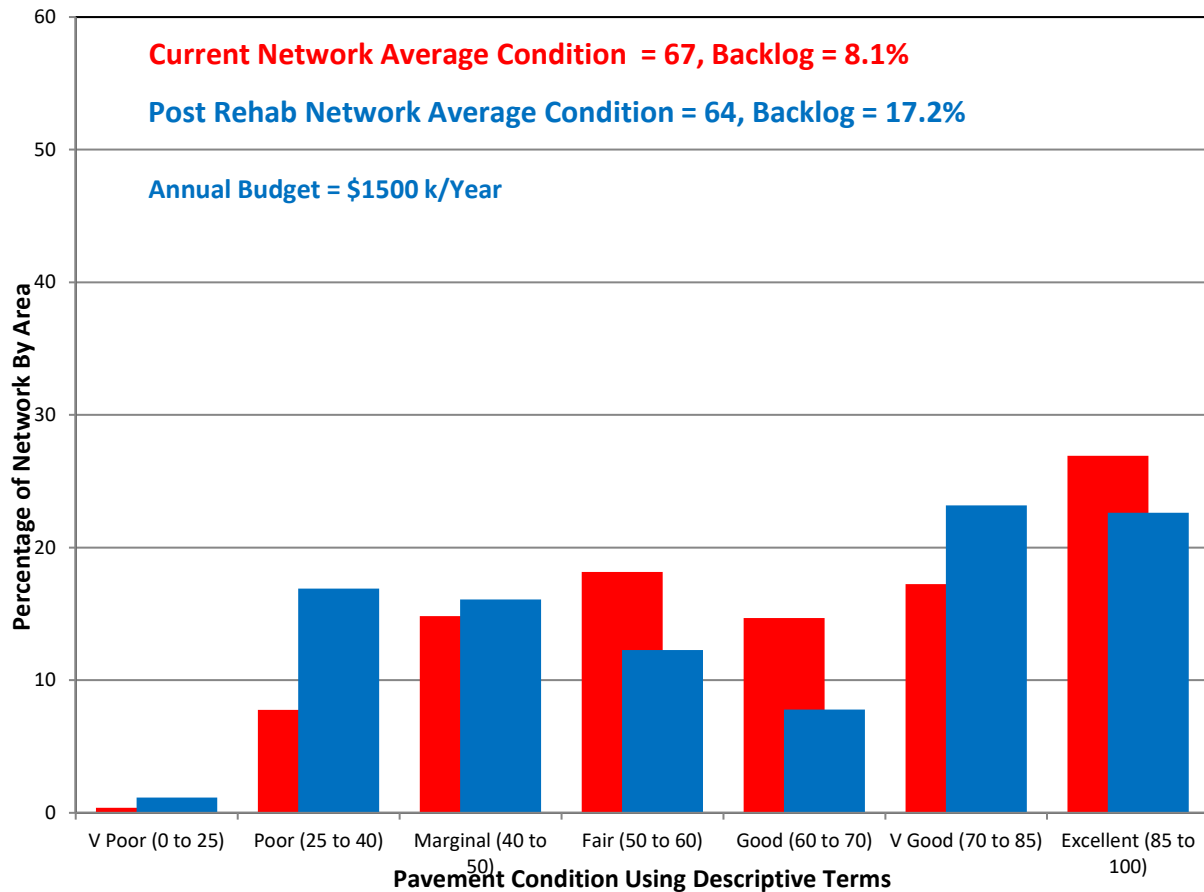


Figure 16 – 5-Year Annual PCI

#### 4.4. Post Rehabilitation Condition

The following figure (**Figure 17**) compares the current network condition distribution (red) against the projected 5-year (blue) post rehabilitation distribution at the current estimated \$1.5M/Yr. funding level. Based on current models, the average PCI is expected to decrease to 64 by the end of the five-year analysis period and the backlog to increase to approximately 17%.



**Figure 17 – PCI Distribution Comparison - Current Conditions VS 5th Year Post Rehabilitation**

#### 4.5. Summary

This section reviewed the results of the 2022 pavement analysis models that were run in the ESA program. The current City budget of \$1.5M/Yr will result in a 5-year post rehab PCI of 64 and a backlog of approximately 17%. These models demonstrate that a budget that falls below \$2.5M/Yr (steady state PCI model) will result in an overall PCI decrease over the course of 5 years.

## 5.0 PROJECT RECOMMENDATIONS AND COMMENTS

### 5.1. Project Summary and Recommendations

A pavement condition survey was performed in April 2022 on the full City pavement network. The results of the condition survey were aggregated into the ESA pavement management system. This system was used to organize a georeferenced pavement inventory, develop an accurate model of the network's condition and anticipated deterioration, and provide funding recommendations for various level-of-service goals.

The following broad recommendations are presented to the City as an output from the pavement analysis and must be read in conjunction with the previous sections.

- The City should make efforts to keep the ESA spreadsheet up to date.
  - By maintaining and updating the rehabilitation unit rates, work history of the segments, and accuracy of the inventory the City will be able to reliably forecast funding needs for future years. This allows the City to be proactive in maintaining the condition of the pavement network at an acceptable level.
- The City should periodically resurvey the pavement network.
  - Pavement performance over time involves many variables such as, traffic volumes, environmental factors, maintenance timing, and design standards. As these variables change, the rate at which a pavement deteriorates will change with them. The periodic resurvey of pavement conditions allows the City to track these changes and update models accordingly to ensure that appropriate rehabilitation measures are being planned.
- The City should investigate new and additional rehabilitation activities.
  - As technology progresses, improvements in pavement rehabilitation are constantly being made. When possible, the City should refresh the rehabilitation activities planned in ESA to ensure these advances are used to the City's advantage.
- The City should strive to maintain or better its current condition if possible.
  - Generally, it is more cost effective to maintain a condition than it is to regain ground lost to deterioration. The City currently maintains an overall network PCI of 67 with a backlog value of 8%. The current annual budget of \$1.5M will result in a PCI of 64 and a backlog of 17%.

### 5.2. Closing

The IMS Team greatly appreciates the opportunity to work with the City on this pavement management update. Over the course of this project, it has become clear that the City staff demonstrates a strong commitment to providing the highest level of service that they can to their community. IMS stands ready to assist the City with training and technical support as necessary, and we welcome the opportunity to work with the City on future pavement management projects.



**Appendix A**  
**Street Inventory and Condition Summary**

City of Northglenn, CO  
Street Inventory and Condition Summary - Sorted by Street Name



Condition Summary

GISID	On Street	From Street	To Street	FunCL	Pavement Width (ft)	Pavement Length (ft)	Add Area (yd2)	Pavement Area (yd2)	Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)
1066	100TH AVE	NIVER AVE	HURON ST	LOCAL	40	549	122	2,564	27	47	80	Strng	Poor	37	34			33
1068	100TH AVE	HURON ST	LUNCFORD CT	LOCAL	38	627	132	2,781	35	60	60	Mod	Marginal	42	23	95	9/30/22	95
1072	100TH AVE	LUNCFORD CT	CROKE DR	LOCAL	38	809	171	3,586	35	51	60	Mod	Marginal	42	22	95	9/30/22	95
1098	100TH PL	MELODY DR	BELFORD DR	LOCAL	39	408	88	1,856	63	49	80	Strng	Fair	13	24			58
1089	100TH PL	BELFORD DR	NIVER AVE	LOCAL	38	472	100	2,092	49	75	60	Mod	Fair	22	29			57
1076	100TH PL	NIVER AVE	HURON ST	LOCAL	37	752	155	3,248	56	73	60	Mod	Good	20	24			61
1082	100TH PL	HURON ST	CROKE DR	LOCAL	39	1,375	298	6,256	62	60	60	Mod	Good	17	22	95	9/30/22	95
1087	100TH PL	CROKE DR	PECOS ST	LOCAL	40	1,165	259	5,436	36	65	60	Mod	Marginal	45	20			45
1092	101ST AVE	HURON ST	CROKE DR	LOCAL	38	1,302	275	5,773	72	61	60	Mod	Good	13	15			68
1097	101ST AVE	CROKE DR	PECOS ST	LOCAL	38	1,238	261	5,488	81	54	60	Mod	V Good	15	4			72
2268	101ST PL	HURON ST	CROKE DR	LOCAL	38	1,229	259	5,448	69	62	60	Mod	Good	17	14			66
1103	101ST PL	CROKE DR	PECOS ST	LOCAL	39	1,311	284	5,965	100	66	60	Mod	Excellent	0	0			88
1105	102ND AVE	HURON ST	CROKE DR	LOCAL	40	1,156	257	5,395	27	53	60	Mod	Poor	41	29	95	9/30/22	95
1108	102ND AVE	CROKE DR	PECOS ST	LOCAL	39	1,384	300	6,296	74	60	60	Mod	Good	8	17			69
1110	102ND PL	HURON ST	CROKE DR	LOCAL	40	1,083	241	5,056	55	66	60	Mod	Fair	20	25			58
1115	102ND PL	CROKE DR	PECOS ST	LOCAL	37	1,457	300	6,290	63	63	60	Mod	Good	15	22			62
1117	103RD AVE	HURON ST	CROKE DR	LOCAL	39	1,011	219	4,598	21	65	60	Mod	Poor	40	39			35
1120	103RD AVE	CROKE DR	PECOS ST	LOCAL	38	1,529	323	6,777	22	62	60	Mod	Poor	51	21	95	9/30/22	95
1118	103RD AVE	PECOS ST	WEST END	LOCAL	39	143	31	648	45	33	60	Mod	Marginal	34	21			41
1128	103RD PL	CROKE DR	PECOS ST	LOCAL	38	1,578	333	6,995	21	65	60	Mod	Poor	53	19	95	9/30/22	95
1125	103RD PL	HURON ST	CROKE DR	LOCAL	26	943	136	2,862	48	28	60	Mod	Marginal	35	17			41
2286	104TH AVE	FREEWAY	DS@526FT	ARTERIAL	28	526	82	1,719	76	92	60	Mod	V Good	10	14			81
2046	104TH AVE	IRMA DR	DS@5020FT	ARTERIAL	40	985	219	4,597	26	60	60	Mod	Poor	44	28	95	9/30/22	95
2048	104TH AVE	IRMA DR	MARION ST	ARTERIAL	28	2,241	349	7,321	70	77	60	Mod	V Good	16	14			72
2047	104TH AVE	MARION ST	DS@492FT	ARTERIAL	38	492	104	2,183	78	78	60	Mod	V Good	9	13			78
2050	104TH AVE	DS@492FT	WASHINGTON ST	ARTERIAL	39	1,506	326	6,853	83	82	60	Mod	V Good	10	7			82
2280	104TH AVE	WASHINGTON ST	WASHINGTON ST	ARTERIAL	58	40	13	271	96	52	60	Mod	V Good	0	4			81
2049	104TH AVE	WASHINGTON ST	GRANT DR	ARTERIAL	36	1,253	251	5,261	22	72	60	Mod	Poor	42	31	95	9/30/22	95
1167	104TH AVE	GRANT DR	I-25 RAMP	ARTERIAL	45	1,269	317	6,664	32	72	60	Mod	Marginal	35	33	95	9/30/22	95
1168	104TH AVE	I-25 RAMP	W 104TH AVE	ARTERIAL	58	126	41	856	54	34	80	Strng	Marginal	17	30			46
1165	104TH AVE	W 104TH AVE	I-25 RAMP	ARTERIAL	36	168	34	704	54	56	80	Strng	Fair	18	28			54
1164	104TH AVE	E 104TH AVE	I-25	ARTERIAL	121	98	66	1,383	59	77	60	Mod	Good	15	26			65
1161	104TH AVE	I-25	DS@135FT	ARTERIAL	121	135	91	1,906	66	82	60	Mod	V Good	11	23			71
1169	104TH AVE	DS@135FT	I-25 RAMP	ARTERIAL	60	125	42	876	55	53	80	Strng	Fair	19	26			53
1170	104TH AVE	I-25 RAMP	DS@376FT	ARTERIAL	63	376	132	2,764	93	96	60	Mod	Excellent	0	7			93
1163	104TH AVE	DS@376FT	MELODY DR	ARTERIAL	50	976	271	5,696	94	84	60	Mod	Excellent	0	6			90
1159	104TH AVE	MELODY DR	HURON ST	ARTERIAL	50	956	265	5,574	95	90	60	Mod	Excellent	0	5			93
1158	104TH AVE	HURON ST	INCA ST	ARTERIAL	36	458	92	1,922	89	89	60	Mod	Excellent	7	4			89
1157	104TH AVE	INCA ST	DS@1401FT	ARTERIAL	36	1,401	280	5,884	92	100	60	Mod	Excellent	0	8			94
1153	104TH AVE	LIVINGSTON DR	QUIVAS ST	ARTERIAL	29	1,163	187	3,934	87	100	60	Mod	Excellent	5	8			91
1152	104TH AVE	QUIVAS ST	URA LN	ARTERIAL	41	1,313	299	6,282	87	97	60	Mod	Excellent	8	5			90
1150	104TH AVE	URA LN	WEST END	ARTERIAL	32	947	168	3,535	83	94	60	Mod	Excellent	7	10			86
1138	104TH AVE	WEST END	URA LN	ARTERIAL	49	491	134	2,807	89	80	60	Mod	Excellent	5	6			85

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)
1141	104TH AVE	URA LN	QUIVAS ST	ARTERIAL	38	1,313	277	5,823	93	95	60	Mod	Excellent	3	4	93		
1143	104TH AVE	QUIVAS ST	PECOS ST	ARTERIAL	34	525	99	2,082	92	84	60	Mod	Excellent	0	8	89		
1139	104TH AVE	PECOS ST	CROKE DR	ARTERIAL	28	1,575	245	5,146	83	84	60	Mod	V Good	8	9	83		
1145	104TH AVE	CROKE DR	HURON ST	ARTERIAL	36	928	186	3,899	95	82	60	Mod	Excellent	0	5	90		
1147	104TH AVE	HURON ST	MELODY DR	ARTERIAL	50	956	265	5,574	96	81	60	Mod	Excellent	0	4	91		
1148	104TH AVE	MELODY DR	BANNOCK ST	ARTERIAL	38	975	206	4,325	94	82	60	Mod	Excellent	0	6	89		
1154	104TH AVE	BANNOCK ST	I-25 RAMP	ARTERIAL	61	385	130	2,739	84	76	60	Mod	V Good	7	9	81		
1160	104TH AVE	I-25 RAMP	W 104TH AVE	ARTERIAL	62	116	40	842	57	47	80	Strng	Fair	18	26	53		
1196	104TH PL	LINCOLN CT	LINCOLN ST	LOCAL	37	251	52	1,085	55	67	60	Mod	Fair	17	28	59		
1190	104TH PL	LINCOLN ST	SHERMAN ST	LOCAL	39	261	56	1,185	52	73	60	Mod	Fair	21	28	58		
1191	104TH PL	SHERMAN ST	GRANT ST	LOCAL	35	259	50	1,059	64	73	60	Mod	Good	14	22	67		
1192	104TH PL	GRANT ST	GRANT DR	LOCAL	38	279	59	1,236	64	51	80	Strng	Fair	10	26	59		
1194	104TH PL	GRANT DR	WASHINGTON WAY	LOCAL	39	268	58	1,221	52	44	80	Strng	Marginal	21	27	49		
1195	104TH PL	WASHINGTON WAY	PENNSYLVANIA ST	LOCAL	39	249	54	1,134	54	79	60	Mod	Good	19	27	62		
2054	104TH PL	PENNSYLVANIA ST	PEARL WAY	LOCAL	38	250	53	1,107	54	73	60	Mod	Good	16	29	60		
2055	104TH PL	PEARL WAY	PEARL ST	LOCAL	37	282	58	1,217	54	61	60	Mod	Fair	19	27	56		
2056	104TH PL	MADISON WAY	SE END	LOCAL	39	395	86	1,801	66	42	80	Strng	Fair	16	18	58		
1178	104TH PL	QUIVAS ST	ROMBLON WAY	LOCAL	39	504	109	2,294	43	60	60	Mod	Marginal	30	22	48		
1177	104TH PL	ROMBLON WAY	SPERRY ST	LOCAL	39	270	58	1,227	63	75	60	Mod	Good	19	18	67		
1176	104TH PL	SPERRY ST	TANCRED ST	LOCAL	38	268	57	1,189	43	61	60	Mod	Marginal	36	21	48		
1175	104TH PL	TANCRED ST	URA LN	LOCAL	39	271	59	1,234	31	48	60	Mod	Poor	42	27	36		
1173	104TH PL	URA LN	VARESE LN	LOCAL	38	270	57	1,196	40	64	60	Mod	Marginal	36	17	47		
1172	104TH PL	VARESE LN	WYANDOT ST	LOCAL	39	270	58	1,227	46	72	60	Mod	Fair	38	17	54		
1181	104TH PL	WYANDOT ST	W 104TH PL (CDS)	LOCAL	39	248	54	1,130	46	52	60	Mod	Marginal	29	25	48		
1185	104TH PL	LIPAN ST	RONALD LN	LOCAL	37	198	41	857	53	60	60	Mod	Fair	25	22	55		
1184	104TH PL	RONALD LN	LIVINGSTON DR	LOCAL	39	292	63	1,330	59	50	80	Strng	Fair	16	24	55		
1182	104TH PL	LIVINGSTON DR	BREWER DR	LOCAL	38	295	62	1,309	64	48	80	Strng	Fair	16	20	58		
1189	104TH PL	BREWER DR	CARMELA LN	LOCAL	38	275	58	1,217	56	60	80	Strng	Fair	15	29	57		
1180	104TH PL (CDS)	SW END	W 104TH PL	LOCAL	87	37	18	377	69	25	80	Strng	Fair	8	23	54		
1204	105TH AVE	LINCOLN CT	NW END	LOCAL	72	99	39	829	71	64	60	Mod	Good	7	23	68		
2270	105TH AVE	LINCOLN CT	LINCOLN ST	LOCAL	39	239	52	1,089	66	63	60	Mod	Good	28	5	65		
2271	105TH AVE	LINCOLN ST	SHERMAN ST	LOCAL	38	260	55	1,153	97	82	60	Mod	Excellent	0	3	92		
2272	105TH AVE	SHERMAN ST	GRANT ST	LOCAL	39	260	56	1,182	88	72	60	Mod	V Good	8	4	82		
2273	105TH AVE	GRANT ST	GRANT DR	LOCAL	38	279	59	1,236	79	51	60	Mod	V Good	15	5	69		
2107	105TH CT	NW END	MADISON WAY	LOCAL	36	525	106	2,226	49	43	80	Strng	Marginal	20	31	46		
2088	105TH PL	WASHINGTON ST	CLARKSON ST	LOCAL	40	210	47	978	60	62	60	Mod	Good	22	18	60		
2093	105TH PL	CLARKSON ST	CARLILE ST	LOCAL	38	256	54	1,133	64	64	60	Mod	Good	13	23	64		
2098	105TH PL	CARLILE ST	LARSON DR	LOCAL	38	121	26	536	41	25	80	Strng	Poor	31	28	35		
2101	105TH PL	LARSON DR	EMERSON ST	LOCAL	37	145	30	625	54	65	80	Strng	Fair	12	34	57		
2100	105TH PL	EMERSON ST	OGDEN ST	LOCAL	39	262	57	1,194	55	61	60	Mod	Fair	23	22	56		
2092	105TH PL	OGDEN ST	CORONA ST	LOCAL	37	268	55	1,156	60	79	60	Mod	Good	15	25	65		
2082	105TH PL	CORONA ST	DOWNING ST	LOCAL	38	262	55	1,163	58	68	60	Mod	Good	14	28	61		
2080	105TH PL	DOWNING ST	MARION WAY	LOCAL	37	258	53	1,114	52	71	80	Strng	Fair	16	31	58		

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)				
2078	105TH PL	MARION WAY	MARION ST	LOCAL	38	266	56	1,178	54	43	80	Strng	Fair	20	27							49
2076	105TH PL	MARION ST	LAFAYETTE ST	LOCAL	37	264	54	1,141	39	51	60	Mod	Marginal	38	23							42
2072	105TH PL	LAFAYETTE ST	HUMBOLDT ST	LOCAL	39	260	56	1,181	41	61	60	Mod	Marginal	32	27							47
2069	105TH PL	HUMBOLDT ST	FRANKLIN WAY	LOCAL	38	260	55	1,152	43	57	60	Mod	Marginal	28	29							47
2073	105TH PL	FRANKLIN WAY	FRANKLIN ST	LOCAL	38	260	55	1,154	46	65	60	Mod	Fair	28	26							52
2095	105TH PL	FRANKLIN ST	GILPIN ST	LOCAL	38	272	57	1,205	42	80	60	Mod	Fair	30	28							54
2102	105TH PL	GILPIN ST	WILLIAMS ST	LOCAL	38	284	60	1,259	53	77	60	Mod	Good	19	28							60
2105	105TH PL	WILLIAMS ST	IRMA DR	LOCAL	38	509	107	2,257	55	48	60	Mod	Fair	25	19							52
2065	105TH PL	WEST END	ADAMS ST	LOCAL	41	260	59	1,245	71	40	80	Strng	Good	10	18							60
2066	105TH PL	ADAMS ST	NE END	LOCAL	42	239	56	1,172	50	43	80	Strng	Marginal	19	31							47
1238	105TH PL	URA LN	VARESE LN	LOCAL	39	267	58	1,215	35	33	80	Strng	Poor	34	30	95	9/30/22					95
1227	105TH PL	VARESE LN	WEST END	LOCAL	39	231	50	1,051	43	44	60	Mod	Marginal	32	25							42
1233	106TH AVE	LINCOLN ST	GRANT DR	LOCAL	38	1,084	229	4,804	66	56	60	Mod	Good	18	16							63
2133	106TH AVE	MILWAUKEE ST	STEELE ST	LOCAL	34	753	142	2,989	51	48	80	Strng	Fair	13	36							49
1235	106TH AVE	LIVINGSTON DR	BREWER DR	LOCAL	39	294	64	1,336	88	68	60	Mod	V Good	5	7							81
1244	106TH AVE	BREWER DR	PALEY ST	LOCAL	39	738	160	3,360	94	68	60	Mod	Excellent	0	6							85
1252	106TH AVE	PALEY ST	QUIVAS ST	LOCAL	38	411	87	1,821	88	70	60	Mod	V Good	0	12							82
1242	106TH AVE	MELODY DR	HURON ST	LOCAL	39	965	209	4,390	76	59	60	Mod	V Good	0	0							70
2110	106TH CT	WEST END	MADISON WAY	LOCAL	44	191	46	973	64	30	80	Strng	Fair	11	25							52
1264	106TH PL	LINCOLN ST	GRANT DR	LOCAL	38	902	190	4,000	74	63	60	Mod	V Good	10	16							70
2146	106TH PL	MILWAUKEE ST	MILWAUKEE CT	LOCAL	35	245	48	1,002	74	44	60	Mod	Good	13	14							63
2138	106TH PL	MILWAUKEE CT	ST PAUL CT	LOCAL	36	259	52	1,089	78	52	80	Strng	Good	2	20							69
2122	106TH PL	ST PAUL CT	STEELE ST	LOCAL	36	247	49	1,037	75	45	80	Strng	Good	7	18							65
2130	106TH PL	FOX RUN CIR	MADISON WAY	LOCAL	35	246	48	1,005	44	46	80	Strng	Marginal	24	32							44
2125	106TH PL	MADISON WAY	NE END	LOCAL	42	34	8	166	55	49	80	Strng	Fair	13	32							52
1269	107TH AVE	LINCOLN ST	GRANT DR	LOCAL	39	807	175	3,670	89	61	60	Mod	V Good	8	4							79
2147	107TH AVE	FOX RUN CIR	MADISON WAY	LOCAL	35	249	48	1,017	48	47	80	Strng	Marginal	20	33							46
2148	107TH AVE	MADISON WAY	SE END	LOCAL	42	36	8	177	66	34	80	Strng	Fair	0	34							55
2183	107TH CT	WEST END	FILLMORE CT	LOCAL	41	269	61	1,285	70	68	60	Mod	Good	8	22							69
2185	107TH CT	FILLMORE CT	FILLMORE WAY	LOCAL	35	327	64	1,335	72	58	60	Mod	Good	9	19							67
2187	107TH CT	FILLMORE WAY	FOX RUN PKWY	LOCAL	36	176	35	738	62	39	80	Strng	Fair	15	23							54
2189	107TH CT	WEST END	STEELE ST	LOCAL	46	198	50	1,053	66	33	80	Strng	Fair	5	29							54
2168	107TH CT	WEST END	MADISON WAY	LOCAL	47	131	35	725	46	29	80	Strng	Marginal	22	32							40
1293	107TH PL	LINCOLN ST	GRANT DR	LOCAL	39	727	157	3,306	65	57	60	Mod	Good	14	21							61
1308	108TH AVE	LINCOLN ST	GRANT DR	LOCAL	37	737	152	3,182	65	58	60	Mod	Good	13	22							62
2213	108TH AVE	BLUE JAY LN	PEARL CT	LOCAL	38	249	53	1,105	46	68	60	Mod	Fair	25	29							53
2214	108TH AVE	PEARL CT	PEARL ST	LOCAL	40	256	57	1,195	50	67	60	Mod	Fair	22	28							55
2215	108TH AVE	PEARL ST	WASHINGTON ST	LOCAL	39	177	38	806	48	42	80	Strng	Marginal	19	33							45
2206	108TH AVE	LARSON DR	LARRY DR	LOCAL	40	439	98	2,048	66	56	60	Mod	Good	16	18							62
2209	108TH AVE	DOWNING CT	REGINA LN	LOCAL	40	767	171	3,582	45	64	60	Mod	Fair	31	24							51
2217	108TH AVE	REGINA LN	ROSALIE DR	LOCAL	39	270	58	1,228	52	72	60	Mod	Fair	24	24							58
2220	108TH AVE	ROSALIE DR	MURRAY DR	LOCAL	39	250	54	1,137	38	52	80	Strng	Marginal	19	43							42
2231	108TH AVE	MURRAY DR	IRMA DR	LOCAL	40	408	91	1,905	46	52	80	Strng	Marginal	22	32							47

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2208	108TH AVE	FOX RUN PKWY	MILWAUKEE ST	LOCAL	32	155	28	578	60	48	80	Strng	Fair	15	25	55
2195	108TH AVE	ADAMS ST	NW END	LOCAL	42	37	9	179	70	32	80	Strng	Fair	7	23	57
2203	108TH AVE	ADAMS ST	ADAMS CT	LOCAL	34	221	42	876	65	66	60	Mod	Good	16	19	65
2202	108TH AVE	ADAMS CT	COOK ST	LOCAL	35	248	48	1,013	49	48	80	Strng	Marginal	20	32	48
2200	108TH AVE	COOK ST	COOK CT	LOCAL	35	247	48	1,007	62	49	80	Strng	Fair	13	25	57
2198	108TH AVE	COOK CT	MADISON WAY	LOCAL	33	245	45	941	54	51	80	Strng	Fair	19	27	52
2233	108TH DR	SW END	FOX RUN PKWY	LOCAL	43	243	59	1,231	58	28	80	Strng	Marginal	15	27	47
2240	108TH DR	FOX RUN PKWY	ST PAUL WAY	LOCAL	34	406	77	1,611	66	60	80	Strng	Good	9	25	63
2241	108TH DR	ST PAUL WAY	EAST END	LOCAL	43	188	45	953	76	23	80	Strng	Fair	0	24	58
2239	108TH PL	LARRY DR	ROSALIE DR	LOCAL	37	808	166	3,487	44	54	60	Mod	Marginal	30	26	46
1377	109TH AVE	NORTHGLENN DR	PIKE ST	LOCAL	39	254	55	1,157	68	73	80	Strng	V Good	3	29	69
1379	109TH AVE	PIKE ST	TEAL ST	LOCAL	37	259	53	1,120	63	75	60	Mod	Good	14	23	67
1371	109TH AVE	TEAL ST	GRANT DR	LOCAL	38	765	161	3,390	68	66	60	Mod	Good	9	23	67
1360	109TH AVE	GRANT DR	BLUE JAY LN	LOCAL	38	663	140	2,939	60	56	60	Mod	Fair	19	20	58
2254	109TH AVE	COLUMBINE ST	ELIZABETH ST	LOCAL	34	227	43	902	64	73	60	Mod	Good	10	26	67
2251	109TH AVE	ELIZABETH ST	CLAYTON ST	LOCAL	34	251	47	994	62	63	80	Strng	Good	9	30	62
2258	109TH AVE	CLAYTON ST	FOX RUN PKWY	LOCAL	35	581	113	2,372	61	63	80	Strng	Good	13	26	61
1659	109TH AVE	FOX RUN PKWY	N STEELE ST	LOCAL	35	1,077	209	4,397	79	56	80	Strng	V Good	0	22	70
1632	109TH CT	CLAYTON ST	EAST END	LOCAL	44	155	38	790	76	27	80	Strng	Fair	9	15	59
1631	109TH DR	GAYLORD ST	YORK ST	LOCAL	36	433	87	1,818	65	78	60	Mod	Good	9	27	69
1629	109TH DR	YORK ST	JOSEPHINE ST	LOCAL	34	241	45	954	69	68	80	Strng	Good	0	31	68
1630	109TH DR	JOSEPHINE ST	COLUMBINE ST	LOCAL	35	241	47	982	59	65	60	Mod	Good	17	24	60
1383	109TH PL	NORTHGLENN DR	GRANT DR	LOCAL	38	1,401	296	6,210	62	65	60	Mod	Good	19	19	62
2263	109TH PL	ALVIN DR	MURIEL DR	LOCAL	39	1,424	308	6,478	94	68	60	Mod	Excellent	3	3	85
1644	109TH PL	GAYLORD ST	CLAUDE CT	LOCAL	34	264	50	1,047	57	53	80	Strng	Fair	10	33	55
1639	110TH AVE	E 110TH AVE EXT	CLAYTON ST	LOCAL	35	227	44	929	74	58	80	Strng	Good	5	21	68
1645	110TH AVE	CLAYTON ST	FOX RUN PKWY	LOCAL	35	242	47	986	61	23	80	Strng	Marginal	13	26	48
1654	110TH AVE	FOX RUN PKWY	CLAYTON ST	LOCAL	32	157	28	586	72	23	80	Strng	Fair	8	20	55
1684	110TH CT	WEST END	CLAYTON ST	LOCAL	46	164	42	882	71	38	80	Strng	Fair	5	24	59
1660	110TH DR	YORK ST	JOSEPHINE ST	LOCAL	33	230	42	886	52	75	60	Mod	Fair	16	32	59
1662	110TH DR	JOSEPHINE ST	COLUMBINE ST	LOCAL	33	243	45	936	49	34	80	Strng	Marginal	17	34	44
1669	110TH DR	COLUMBINE ST	FOX RUN PKWY	LOCAL	35	296	58	1,208	48	58	80	Strng	Fair	18	34	51
1655	110TH DR	E 110TH AVE	E 110TH DR EXT	LOCAL	33	125	23	483	75	40	80	Strng	Good	3	22	63
1682	110TH DR	E 110TH DR EXT	N STEELE ST	LOCAL	35	1,088	212	4,442	74	59	80	Strng	Good	4	22	69
1649	110TH DR EXT	SOUTH END	E 110TH DR	LOCAL	42	10	2	51	78	30	80	Strng	Good	0	22	61
1691	110TH PL	LARSON DR	LUCILLE CT	LOCAL	38	318	67	1,410	90	74	60	Mod	Excellent	0	10	85
1689	110TH PL	LUCILLE CT	ELMER DR	LOCAL	39	434	94	1,974	100	84	60	Mod	Excellent	0	0	94
1681	110TH PL	ELMER DR	MURRAY DR	LOCAL	39	297	64	1,352	94	76	60	Mod	Excellent	0	6	88
1678	110TH PL	MURRAY DR	IRMA DR	LOCAL	38	410	87	1,818	97	73	60	Mod	Excellent	0	3	89
1686	110TH PL	YORK WAY	110TH PL\JOSEPHINE WY E)	LOCAL	36	453	91	1,901	71	65	80	Strng	Good	0	29	69
1433	111TH AVE	LIVINGSTON DR	W 111TH AVE (CDS)	LOCAL	35	682	133	2,786	63	55	80	Strng	Good	9	28	60
1431	111TH AVE	W 111TH AVE (CDS)	NAVAJO ST	LOCAL	35	343	67	1,399	56	68	80	Strng	Fair	11	33	59
1426	111TH AVE	NAVAJO ST	OSAGE ST	LOCAL	35	252	49	1,028	60	68	80	Strng	Good	9	31	62

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	
1424	111TH AVE	OSAGE ST	PARK VISTAS DR	LOCAL	36	266	53	1,118	60	58	80	Strng	Fair	14	26	59
1434	111TH AVE	SE END	LIVINGSTON DR	LOCAL	73	33	14	284	61	12	80	Strng	Marginal	15	24	44
1432	111TH AVE (CDS)	NW END	W 111TH AVE	LOCAL	48	96	26	540	46	31	80	Strng	Marginal	29	24	40
1740	111TH DR	WEST END	GAYLORD ST	LOCAL	39	284	62	1,310	57	55	80	Strng	Fair	11	32	56
1742	111TH DR	GAYLORD ST	YORK ST	LOCAL	32	142	25	531	56	53	80	Strng	Fair	12	32	54
1743	111TH DR	YORK ST	YORK WAY	LOCAL	33	103	19	398	60	57	80	Strng	Fair	8	32	58
1745	111TH DR	YORK WAY	JOSEPHINE WAY	LOCAL	32	151	27	565	73	46	80	Strng	Good	4	23	63
1715	111TH DR	JOSEPHINE WAY	FOX RUN PKWY	LOCAL	39	170	37	775	56	32	80	Strng	Marginal	16	28	47
1723	111TH DR	FOX RUN PKWY	COLUMBINE CT	LOCAL	32	160	29	599	53	47	80	Strng	Fair	18	30	50
1730	111TH DR	COLUMBINE CT	CLAYTON ST	LOCAL	35	448	87	1,828	49	59	60	Mod	Fair	26	25	52
1732	111TH DR	CLAYTON ST	DETROIT WAY	LOCAL	34	250	47	994	49	56	60	Mod	Fair	25	27	50
1734	111TH DR	DETROIT WAY	FILLMORE WAY	LOCAL	34	207	39	823	57	59	80	Strng	Fair	12	30	57
1455	111TH PL	NORTHGLENN DR	GRANT DR	LOCAL	38	1,195	252	5,299	64	54	60	Mod	Good	17	19	60
1752	111TH PL	GRANT DR	PEARL ST	LOCAL	38	714	151	3,166	72	56	80	Strng	Good	8	19	66
1737	111TH PL	EMERSON ST	LARSON DR	LOCAL	38	990	209	4,390	57	61	60	Mod	Fair	17	27	57
1736	111TH PL	LARSON DR	E 111TH PL (CDS)	LOCAL	38	958	202	4,245	59	63	60	Mod	Good	21	20	60
1722	111TH PL	E 111TH PL (CDS)	MURRAY DR	LOCAL	36	58	12	242	65	50	80	Strng	Good	14	20	60
1447	111TH PL	LIVINGSTON DR	NAVAJO ST	LOCAL	35	983	191	4,013	41	46	60	Mod	Marginal	33	26	42
1445	111TH PL	NAVAJO ST	OSAGE ST	LOCAL	35	251	49	1,023	55	75	60	Mod	Good	17	28	61
1444	111TH PL	OSAGE ST	PARK VISTAS DR	LOCAL	33	247	45	950	66	61	60	Mod	Good	12	22	64
1727	111TH PL (CDS)	E 111TH PL	NE END	LOCAL	69	107	41	865	62	25	80	Strng	Marginal	21	17	49
1457	111TH WAY	W 112TH AVE	PARK VISTAS DR	LOCAL	35	531	103	2,166	54	54	80	Strng	Fair	20	25	54
1767	112TH AVE	WASHINGTON ST	LARSON DR	ARTERIAL	40	1,725	383	8,048	84	82	60	Mod	V Good	8	8	83
1765	112TH AVE	LARSON DR	IRMA DR	ARTERIAL	40	1,425	317	6,651	78	85	60	Mod	V Good	14	8	80
1763	112TH AVE	IRMA DR	YORK ST	ARTERIAL	40	2,101	467	9,802	78	65	60	Mod	V Good	14	7	73
1760	112TH AVE	YORK ST	FOX RUN PKWY	ARTERIAL	62	585	201	4,230	81	93	60	Mod	Excellent	12	8	84
1468	112TH AVE	COMMUNITY CENTER DR	PINNACLE CREEK	ARTERIAL	77	654	280	5,874	54	73	60	Mod	Good	27	19	59
1467	112TH AVE	PINNACLE CREEK	LIVINGSTON DR	ARTERIAL	64	345	123	2,574	26	59	60	Mod	Poor	51	20	36
1458	112TH AVE	LIVINGSTON DR	W 111TH WAY	ARTERIAL	62	1,638	564	11,851	34	65	60	Mod	Marginal	44	22	43
2266	112TH AVE	W 111TH WAY	WEST END	ARTERIAL	70	2,707	1,053	22,109	46	65	60	Mod	Fair	36	19	51
1466	112TH AVE	CHEROKEE ST	ELATI ST	LOCAL	25	344	48	1,002	71	58	60	Mod	Good	11	18	66
1465	112TH AVE	ELATI ST	MELODY DR	LOCAL	26	366	53	1,111	51	62	60	Mod	Fair	26	23	54
1478	112TH CIR	E 112TH PL	NORTH END	LOCAL	46	149	38	806	45	34	80	Strng	Marginal	27	28	41
1480	112TH DR	E 112TH PL	E 112TH PL	LOCAL	33	1,285	236	4,947	58	55	80	Strng	Fair	18	24	56
1477	112TH PL	HIGHLINE DR	E 112TH DR	LOCAL	36	245	49	1,028	55	46	80	Strng	Fair	20	25	51
1476	112TH PL	E 112TH DR	E 112TH CIR	LOCAL	35	295	57	1,204	55	60	60	Mod	Fair	25	20	56
1475	112TH PL	E 112TH CIR	E 112TH DR	LOCAL	35	291	56	1,186	65	68	60	Mod	Good	18	17	65
1474	112TH PL	E 112TH DR	GRANT DR	LOCAL	35	315	61	1,287	44	41	80	Strng	Marginal	30	26	42
1782	112TH PL	SW END	CLARKSON ST	LOCAL	42	35	8	171	83	43	80	Strng	Good	0	17	69
1783	112TH PL	CLARKSON ST	OGDEN DR	LOCAL	37	643	132	2,775	67	67	60	Mod	Good	9	24	66
1779	112TH PL	OGDEN DR	CORONA DR	LOCAL	40	261	58	1,217	66	57	60	Mod	Good	13	21	63
1778	112TH PL	CORONA DR	DOWNING DR	LOCAL	38	248	52	1,101	75	81	60	Mod	V Good	8	17	77
1777	112TH PL	DOWNING DR	LARSON LN	LOCAL	39	251	54	1,141	80	68	60	Mod	V Good	0	20	75

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1776	112TH PL	LARSON LN	LARSON DR	LOCAL	39	107	23	485	76	65	60	Mod	V Good	10	14	72
1775	112TH PL	LARSON DR	MARION ST	LOCAL	38	151	32	671	79	61	60	Mod	V Good	7	15	72
1773	112TH PL	MARION ST	LAFAYETTE ST	LOCAL	37	250	51	1,079	74	84	60	Mod	V Good	11	15	77
1772	112TH PL	LAFAYETTE ST	HUMBOLDT ST	LOCAL	38	261	55	1,157	66	80	60	Mod	V Good	13	22	70
1771	112TH PL	HUMBOLDT ST	FRANKLIN ST	LOCAL	38	249	53	1,104	69	74	60	Mod	V Good	9	21	70
1770	112TH PL	FRANKLIN ST	FOWLER DR	LOCAL	38	260	55	1,150	67	82	60	Mod	V Good	8	24	72
1769	112TH PL	FOWLER DR	IRMA DR	LOCAL	37	254	52	1,095	76	65	60	Mod	V Good	3	21	72
1780	112TH PL	IRMA DR	CLAUDE CT	LOCAL	38	1,329	281	5,892	73	79	60	Mod	V Good	8	19	74
1781	112TH PL	CLAUDE CT	SE END	LOCAL	79	59	26	541	37	37	80	Strng	Poor	35	28	36
5006	112TH PL PUBLIC PARKING	0	0	PARKING LOT	27	465	69	1,447	89	39	60	Mod	V Good	3	9	72
1794	113TH AVE	E 113TH PL	HIGH ST	LOCAL	37	445	92	1,923	73	52	60	Mod	Good	12	15	65
1795	113TH AVE	E 113TH PL	CLAUDE CT	LOCAL	38	536	113	2,374	72	55	80	Strng	Good	6	22	66
1485	113TH PL	WEST END	LINCOLN DR	LOCAL	38	345	72	1,517	24	38	60	Mod	Poor	49	27	28
1486	113TH PL	LINCOLN DR	SHERMAN DR	LOCAL	35	268	52	1,093	25	52	60	Mod	Poor	56	19	33
1487	113TH PL	SHERMAN DR	HIGHLINE DR	LOCAL	35	211	41	863	34	37	60	Mod	Poor	42	25	34
1797	113TH PL	E 113TH AVE	E 113TH PL (CDS)	LOCAL	36	159	32	667	76	46	60	Mod	Good	9	15	65
1809	113TH PL	E 113TH PL (CDS)	HIGH ST	LOCAL	38	365	77	1,619	79	43	60	Mod	Good	11	10	67
1796	113TH PL	E 113TH AVE	E 113TH PL (CDS)	LOCAL	39	112	24	511	76	58	80	Strng	V Good	2	22	70
1803	113TH PL	E 113TH PL (CDS)	CLAUDE CT	LOCAL	37	521	107	2,250	72	55	60	Mod	Good	10	18	66
1798	113TH PL (CDS)	E 113TH PL	NW END	LOCAL	42	37	9	179	87	42	80	Strng	V Good	0	13	71
1799	113TH PL (CDS)	E 113TH PL	NW END	LOCAL	42	42	10	207	73	35	80	Strng	Good	11	15	60
1837	114TH AVE	SW END	CLARKSON ST	LOCAL	42	35	8	170	65	27	80	Strng	Fair	9	26	52
1829	114TH AVE	E 114TH PL	HIGH ST	LOCAL	39	469	102	2,133	57	52	60	Mod	Fair	21	22	95
1830	114TH AVE	E 114TH PL	CLAUDE CT	LOCAL	38	726	153	3,218	57	60	60	Mod	Fair	19	24	57
1492	114TH AVE	CHEROKEE ST	MELODY DR	LOCAL	42	621	145	3,044	47	59	80	Strng	Fair	24	29	50
1490	114TH AVE	MELODY DR	MELODY DR	LOCAL	41	197	45	942	45	56	60	Mod	Marginal	27	28	48
1489	114TH AVE	MELODY DR	HURON ST	LOCAL	41	693	158	3,313	34	58	60	Mod	Marginal	33	33	41
1861	114TH CT	WEST END	PEARL ST	LOCAL	25	399	55	1,165	31	38	60	Mod	Poor	44	25	33
1855	114TH CT	PEARL ST	WASHINGTON ST	LOCAL	35	355	69	1,448	26	51	60	Mod	Poor	41	34	95
1838	114TH PL	E 114TH AVE	CARLILE ST	LOCAL	40	316	70	1,473	43	60	80	Strng	Marginal	24	33	48
1843	114TH PL	CARLILE ST	EMERSON ST	LOCAL	39	335	73	1,523	43	73	60	Mod	Fair	33	24	52
1846	114TH PL	EMERSON ST	LARSON LN	LOCAL	38	261	55	1,156	38	62	60	Mod	Marginal	34	28	45
1815	114TH PL	FRANKLIN ST	FOWLER DR	LOCAL	37	259	53	1,118	44	54	60	Mod	Marginal	36	20	47
1818	114TH PL	FOWLER DR	IRMA DR	LOCAL	39	256	55	1,164	79	48	60	Mod	Good	9	12	68
1840	114TH PL	E 114TH AVE	HIGH ST	LOCAL	39	521	113	2,371	42	55	80	Strng	Marginal	24	34	95
1831	114TH PL	E 114TH AVE	E 114TH PL (CDS)	LOCAL	37	127	26	549	62	56	80	Strng	Fair	13	26	59
1834	114TH PL	E 114TH PL (CDS)	CLAUDE CT	LOCAL	39	716	155	3,260	39	61	80	Strng	Marginal	28	34	46
1510	114TH PL	CHEROKEE ST	W 114TH WAY	LOCAL	35	172	33	702	40	32	80	Strng	Poor	26	33	95
1506	114TH PL	W 114TH WAY	MELODY DR	LOCAL	35	692	135	2,827	38	42	80	Strng	Poor	32	30	39
1832	114TH PL (CDS)	E 114TH PL	NW END	LOCAL	42	43	10	209	47	37	80	Strng	Marginal	25	28	44
1517	114TH WAY	W 114TH PL	MELODY DR	LOCAL	35	946	184	3,861	55	50	80	Strng	Fair	21	25	53
1880	115TH AVE	PEARL ST	WASHINGTON ST	LOCAL	35	406	79	1,657	77	62	60	Mod	V Good	13	10	72
1868	115TH AVE	CLARKSON ST	CARLILE ST	LOCAL	38	216	45	955	100	75	60	Mod	Excellent	0	0	91

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1870	115TH AVE	CARLILE ST	EMERSON ST	LOCAL	36	261	52	1,095	98	83	60	Mod	Excellent	2	1							92
1874	115TH AVE	EMERSON ST	LARSON LN	LOCAL	38	251	53	1,113	100	48	60	Mod	V Good	0	0							83
1844	115TH AVE	IRMA DR	GILPIN ST	LOCAL	39	295	64	1,343	81	60	60	Mod	V Good	5	14							73
1850	115TH AVE	GILPIN ST	HIGH ST	LOCAL	38	422	89	1,871	75	86	60	Mod	V Good	8	18							78
1857	115TH AVE	HIGH ST	WYCO DR	LOCAL	40	202	45	943	78	79	60	Mod	V Good	13	10							78
1860	115TH AVE	WYCO DR	FISHER WAY	LOCAL	37	145	30	627	74	63	60	Mod	V Good	15	11							70
1862	115TH AVE	FISHER WAY	CLAUDE CT	LOCAL	38	918	194	4,071	86	73	60	Mod	V Good	8	6							82
1521	115TH AVE	CHEROKEE ST	MELODY DR	LOCAL	40	817	181	3,811	40	53	60	Mod	Marginal	35	25							44
1519	115TH AVE	MELODY DR	HURON ST	LOCAL	41	492	112	2,353	36	44	60	Mod	Poor	48	16							38
1883	115TH PL	WEST END	LARSON LN	LOCAL	45	478	119	2,496	100	60	60	Mod	Excellent	0	0							86
1873	115TH PL	WYCO DR	CLAUDE CT	LOCAL	37	1,163	239	5,019	40	60	60	Mod	Marginal	33	26							46
1529	116TH AVE	LINCOLN ST	GRANT ST	LOCAL	29	499	80	1,687	55	56	80	Strng	Fair	18	27							54
1890	116TH AVE	LOGAN ST	PEARL ST	LOCAL	30	500	83	1,750	66	45	80	Strng	Fair	9	25							59
1878	116TH AVE	WYCO DR	CLAUDE CT	LOCAL	39	1,261	273	5,738	61	60	80	Strng	Good	12	27							60
1527	116TH AVE	ACOMA ST	MELODY DR	LOCAL	34	905	171	3,591	50	67	60	Mod	Fair	27	23							55
1526	116TH AVE	MELODY DR	GALAPAGO CT	LOCAL	34	305	58	1,210	56	58	60	Mod	Fair	21	23							56
1525	116TH AVE	GALAPAGO CT	HURON ST	LOCAL	35	188	37	769	60	36	80	Strng	Fair	22	18							51
1528	116TH AVE	ACOMA ST	SE END	LOCAL	72	41	16	346	100	39	60	Mod	V Good	0	0							79
1904	116TH DR	E 116TH PL	E 116TH PL	LOCAL	37	922	190	3,981	38	64	60	Mod	Marginal	30	32	95	9/30/22					95
1903	116TH DR	E 116TH PL	CLAUDE CT	LOCAL	39	278	60	1,264	38	49	80	Strng	Marginal	29	33	95	9/30/22					95
1887	116TH PL	WYCO DR	E 116TH DR	LOCAL	39	384	83	1,748	60	56	60	Mod	Fair	18	22							58
1902	116TH PL	E 116TH DR	E 116TH DR	LOCAL	40	922	205	4,302	57	62	60	Mod	Fair	18	25							58
1540	116TH PL	EAST END	W 116TH WAY	LOCAL	46	308	79	1,665	75	71	60	Mod	V Good	9	16							73
1539	116TH PL	W 116TH WAY	MELODY DR	LOCAL	42	312	73	1,526	77	83	60	Mod	V Good	8	15							79
1537	116TH PL	MELODY DR	DS@260FT	LOCAL	42	260	61	1,273	68	53	60	Mod	Good	16	16							62
1541	116TH PL	DS@260FT	HURON ST	LOCAL	22	231	28	592	59	32	80	Strng	Marginal	25	17							49
1534	116TH PL	HURON ST	GALAPAGO CT	LOCAL	22	190	23	487	71	35	80	Strng	Fair	12	16							59
1552	116TH WAY	ACOMA ST	W 116TH WY	LOCAL	34	567	107	2,248	60	38	80	Strng	Fair	22	18	95	9/30/22					95
1550	116TH WAY	W 116TH WY	W 116TH PL	LOCAL	35	273	53	1,115	62	58	60	Mod	Good	17	20							60
1553	116TH WY	NW END	W 116TH WAY	LOCAL	69	33	13	264	49	32	80	Strng	Marginal	0	51							43
1542	117TH AVE	WEST END	LINCOLN ST	LOCAL	40	123	27	575	91	79	60	Mod	Excellent	0	9							87
1544	117TH AVE	LINCOLN ST	SHERMAN ST	LOCAL	38	251	53	1,112	90	82	60	Mod	Excellent	0	10							87
1546	117TH AVE	SHERMAN ST	GRANT ST	LOCAL	37	251	52	1,083	91	78	60	Mod	Excellent	3	5							87
1548	117TH AVE	GRANT ST	LOGAN ST	LOCAL	39	248	54	1,130	94	80	60	Mod	Excellent	0	6							89
1919	117TH AVE	LOGAN ST	PENNSYLVANIA ST	LOCAL	38	251	53	1,113	86	82	60	Mod	Excellent	8	6							85
1921	117TH AVE	PENNSYLVANIA ST	PEARL ST	LOCAL	38	252	53	1,117	91	70	60	Mod	V Good	0	9							83
1917	117TH AVE	PEARL ST	WASHINGTON ST	LOCAL	21	166	19	408	89	41	60	Mod	V Good	3	9							72
1924	117TH AVE	WASHINGTON ST	PEARL ST	LOCAL	22	167	20	429	84	52	60	Mod	V Good	3	13							73
1561	117TH AVE	ACOMA ST	DELAWARE CT	LOCAL	33	301	55	1,160	50	54	60	Mod	Fair	25	26							50
1560	117TH AVE	DELAWARE CT	ELATI CT	LOCAL	33	295	54	1,136	45	60	60	Mod	Marginal	29	26							49
1559	117TH AVE	ELATI CT	MELODY DR	LOCAL	34	286	54	1,136	34	48	60	Mod	Poor	44	21							38
1557	117TH AVE	MELODY DR	GALAPAGO CT	LOCAL	33	306	56	1,177	42	52	60	Mod	Marginal	36	21							45
1556	117TH AVE	GALAPAGO CT	WEST END	LOCAL	32	120	21	446	54	45	80	Strng	Fair	20	26							50



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1926	117TH CT	WYCO DR	E 117TH WAY	LOCAL	36	612	122	2,569	61	53	60	Mod	Fair	19	21						57
1555	117TH PL	LINCOLN ST	SHERMAN ST	LOCAL	34	262	49	1,039	54	60	60	Mod	Fair	27	19						56
1918	117TH WAY	WYCO DR	E 117TH CT	LOCAL	38	563	119	2,494	86	54	60	Mod	V Good	3	11						75
1931	117TH WAY	E 117TH CT	PHILLIPS DR	LOCAL	38	290	61	1,287	84	57	60	Mod	V Good	8	9						74
1570	118TH AVE	LINCOLN ST	SHERMAN ST	LOCAL	33	224	41	861	31	54	60	Mod	Poor	46	23	95	9/30/22				95
1572	118TH AVE	SHERMAN ST	GRANT ST	LOCAL	34	223	42	886	32	66	60	Mod	Marginal	43	25	95	9/30/22				95
1566	118TH AVE	GRANT ST	LOGAN ST	LOCAL	33	231	42	890	37	46	60	Mod	Marginal	35	28	95	9/30/22				95
1569	118TH AVE	LOGAN ST	PENNSYLVANIA ST	LOCAL	32	219	39	819	33	57	60	Mod	Marginal	42	24	95	9/30/22				95
1954	118TH AVE	PENNSYLVANIA ST	DS@307FT	LOCAL	23	307	39	825	24	42	80	Strng	Poor	36	23	95	9/30/22				95
1955	118TH AVE	DS@307FT	WASHINGTON ST	LOCAL	23	226	29	606	28	41	80	Strng	Poor	39	24	95	9/30/22				95
1958	118TH AVE	WASHINGTON ST	DS@226FT	LOCAL	23	226	29	605	17	40	80	Strng	V Poor	30	30						24
1957	118TH AVE	DS@226FT	DS@533FT	LOCAL	24	308	41	862	21	62	60	Mod	Poor	46	32						34
1576	118TH AVE	DS@533FT	DS@752FT	LOCAL	24	219	29	613	41	67	60	Mod	Marginal	33	26						49
1575	118TH AVE	DS@752FT	GRANT ST	LOCAL	22	231	28	593	48	48	80	Strng	Marginal	25	20						47
1587	118TH AVE	ACOMA ST	DELAWARE CT	LOCAL	36	258	52	1,082	33	52	60	Mod	Poor	45	22	95	9/30/22				95
1585	118TH AVE	DELAWARE CT	ELATI CT	LOCAL	38	374	79	1,658	25	49	60	Mod	Poor	50	24	95	9/30/22				95
1582	118TH AVE	ELATI CT	MELODY DR	LOCAL	39	296	64	1,346	26	59	60	Mod	Poor	53	21	95	9/30/22				95
1580	118TH AVE	MELODY DR	GALAPAGO CT	LOCAL	39	296	64	1,346	26	56	60	Mod	Poor	52	22	95	9/30/22				95
1578	118TH AVE	GALAPAGO CT	HURON ST	LOCAL	38	189	40	838	23	37	60	Mod	Poor	55	22						27
1593	118TH PL	LINCOLN ST	SHERMAN ST	LOCAL	33	217	40	835	50	62	60	Mod	Fair	24	25						54
1595	118TH PL	SHERMAN ST	GRANT ST	LOCAL	34	222	42	881	52	49	60	Mod	Fair	27	22						50
1995	119TH PL	SYLVIA DR	MCCRUMB DR	LOCAL	37	261	54	1,124	60	72	60	Mod	Good	21	19						64
1997	119TH PL	MCCRUMB DR	QUAM DR	LOCAL	40	261	58	1,217	71	91	60	Mod	V Good	3	26						77
1999	119TH PL	QUAM DR	SPRING DR	LOCAL	39	259	56	1,178	71	85	60	Mod	V Good	4	25						75
2001	119TH PL	SPRING DR	KEOUGH DR	LOCAL	38	261	55	1,156	69	73	60	Mod	V Good	10	21						70
2004	119TH PL	KEOUGH DR	LAFAYETTE ST	LOCAL	38	259	55	1,147	71	79	60	Mod	V Good	3	26						73
2005	119TH PL	LAFAYETTE ST	HUMBOLDT DR	LOCAL	38	259	55	1,149	74	82	60	Mod	V Good	4	22						76
2007	119TH PL	HUMBOLDT DR	IRMA DR	LOCAL	38	265	56	1,174	66	50	80	Strng	Good	12	22						60
1988	119TH PL	IRMA DR	WILLIAMS WAY	LOCAL	38	339	71	1,501	64	68	60	Mod	Good	15	22						65
1990	119TH PL	WILLIAMS WAY	HIGH ST	LOCAL	40	260	58	1,211	69	81	60	Mod	V Good	7	24						72
2009	119TH PL	HIGH ST	LAVINIA WAY	LOCAL	39	285	62	1,295	70	83	60	Mod	V Good	14	16						74
2011	119TH PL	LAVINIA WAY	LAVINIA LN	LOCAL	38	262	55	1,163	74	92	60	Mod	V Good	6	20						79
2013	119TH PL	LAVINIA LN	GAYLORD WAY	LOCAL	38	260	55	1,152	68	77	60	Mod	V Good	12	19						71
2015	119TH PL	GAYLORD WAY	CLAUDE WAY	LOCAL	37	258	53	1,114	72	75	60	Mod	V Good	3	24						73
2017	119TH PL	CLAUDE WAY	UNNAMED	LOCAL	38	188	40	834	71	79	60	Mod	V Good	13	16						73
1993	119TH PL	CLAUDE CT	UNNAMED	LOCAL	38	716	151	3,174	71	63	60	Mod	Good	9	20						67
1619	120TH AVE	I-25 RAMP	DS@583FT	ARTERIAL	46	583	149	3,129	73	72	60	Mod	V Good	11	16						72
1620	120TH AVE	DS@583FT	GRANT ST	ARTERIAL	48	396	106	2,216	43	62	60	Mod	Marginal	38	19						48
1622	120TH AVE	GRANT ST	WASHINGTON ST	ARTERIAL	42	2,015	470	9,876	41	68	60	Mod	Fair	36	22						49
2279	120TH AVE	WASHINGTON ST	WASHINGTON ST	ARTERIAL	58	45	15	305	27	49	60	Mod	Poor	44	29						33
2023	120TH AVE	WASHINGTON ST	SYLVIA DR	ARTERIAL	40	940	209	4,386	37	67	80	Strng	Marginal	27	33						46
2025	120TH AVE	SYLVIA DR	IRMA DR	ARTERIAL	28	1,825	284	5,963	65	72	60	Mod	Good	23	9						66
2026	120TH AVE	IRMA DR	CLAUDE CT	ARTERIAL	29	2,160	348	7,309	74	76	60	Mod	V Good	13	12						74

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)			
2020	120TH AVE	CLAUDE CT	EAST END	ARTERIAL	30	333	56	1,167	60	70	60	Mod	Good	25	15						62
2029	120TH AVE	IRMA DR	RACE ST	ARTERIAL	28	1,176	183	3,843	75	80	60	Mod	V Good	13	12						76
2030	120TH AVE	RACE ST	CLAUDE CT	ARTERIAL	38	984	208	4,363	57	88	60	Mod	Good	24	19						67
2028	120TH AVE	CLAUDE CT	EAST END	ARTERIAL	49	332	90	1,896	31	79	30	Weak	Marginal	51	18						46
2285	121ST AVE	IRMA DR	RACE ST	LOCAL	42	1,211	283	5,936	99	72	60	Mod	Excellent	1	0						89
2284	122ND AVE	IRMA DR	RACE ST	LOCAL	42	1,286	300	6,301	90	66	60	Mod	V Good	9	1						82
2283	123RD AVE	IRMA DR	RACE ST	LOCAL	42	1,268	296	6,214	93	62	60	Mod	V Good	2	4						83
1014	97TH AVE	HURON ST	DS@109FT	LOCAL	35	109	21	445	50	49	60	Mod	Marginal	36	14	95	9/30/22				95
1023	97TH AVE	DS@109FT	CROKE DR	LOCAL	34	1,451	274	5,757	50	57	60	Mod	Fair	26	24	95	9/30/22				95
1027	98TH AVE	HURON ST	DS@109FT	LOCAL	33	109	20	419	60	21	80	Strng	Marginal	20	21						46
1026	98TH AVE	DS@109FT	PALMER LN	LOCAL	33	267	49	1,026	52	71	60	Mod	Fair	29	19						58
1025	98TH AVE	PALMER LN	LUNCEFORD LN	LOCAL	33	350	64	1,346	47	59	60	Mod	Fair	34	19						50
1030	98TH AVE	LUNCEFORD LN	RAPP LN	LOCAL	33	386	71	1,488	44	54	60	Mod	Marginal	34	22						47
1033	98TH AVE	RAPP LN	CROKE DR	LOCAL	34	360	68	1,427	59	51	60	Mod	Fair	22	19						56
1036	99TH AVE	MELODY DR	ALAMO DR	LOCAL	38	295	62	1,310	74	53	60	Mod	Good	10	16						66
1039	99TH AVE	ALAMO DR	BRUCE LN	LOCAL	39	560	121	2,550	75	64	60	Mod	V Good	6	19						71
1040	99TH AVE	BRUCE LN	DODGE DR	LOCAL	39	583	126	2,654	90	74	60	Mod	Excellent	5	4						85
1046	99TH AVE	DODGE DR	HURON ST	LOCAL	39	459	99	2,089	73	61	60	Mod	Good	17	10						69
1045	99TH AVE	HURON ST	PALMER LN	LOCAL	33	373	68	1,437	86	56	60	Mod	V Good	0	14						76
1042	99TH AVE	PALMER LN	LUNCEFORD LN	LOCAL	33	351	64	1,350	77	78	60	Mod	V Good	9	14						77
1048	99TH AVE	LUNCEFORD LN	RAPP LN	LOCAL	32	313	56	1,167	58	66	60	Mod	Good	19	23						60
1051	99TH AVE	RAPP LN	CROKE DR	LOCAL	32	406	72	1,517	76	63	60	Mod	V Good	6	18						71
1310	ACOMA ST	KENNEDY DR	WELLINGTON ST	LOCAL	37	274	56	1,182	100	60	60	Mod	Excellent	0	0						86
1329	ACOMA ST	WELLINGTON ST	WALLACE ST	LOCAL	37	289	59	1,245	100	73	60	Mod	Excellent	0	0						91
1355	ACOMA ST	WALLACE ST	VERNA LN	LOCAL	39	291	63	1,324	100	81	60	Mod	Excellent	0	0						93
1390	ACOMA ST	VERNA LN	BONITA PL	LOCAL	38	1,010	213	4,478	100	82	60	Mod	Excellent	0	0						93
1453	ACOMA ST	BONITA PL	PINYON DR	LOCAL	36	1,005	201	4,220	100	68	60	Mod	Excellent	0	0						89
1470	ACOMA ST	PINYON DR	NORTH END	LOCAL	35	146	28	594	86	62	60	Mod	V Good	0	14						78
1551	ACOMA ST	W 116TH AVE	W 116TH WAY	LOCAL	36	567	113	2,380	94	68	60	Mod	Excellent	3	3						85
1562	ACOMA ST	W 116TH AVE	W 117TH AVE	LOCAL	33	194	36	748	100	54	60	Mod	V Good	0	0						84
1584	ACOMA ST	W 117TH AVE	W 118TH AVE	LOCAL	36	621	124	2,607	100	67	60	Mod	Excellent	0	0						89
2104	ADAMS CIR	ADAMS ST	NE END	LOCAL	43	175	42	888	50	33	80	Strng	Marginal	16	33						44
2108	ADAMS CT	SW END	ADAMS ST	LOCAL	40	283	62	1,310	41	32	80	Strng	Poor	30	29						38
2201	ADAMS CT	SOUTH END	E 108TH AVE	LOCAL	44	186	45	954	63	25	80	Strng	Fair	11	26						50
2057	ADAMS ST	MADISON WAY	COOK CT	LOCAL	34	188	35	744	59	63	60	Mod	Good	26	15						60
2064	ADAMS ST	COOK CT	E 105TH PL	LOCAL	34	351	66	1,393	68	65	60	Mod	Good	12	20						66
2097	ADAMS ST	E 105TH PL	ADAMS CIR	LOCAL	35	313	61	1,278	60	70	60	Mod	Good	19	21						63
2109	ADAMS ST	ADAMS CIR	ADAMS CT	LOCAL	32	225	40	840	60	58	60	Mod	Fair	20	20						59
2115	ADAMS ST	ADAMS CT	FOX RUN PKWY	LOCAL	32	255	45	952	47	35	80	Strng	Marginal	31	21						42
2194	ADAMS ST	FOX RUN CIR	E 108TH AVE	LOCAL	33	316	58	1,216	49	55	80	Strng	Fair	19	32						50
1053	ALAMO DR	W 99TH AVE	BELL LN	LOCAL	34	726	137	2,878	38	58	60	Mod	Marginal	33	28	95	9/30/22				95
1058	ALAMO DR	BELL LN	BELFORD DR	LOCAL	39	343	74	1,560	37	54	60	Mod	Marginal	37	27	95	9/30/22				95
1641	ALVIN DR	E 109TH PL	MURIEL DR	LOCAL	37	1,154	237	4,980	94	66	60	Mod	V Good	1	5						85

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	
1149	BANNOCK ST	SOUTH END	W 104TH AVE	LOCAL	44	1,620	396	8,316	28	43	60	Mod	Poor	45	24	33
1389	BANNOCK ST	VERNA LN	BONITA PL	LOCAL	37	926	190	3,997	69	60	60	Mod	Good	13	17	66
1059	BELFORD DR	MELODY DR	ALAMO DR	LOCAL	37	264	54	1,140	80	66	60	Mod	V Good	7	13	75
1070	BELFORD DR	ALAMO DR	BRUCE LN	LOCAL	37	421	87	1,817	80	80	60	Mod	V Good	8	12	80
1074	BELFORD DR	BRUCE LN	CLARK DR	LOCAL	38	276	58	1,224	77	79	60	Mod	V Good	6	16	78
1080	BELFORD DR	CLARK DR	DODGE DR	LOCAL	36	289	58	1,215	72	65	60	Mod	Good	14	14	69
1090	BELFORD DR	DODGE DR	W 100TH PL	LOCAL	36	288	58	1,208	83	58	60	Mod	V Good	5	12	74
1052	BELL LN	ALAMO DR	BRUCE LN	LOCAL	35	747	145	3,049	65	51	60	Mod	Good	18	17	60
1362	BETH LN	LIVINGSTON DR	CLAIRE LN	LOCAL	36	1,195	239	5,020	88	72	60	Mod	V Good	7	5	82
2212	BLUE JAY LN	GARLAND DR	E 108TH AVE	LOCAL	35	769	150	3,140	48	56	60	Mod	Fair	26	26	50
2235	BLUE JAY LN	E 108TH AVE	E 109TH AVE	LOCAL	37	369	76	1,593	49	73	60	Mod	Fair	25	25	57
1369	BLUE JAY LN	E 109TH AVE	E 109TH PL	LOCAL	39	735	159	3,342	45	77	60	Mod	Fair	29	26	55
1404	BONITA PL	ACOMA ST	BANNOCK ST	LOCAL	38	361	76	1,601	100	64	60	Mod	Excellent	0	0	88
1399	BONITA PL	BANNOCK ST	VERNA LN	LOCAL	38	292	62	1,295	100	45	60	Mod	V Good	0	0	82
1396	BONITA PL	VERNA LN	MELODY DR	LOCAL	37	298	61	1,286	100	52	60	Mod	V Good	0	0	84
1910	BOWMAN PL	MARION ST	IRMA DR	LOCAL	37	958	197	4,135	33	51	60	Mod	Poor	38	29	38
1217	BREWER DR	W 104TH PL	CARMELA LN	LOCAL	38	794	168	3,520	85	79	60	Mod	V Good	7	7	83
1234	BREWER DR	CARMELA LN	W 106TH AVE	LOCAL	39	287	62	1,307	88	56	60	Mod	V Good	5	7	77
1277	BREWER DR	W 106TH AVE	ROSEANNA DR	LOCAL	38	914	193	4,054	66	56	60	Mod	Good	14	20	62
1343	BREWER DR	ROSEANNA DR	CLAIRE LN	LOCAL	37	1,447	297	6,245	86	63	60	Mod	V Good	9	5	78
1083	BRIGITTE DR	MELODY DR	BRUCE LN	LOCAL	36	384	77	1,612	47	64	60	Mod	Fair	26	27	52
1093	BRIGITTE DR	BRUCE LN	BRIGITTE DR	LOCAL	38	288	61	1,275	39	71	60	Mod	Marginal	28	33	49
1094	BRIGITTE DR	BRIGITTE DR	MELODY DR	LOCAL	38	642	136	2,848	42	60	60	Mod	Marginal	30	28	47
1049	BRUCE LN	W 99TH AVE	BELL LN	LOCAL	36	337	67	1,414	44	34	80	Strng	Marginal	34	15	40
1069	BRUCE LN	BELL LN	BELFORD DR	LOCAL	35	861	167	3,516	52	41	60	Mod	Marginal	30	19	47
1084	BRUCE LN	BRIGITTE DR	MELODY DR	LOCAL	37	456	94	1,969	66	55	80	Strng	Good	10	24	62
2094	CARLILE ST	CLARKSON ST	E 105TH PL	LOCAL	36	847	169	3,559	35	57	60	Mod	Marginal	38	27	42
1738	CARLILE ST	MURIEL DR	CLARKSON ST	LOCAL	38	763	161	3,381	97	74	60	Mod	Excellent	2	2	89
1867	CARLILE ST	E 114TH PL	E 115TH AVE	LOCAL	38	705	149	3,124	32	47	80	Strng	Poor	33	35	35
1220	CARMELA LN	W 104TH PL	BREWER DR	LOCAL	37	1,130	232	4,879	31	54	60	Mod	Poor	42	27	38
1321	CARROL LN	GARLAND DR	PIKE ST	LOCAL	38	911	192	4,038	100	75	60	Mod	Excellent	0	0	91
1345	CARROL LN	PIKE ST	TEAL ST	LOCAL	38	715	151	3,172	100	79	60	Mod	Excellent	0	0	93
1460	CHEROKEE ST	PINYON DR	W 112TH AVE	LOCAL	35	951	185	3,881	100	64	60	Mod	Excellent	0	0	88
1511	CHEROKEE ST	W 114TH AVE	W 114TH PL	LOCAL	40	484	108	2,259	35	53	60	Mod	Marginal	36	30	40
1518	CHEROKEE ST	W 114TH PL	W 115TH AVE	LOCAL	40	370	82	1,726	41	54	60	Mod	Marginal	31	27	45
1401	CLAIRE CIR	CLAIRE LN	CLAIRE CIR (CDS)	LOCAL	35	638	124	2,605	86	67	60	Mod	V Good	5	9	79
1409	CLAIRE CIR	CLAIRE CIR (CDS)	CLAIRE LN	LOCAL	36	511	102	2,145	82	59	60	Mod	V Good	6	12	74
1392	CLAIRE CIR (CDS)	CLAIRE CIR	SE END	LOCAL	77	52	22	470	70	22	80	Strng	Fair	0	30	54
1342	CLAIRE LN	ROSEANNA DR	BREWER DR	LOCAL	37	1,140	234	4,921	77	74	60	Mod	V Good	11	12	76
1361	CLAIRE LN	BREWER DR	BETH LN	LOCAL	40	287	64	1,339	80	82	60	Mod	V Good	0	20	80
1373	CLAIRE LN	BETH LN	KENNEDY DR	LOCAL	38	296	63	1,313	82	52	60	Mod	V Good	6	12	71
1395	CLAIRE LN	KENNEDY DR	CLAIRE LN	LOCAL	37	325	67	1,401	90	66	60	Mod	V Good	0	10	82
1407	CLAIRE LN	ROSEANNA DR	MANET WAY	LOCAL	38	545	115	2,417	79	65	60	Mod	V Good	10	11	74

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1411	CLAIRE LN	MANET WAY	LIVINGSTON DR	LOCAL	37	339	70	1,463	84	76	60	Mod	V Good	0	16						81
1421	CLAIRE LN	LIVINGSTON DR	LAMBERT LN	LOCAL	38	419	89	1,859	77	61	60	Mod	V Good	12	10						72
1419	CLAIRE LN	LAMBERT LN	CLAIRE CIR	LOCAL	36	314	63	1,319	88	69	60	Mod	V Good	0	12						81
1408	CLAIRE LN	CLAIRE CIR	CLAIRE CIR	LOCAL	37	484	100	2,090	87	78	60	Mod	V Good	3	10						84
1400	CLAIRE LN	CLAIRE CIR	CLAIRE LN	LOCAL	36	284	57	1,195	84	59	60	Mod	V Good	9	7						75
1073	CLARK DR	DODGE DR	BELFORD DR	LOCAL	37	1,330	273	5,740	26	55	60	Mod	Poor	48	26	95	9/30/22				95
2164	CLARKSON CT	SOUTH END	GARLAND DR	LOCAL	44	332	81	1,699	66	48	80	Strng	Fair	11	23						59
2174	CLARKSON CT	GARLAND DR	NORTH END	LOCAL	56	100	31	649	66	33	80	Strng	Fair	10	24						54
1977	CLARKSON CT	SOUTH END	SYLVIA DR	LOCAL	45	297	74	1,561	86	48	80	Strng	V Good	0	14						73
2083	CLARKSON ST	CARLILE ST	E 105TH PL	LOCAL	36	779	156	3,272	35	54	60	Mod	Marginal	35	30						41
2226	CLARKSON ST	SOUTH END	MILDRED DR	LOCAL	60	91	31	642	45	30	80	Strng	Marginal	27	28						39
2237	CLARKSON ST	MILDRED DR	NORTH END	LOCAL	47	257	67	1,409	71	45	80	Strng	Good	0	29						62
1739	CLARKSON ST	MURIEL DR	CARLILE ST	LOCAL	36	768	154	3,226	100	75	60	Mod	Excellent	0	0						91
1788	CLARKSON ST	E 112TH PL	OGDEN DR	LOCAL	36	223	45	937	70	55	80	Strng	Good	9	21						65
1791	CLARKSON ST	OGDEN DR	CORONA DR	LOCAL	37	250	51	1,079	55	64	60	Mod	Fair	24	21						57
1804	CLARKSON ST	CORONA DR	DOWNING DR	LOCAL	38	250	53	1,108	67	60	60	Mod	Good	15	18						64
1820	CLARKSON ST	DOWNING DR	MALLEY DR	LOCAL	38	255	54	1,132	42	42	60	Mod	Marginal	35	23						41
1866	CLARKSON ST	E 114TH AVE	E 115TH AVE	LOCAL	38	670	141	2,971	85	65	60	Mod	V Good	4	10						78
1702	CLAUDE CT	E 109TH PL	GAYLORD ST	LOCAL	31	705	121	2,549	52	59	80	Strng	Fair	14	34						53
1784	CLAUDE CT	E 112TH PL	GRAVES CT	COLLECTOR	37	234	48	1,010	70	65	60	Mod	Good	15	15						68
1790	CLAUDE CT	GRAVES CT	E 113TH AVE	COLLECTOR	36	262	52	1,099	79	59	60	Mod	V Good	11	10						72
1802	CLAUDE CT	E 113TH AVE	E 113TH PL	COLLECTOR	37	251	52	1,083	78	75	60	Mod	V Good	8	14						77
1817	CLAUDE CT	E 113TH PL	E 114TH AVE	COLLECTOR	37	252	52	1,088	82	58	60	Mod	V Good	4	13						74
1833	CLAUDE CT	E 114TH AVE	E 114TH PL	COLLECTOR	37	253	52	1,090	78	85	60	Mod	V Good	10	13						80
1848	CLAUDE CT	E 114TH PL	FISHER WAY	COLLECTOR	36	253	51	1,061	80	69	60	Mod	V Good	8	12						76
1858	CLAUDE CT	FISHER WAY	E 115TH AVE	COLLECTOR	36	256	51	1,076	82	77	60	Mod	V Good	3	15						80
1872	CLAUDE CT	E 115TH AVE	E 115TH PL	COLLECTOR	37	257	53	1,111	100	57	60	Mod	Excellent	0	0						85
1877	CLAUDE CT	E 115TH PL	E 116TH AVE	COLLECTOR	36	251	50	1,054	100	83	60	Mod	Excellent	0	0						94
1900	CLAUDE CT	E 116TH AVE	E 116TH DR	COLLECTOR	37	508	104	2,191	95	63	60	Mod	V Good	0	5						84
1912	CLAUDE CT	E 116TH DR	PHILLIPS DR	COLLECTOR	27	260	39	817	97	39	60	Mod	V Good	0	3						77
1933	CLAUDE CT	PHILLIPS DR	TRUDA DR	COLLECTOR	37	263	54	1,133	100	59	60	Mod	Excellent	0	0						86
1942	CLAUDE CT	TRUDA DR	LAVINIA LN	COLLECTOR	36	257	51	1,081	82	76	60	Mod	V Good	3	16						79
1971	CLAUDE CT	LAVINIA LN	CLAUDE WAY	COLLECTOR	37	588	121	2,539	72	66	60	Mod	V Good	14	13						70
1983	CLAUDE CT	CLAUDE WAY	E 119TH PL	COLLECTOR	37	260	53	1,121	68	58	60	Mod	Good	20	12						64
2019	CLAUDE CT	E 119TH PL	E 120TH AVE	COLLECTOR	36	639	128	2,682	69	63	60	Mod	Good	20	12						66
1984	CLAUDE CT	E 119TH PL	EAST END	LOCAL	76	96	41	854	42	51	80	Strng	Marginal	28	30						44
2042	CLAUDE CT	E 120TH AVE	E 124TH AVE	COLLECTOR	25	2,833	393	8,263	32	63	60	Mod	Marginal	48	20	95	9/30/22				95
2016	CLAUDE WAY	CLAUDE CT	E 119TH PL	LOCAL	39	1,148	249	5,224	46	62	60	Mod	Fair	24	30						51
2262	CLAYTON ST	E 109TH AVE	E 109TH CT	LOCAL	35	266	52	1,085	78	62	60	Mod	V Good	6	16						72
1640	CLAYTON ST	E 109TH CT	E 110TH AVE	LOCAL	34	200	38	795	72	31	80	Strng	Fair	6	22						58
1667	CLAYTON ST	E 110TH AVE	FILLMORE WAY	LOCAL	36	125	25	526	63	48	80	Strng	Fair	15	22						57
1685	CLAYTON ST	FILLMORE WAY	E 110TH CT	LOCAL	32	265	47	990	55	48	80	Strng	Fair	17	28						52
1731	CLAYTON ST	E 110TH CT	E 111TH DR	LOCAL	34	539	102	2,137	59	49	80	Strng	Fair	16	25						55

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1724	COLUMBINE CT	SE END	E 111TH DR	LOCAL	41	237	54	1,136	67	35	80	Strng	Fair	5	28						56
2261	COLUMBINE ST	E 109TH AVE	E 109TH DR	LOCAL	35	215	42	878	66	58	60	Mod	Good	14	20						63
1663	COLUMBINE ST	E 109TH DR	E 110TH DR	LOCAL	34	462	87	1,833	43	72	80	Strng	Fair	19	37						52
1481	COMMUNITY CENTER DR	HURON ST	I-25	ARTERIAL	30	2,480	413	8,679	40	67	60	Mod	Marginal	29	29						48
1500	COMMUNITY CENTER DR	I-25	MALLEY DR	ARTERIAL	36	933	187	3,918	25	72	60	Mod	Marginal	42	33						40
1601	COMMUNITY CENTER DR	MALLEY DR	GRANT ST	ARTERIAL	39	2,958	641	13,457	25	59	60	Mod	Poor	49	26						35
2063	COOK CT	ADAMS ST	NE END	LOCAL	42	304	71	1,483	58	40	80	Strng	Fair	12	30						52
2197	COOK CT	SOUTH END	E 108TH AVE	LOCAL	45	188	47	982	58	44	80	Strng	Fair	13	29						53
2199	COOK ST	FOX RUN CIR	E 108TH AVE	LOCAL	35	340	66	1,389	54	59	60	Mod	Fair	19	27						55
2186	CORING PL	DOWNING CT	LOREN LN	LOCAL	38	945	200	4,191	38	74	60	Mod	Fair	31	31						49
2173	CORING PL	LOREN LN	LERROY DR	LOCAL	37	714	147	3,082	35	74	60	Mod	Marginal	42	23						48
1792	CORONA DR	E 112TH PL	CLARKSON ST	LOCAL	39	1,286	279	5,852	54	61	60	Mod	Fair	23	23						56
2081	CORONA ST	DOWNING ST	E 105TH PL	LOCAL	38	788	166	3,494	99	65	60	Mod	Excellent	0	1						87
2191	CORONA ST	LERROY DR	DOWNING CT	LOCAL	38	1,066	225	4,727	53	49	80	Strng	Fair	11	36						51
1019	COVEY CT	LOU DR	FRED DR	LOCAL	38	583	123	2,585	82	61	60	Mod	V Good	8	10						75
1022	CROKE DR	SW END	W 97TH AVE	LOCAL	34	116	22	460	69	40	80	Strng	Fair	13	18	95	9/30/22				95
1032	CROKE DR	W 97TH AVE	W 98TH AVE	LOCAL	37	268	55	1,157	26	42	80	Strng	Poor	41	19	95	9/30/22				95
1050	CROKE DR	W 98TH AVE	W 99TH AVE	LOCAL	36	697	139	2,929	18	53	80	Strng	Poor	32	27	95	9/30/22				95
2277	CROKE DR	W 99TH AVE	DS@854FT	LOCAL	39	854	185	3,887	13	52	80	Strng	Poor	48	18	95	9/30/22				95
1081	CROKE DR	DS@854FT	W 100TH PL	LOCAL	37	250	51	1,079	33	57	80	Strng	Marginal	28	39	95	9/30/22				95
1091	CROKE DR	W 100TH PL	W 101ST AVE	LOCAL	35	270	53	1,104	31	69	80	Strng	Marginal	26	43	95	9/30/22				95
1100	CROKE DR	W 101ST AVE	W 101ST PL	LOCAL	38	270	57	1,198	40	76	60	Mod	Fair	30	30	95	9/30/22				95
1104	CROKE DR	W 101ST PL	W 102ND AVE	LOCAL	37	270	55	1,164	38	79	60	Mod	Fair	28	34	95	9/30/22				95
1109	CROKE DR	W 102ND AVE	W 102ND PL	LOCAL	36	271	54	1,138	49	81	60	Mod	Fair	17	34	95	9/30/22				95
1116	CROKE DR	W 102ND PL	W 103RD AVE	LOCAL	36	269	54	1,130	52	73	60	Mod	Fair	19	29	95	9/30/22				95
1124	CROKE DR	W 103RD AVE	W 103RD PL	LOCAL	36	351	70	1,473	46	74	80	Strng	Fair	19	35	95	9/30/22				95
1129	CROKE DR	W 103RD PL	W 103RD PL	LOCAL	39	113	24	512	25	56	60	Mod	Poor	43	32	95	9/30/22				95
1130	CROKE DR	W 103RD PL	SPANGLER DR	LOCAL	38	268	57	1,188	47	48	60	Mod	Marginal	28	25	95	9/30/22				95
1140	CROKE DR	SPANGLER DR	W 104TH AVE	LOCAL	40	214	47	996	29	61	60	Mod	Poor	40	31	95	9/30/22				95
1009	DAMON DR	SWITZER LN	FRED DR	LOCAL	38	1,322	279	5,862	54	62	80	Strng	Fair	18	28						56
1061	DARWIN CT	NIVER AVE	SOUTH END	LOCAL	42	126	29	614	38	35	80	Strng	Poor	31	30	95	9/30/22				95
1913	DEAN DR	KEOUGH DR	MONTE WAY	LOCAL	38	1,079	228	4,783	99	73	60	Mod	Excellent	1	0						90
1943	DEAN DR	MONTE WAY	TRUDA DR	LOCAL	39	772	167	3,512	97	75	60	Mod	Excellent	0	3						89
1953	DEAN DR	TRUDA DR	IRMA DR	LOCAL	38	837	177	3,711	95	62	60	Mod	V Good	0	5						84
1586	DELAWARE CT	W 117TH AVE	W 118TH AVE	LOCAL	38	639	135	2,832	39	56	60	Mod	Marginal	32	29	95	9/30/22				95
1596	DELAWARE CT	W 118TH AVE	NORTH END	LOCAL	37	196	40	847	29	20	80	Strng	Poor	46	26						25
1733	DETROIT WAY	FILLMORE WAY	E 111TH DR	LOCAL	34	595	112	2,362	67	53	80	Strng	Good	8	24						62
1055	DODGE DR	W 99TH AVE	CLARK DR	LOCAL	37	336	69	1,452	71	54	60	Mod	Good	11	18						65
1079	DODGE DR	CLARK DR	BELFORD DR	LOCAL	35	1,180	229	4,816	69	72	60	Mod	Good	10	21						69
2192	DOWNING CT	CORING PL	E 108TH AVE	LOCAL	37	250	51	1,080	58	53	80	Strng	Fair	15	27						56
2211	DOWNING CT	E 108TH AVE	REGINA LN	LOCAL	38	250	53	1,106	70	64	80	Strng	Good	5	25						68
2224	DOWNING CT	REGINA LN	LARRY DR	LOCAL	38	250	53	1,109	53	55	80	Strng	Fair	20	27						53
1800	DOWNING DR	E 112TH PL	DOWNING DR (CDS)	LOCAL	36	882	176	3,706	57	63	60	Mod	Fair	17	24						59

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)			
1805	DOWNING DR	DOWNING DR (CDS)	CLARKSON ST	LOCAL	39	801	174	3,644	49	58	60	Mod	Fair	23	28						51
1814	DOWNING DR (CDS)	DOWNING DR	NORTH END	LOCAL	51	154	43	910	37	41	60	Mod	Poor	38	25						38
2079	DOWNING ST	CORONA ST	E 105TH PL	LOCAL	36	790	158	3,318	98	62	60	Mod	Excellent	0	2						86
2181	DOWNING ST	LEROY DR	DOWNING CT	LOCAL	39	863	187	3,928	50	70	60	Mod	Fair	22	28						56
1885	DOWNING ST	MALLEY DR	OGDEN ST	LOCAL	39	1,212	263	5,517	48	66	60	Mod	Fair	21	31						54
5004	RAINS JR. MEMORIAL PARK - SE	0	0	PARKING LOT	18	1,567	159	3,332	16	34	80	Strng	V Poor	38	26						22
5003	RAINS JR. MEMORIAL PARK - SW	0	0	PARKING LOT	40	916	203	4,272	62	42	60	Mod	Fair	22	16						55
1583	ELATI CT	W 117TH AVE	W 118TH AVE	LOCAL	39	623	135	2,832	32	53	60	Mod	Poor	37	31	95	9/30/22				95
1464	ELATI ST	MELODY DR	W 112TH AVE	LOCAL	38	919	194	4,076	40	53	60	Mod	Marginal	37	23						44
1637	ELIZABETH ST	E 109TH AVE	ELIZABETH ST/E 110TH AVE E	LOCAL	36	468	94	1,964	63	52	80	Strng	Fair	10	27						59
1712	ELMER DR	EMERSON ST	OGDEN ST	LOCAL	39	251	54	1,141	53	67	60	Mod	Fair	24	22						57
1710	ELMER DR	OGDEN ST	LARSON DR	LOCAL	39	747	162	3,397	53	68	60	Mod	Fair	25	22						57
1708	ELMER DR	LARSON DR	E 110TH PL	LOCAL	39	1,045	226	4,756	38	69	60	Mod	Marginal	37	25						48
1665	EMERSON CIR	OGDEN CIR	MURIEL DR	LOCAL	40	642	143	2,995	100	51	60	Mod	V Good	0	0						83
2099	EMERSON ST	OGDEN ST	E 105TH PL	LOCAL	39	909	197	4,137	99	68	60	Mod	Excellent	0	1						88
1711	EMERSON ST	MURIEL DR	ELMER DR	LOCAL	39	542	117	2,467	40	52	80	Strng	Marginal	30	30						44
1728	EMERSON ST	ELMER DR	E 111TH PL	LOCAL	39	214	46	972	55	64	60	Mod	Fair	21	24						58
1871	EMERSON ST	E 114TH PL	E 115TH AVE	LOCAL	39	652	141	2,968	57	46	80	Strng	Fair	15	28						53
1441	EMERY RD	NW END	NORTHGLENN DR	LOCAL	39	146	32	665	20	31	60	Mod	V Poor	58	22						24
1440	EMERY RD	NORTHGLENN DR	GRANT DR	LOCAL	39	1,312	284	5,968	59	62	60	Mod	Fair	18	23						59
1714	EMERY RD	GRANT DR	PEARL ST	LOCAL	39	797	173	3,625	56	52	60	Mod	Fair	22	22						54
2184	FILLMORE CT	SOUTH END	E 107TH CT	LOCAL	43	261	62	1,297	76	37	80	Strng	Good	0	24						63
2170	FILLMORE WAY	SW END	FILLMORE WAY (CDS)	LOCAL	42	337	78	1,640	77	47	80	Strng	Good	7	16						66
2182	FILLMORE WAY	FILLMORE WAY (CDS)	E 107TH CT	LOCAL	36	127	25	532	85	45	60	Mod	V Good	4	11						71
1680	FILLMORE WAY	CLAYTON ST	DETROIT WAY	LOCAL	35	250	49	1,023	65	49	80	Strng	Fair	8	27						59
1729	FILLMORE WAY	DETROIT WAY	E 111TH DR	LOCAL	36	628	126	2,636	62	58	80	Strng	Good	9	29						60
2171	FILLMORE WAY (CDS)	FILLMORE WAY	EAST END	LOCAL	69	15	6	121	89	19	80	Strng	Good	0	11						65
1859	FISHER WAY	E 115TH AVE	CLAUDE CT	LOCAL	38	1,094	231	4,850	55	58	60	Mod	Fair	23	22						56
1816	FOWLER DR	E 112TH PL	E 114TH PL	LOCAL	39	949	206	4,318	44	61	60	Mod	Marginal	40	16						49
1853	FOWLER DR	E 114TH PL	MALLEY DR	LOCAL	39	1,072	232	4,876	40	59	60	Mod	Marginal	31	29						46
1895	FOWLER DR	MALLEY DR	HUMBOLDT ST	LOCAL	39	1,226	266	5,580	35	57	60	Mod	Marginal	42	23						41
1897	FOWLER DR	HUMBOLDT ST	IRMA DR	LOCAL	39	264	57	1,203	30	35	60	Mod	Poor	44	25						31
1133	FOX CT	MONTEREY CIR	SW END	LOCAL	44	479	118	2,475	65	55	80	Strng	Good	10	25						61
2152	FOX RUN CIR	FOX RUN PKWY	KIMBLEWYCK CIR	LOCAL	39	105	23	478	47	38	80	Strng	Marginal	22	31						43
2175	FOX RUN CIR	KIMBLEWYCK CIR	ADAMS ST	LOCAL	38	327	69	1,452	39	66	80	Strng	Marginal	26	36						47
2176	FOX RUN CIR	ADAMS ST	KIMBLEWYCK CIR	LOCAL	38	228	48	1,012	44	66	60	Mod	Fair	27	29						51
2178	FOX RUN CIR	KIMBLEWYCK CIR	COOK ST	LOCAL	38	267	56	1,183	32	57	80	Strng	Marginal	28	40						40
2114	FOX RUN CIR	FOX RUN PKWY	KIMBLEWYCK CIR	LOCAL	38	239	50	1,059	45	53	80	Strng	Marginal	23	32						47
2129	FOX RUN CIR	KIMBLEWYCK CIR	E 106TH PL	LOCAL	39	227	49	1,033	38	60	80	Strng	Marginal	27	35						45
2143	FOX RUN CIR	E 106TH PL	E 107TH AVE	LOCAL	38	250	53	1,109	52	59	80	Strng	Fair	20	28						54
2179	FOX RUN CIR	E 107TH AVE	COOK ST	LOCAL	39	451	98	2,054	38	59	80	Strng	Marginal	29	33						44
2061	FOX RUN PKWY	MADISON WAY	SE END	COLLECTOR	14	234	18	382	26	40	60	Mod	Poor	56	18						29
2062	FOX RUN PKWY	SE END	MADISON WAY	COLLECTOR	15	240	20	420	26	47	60	Mod	Poor	45	29						32

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)				
2112	FOX RUN PKWY	MADISON WAY	FOX RUN CIR	COLLECTOR	39	878	190	3,994	44	60	60	Mod	Marginal	29	27							49
2116	FOX RUN PKWY	FOX RUN CIR	ADAMS ST	COLLECTOR	39	352	76	1,600	75	64	60	Mod	V Good	9	16							70
2132	FOX RUN PKWY	ADAMS ST	KIMBLEWYCK CIR	COLLECTOR	39	229	50	1,044	61	60	60	Mod	Good	18	22							59
2149	FOX RUN PKWY	KIMBLEWYCK CIR	FOX RUN CIR	COLLECTOR	39	386	84	1,756	62	56	60	Mod	Good	17	21							59
2167	FOX RUN PKWY	FOX RUN CIR	MILWAUKEE ST	COLLECTOR	39	418	91	1,903	60	71	60	Mod	Good	19	21							63
2188	FOX RUN PKWY	MILWAUKEE ST	E 107TH CT	COLLECTOR	39	384	83	1,749	48	61	60	Mod	Fair	27	25							52
2207	FOX RUN PKWY	E 107TH CT	E 108TH AVE	COLLECTOR	39	282	61	1,282	57	64	60	Mod	Fair	19	23							59
2234	FOX RUN PKWY	E 108TH AVE	E 108TH DR	COLLECTOR	39	394	85	1,794	53	59	60	Mod	Fair	29	17							54
2259	FOX RUN PKWY	E 108TH DR	E 109TH AVE	COLLECTOR	40	950	211	4,434	65	60	80	Strng	Good	10	25							63
1646	FOX RUN PKWY	E 109TH AVE	E 110TH AVE	COLLECTOR	39	453	98	2,063	70	64	80	Strng	Good	3	27							67
1670	FOX RUN PKWY	E 110TH AVE	E 110TH DR	COLLECTOR	38	550	116	2,440	68	50	80	Strng	Good	12	19							61
1716	FOX RUN PKWY	E 110TH DR	E 111TH DR	COLLECTOR	39	585	127	2,663	59	54	80	Strng	Fair	18	23							56
1757	FOX RUN PKWY	E 111TH DR	E 112TH AVE	COLLECTOR	59	366	120	2,523	54	49	60	Mod	Fair	26	20							51
1018	FOX ST	SOUTH END	FRED DR	LOCAL	39	646	140	2,939	76	67	60	Mod	V Good	9	15							73
2074	FRANKLIN ST	FRANKLIN WAY	E 105TH PL	LOCAL	39	789	171	3,589	46	56	60	Mod	Marginal	26	27							49
1812	FRANKLIN ST	E 112TH PL	E 114TH PL	LOCAL	40	857	191	4,001	65	66	60	Mod	Good	13	22							65
1828	FRANKLIN ST	E 114TH PL	HUMBOLDT ST	LOCAL	39	362	79	1,649	66	77	60	Mod	Good	10	24							69
2070	FRANKLIN WAY	FRANKLIN ST	E 105TH PL	LOCAL	39	789	171	3,588	48	51	60	Mod	Marginal	27	25							48
2267	FRED DR	DAMON DR	PELON DR	LOCAL	39	290	63	1,318	100	55	60	Mod	Excellent	0	0							85
1013	FRED DR	PELON DR	LOU DR	LOCAL	39	291	63	1,326	100	71	60	Mod	Excellent	0	0							90
1016	FRED DR	LOU DR	LOU DR	LOCAL	39	129	28	588	100	36	60	Mod	V Good	0	0							78
1020	FRED DR	LOU DR	COVEY CT	LOCAL	39	430	93	1,955	95	62	60	Mod	V Good	5	0							84
1021	FRED DR	COVEY CT	FOX ST	LOCAL	39	345	75	1,569	100	50	60	Mod	V Good	0	0							83
1017	FRED DR	FOX ST	HURON ST	LOCAL	39	437	95	1,986	100	62	60	Mod	Excellent	0	0							87
1535	GALAPAGO CT	W 116TH AVE	W 116TH PL	LOCAL	36	355	71	1,489	49	26	80	Strng	Marginal	23	27	95	9/30/22					95
1579	GALAPAGO CT	W 117TH AVE	W 118TH AVE	LOCAL	39	622	135	2,830	39	45	80	Strng	Marginal	33	28	95	9/30/22					95
1250	GARLAND DR	LINCOLN ST	GRANT DR	LOCAL	39	998	216	4,542	42	63	60	Mod	Marginal	29	29							48
1248	GARLAND DR	GRANT DR	LOGAN CT	LOCAL	39	254	55	1,156	89	59	60	Mod	V Good	0	11							79
1258	GARLAND DR	LOGAN CT	NORTHGLENN DR	LOCAL	39	261	57	1,187	87	75	60	Mod	V Good	0	13							83
1262	GARLAND DR	NORTHGLENN DR	CARROL LN	LOCAL	39	315	68	1,431	87	65	60	Mod	V Good	3	10							79
2150	GARLAND DR	CARROL LN	BLUE JAY LN	LOCAL	39	252	55	1,147	56	59	60	Mod	Fair	31	12							56
2161	GARLAND DR	BLUE JAY LN	WASHINGTON ST	LOCAL	39	586	127	2,665	90	66	60	Mod	V Good	5	5							82
2163	GARLAND DR	WASHINGTON ST	CLARKSON CT	LOCAL	40	222	49	1,035	52	53	80	Strng	Fair	21	27							52
2165	GARLAND DR	CLARKSON CT	LARSON DR	LOCAL	39	304	66	1,381	50	60	60	Mod	Fair	26	23							53
5007	GARLAND DR PUBLIC PARKING	0	0	PARKING LOT	18	530	54	1,126	43	18	80	Strng	Poor	31	25							34
1643	GAYLORD ST	E 109TH DR	E 109TH PL	LOCAL	35	236	46	964	67	61	80	Strng	Good	7	25							65
1701	GAYLORD ST	E 109TH PL	CLAUDE CT	LOCAL	34	869	164	3,446	46	73	80	Strng	Fair	17	37							54
1721	GAYLORD ST	CLAUDE CT	DS@162FT	LOCAL	35	162	32	662	42	72	60	Mod	Fair	27	31							52
1741	GAYLORD ST	DS@162FT	E 111TH DR	LOCAL	35	128	25	522	46	34	80	Strng	Marginal	24	30							41
2014	GAYLORD WAY	LAVINIA LN	E 119TH PL	LOCAL	39	1,505	326	6,847	44	66	80	Strng	Fair	22	35							51
2096	GILPIN ST	WILLIAMS ST	E 105TH PL	LOCAL	39	854	185	3,888	65	57	80	Strng	Good	10	25							62
1899	GILPIN ST	E 115TH AVE	IRMA DR	LOCAL	38	1,550	327	6,872	54	60	60	Mod	Fair	25	22							55
1973	GILPIN ST	TRUDA DR	IRMA DR	LOCAL	39	790	171	3,595	88	67	60	Mod	V Good	5	7							80

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1171	GRANT DR	E 104TH AVE	DS@41FT	COLLECTOR	48	41	11	227	59	18	80	Strng	Marginal	0	41	45
1193	GRANT DR	DS@41FT	E 104TH PL	COLLECTOR	42	167	39	820	58	54	60	Mod	Fair	23	19	56
1203	GRANT DR	E 104TH PL	E 105TH AVE	COLLECTOR	39	409	89	1,863	79	74	60	Mod	V Good	9	12	77
1223	GRANT DR	E 105TH AVE	E 106TH AVE	COLLECTOR	40	524	116	2,446	83	75	60	Mod	V Good	8	9	80
1245	GRANT DR	E 106TH AVE	GARLAND DR	COLLECTOR	39	256	55	1,165	84	65	60	Mod	V Good	3	13	77
1259	GRANT DR	GARLAND DR	E 106TH PL	COLLECTOR	39	247	54	1,124	98	73	60	Mod	Excellent	0	2	89
1265	GRANT DR	E 106TH PL	E 107TH AVE	COLLECTOR	40	250	55	1,165	77	93	60	Mod	V Good	7	16	82
1276	GRANT DR	E 107TH AVE	E 107TH PL	COLLECTOR	39	253	55	1,149	68	86	60	Mod	V Good	17	15	73
1300	GRANT DR	E 107TH PL	E 108TH AVE	COLLECTOR	39	249	54	1,134	70	86	60	Mod	V Good	16	14	74
1317	GRANT DR	E 108TH AVE	NORTHGLENN DR	COLLECTOR	39	258	56	1,174	72	91	60	Mod	V Good	13	15	78
1323	GRANT DR	NORTHGLENN DR	PIKE ST	COLLECTOR	39	255	55	1,159	91	82	60	Mod	Excellent	3	7	87
1344	GRANT DR	PIKE ST	TEAL ST	COLLECTOR	39	251	54	1,141	77	92	60	Mod	V Good	10	13	82
1359	GRANT DR	TEAL ST	E 109TH AVE	COLLECTOR	39	250	54	1,138	72	85	60	Mod	V Good	14	14	76
1368	GRANT DR	E 109TH AVE	E 109TH PL	COLLECTOR	39	249	54	1,131	87	85	60	Mod	Excellent	8	5	86
1380	GRANT DR	E 109TH PL	LEONARD LN	COLLECTOR	39	250	54	1,137	89	82	60	Mod	Excellent	4	7	86
1415	GRANT DR	LEONARD LN	MURIEL DR	COLLECTOR	40	687	153	3,208	73	85	60	Mod	V Good	12	14	76
1427	GRANT DR	MURIEL DR	JANICE CT	COLLECTOR	40	255	57	1,188	93	86	60	Mod	Excellent	0	7	90
1438	GRANT DR	JANICE CT	EMERY RD	COLLECTOR	39	250	54	1,139	88	81	60	Mod	Excellent	3	9	85
1451	GRANT DR	EMERY RD	E 111TH PL	COLLECTOR	40	255	57	1,188	88	76	60	Mod	V Good	0	12	84
1473	GRANT DR	E 111TH PL	E 112TH PL	COLLECTOR	39	288	62	1,310	92	67	60	Mod	V Good	0	8	83
1479	GRANT DR	E 112TH PL	MMACULATE HEART ACCES:	COLLECTOR	39	458	99	2,083	92	75	60	Mod	Excellent	0	8	86
1488	GRANT DR	IMMACULATE HEART ACCESS	MALLEY DR	COLLECTOR	38	625	132	2,769	86	72	60	Mod	V Good	4	5	81
1202	GRANT ST	E 104TH PL	E 105TH AVE	LOCAL	39	410	89	1,866	55	41	60	Mod	Fair	26	19	50
1530	GRANT ST	SOUTH END	E 116TH AVE	LOCAL	47	158	41	866	66	38	60	Mod	Fair	20	14	56
1547	GRANT ST	E 116TH AVE	E 117TH AVE	LOCAL	67	446	166	3,488	60	52	80	Strng	Fair	14	26	57
1565	GRANT ST	E 117TH AVE	E 118TH AVE	LOCAL	35	690	134	2,819	47	48	80	Strng	Marginal	23	31	46
1592	GRANT ST	E 118TH AVE	E 118TH PL	LOCAL	34	350	66	1,387	61	51	80	Strng	Fair	14	25	57
1621	GRANT ST	COMMUNITY CENTER DR	E 120TH AVE	ARTERIAL	73	955	387	8,130	74	63	60	Mod	V Good	15	11	70
1787	GRAVES CT	IRMA DR	HIGH ST	LOCAL	39	681	148	3,099	40	64	60	Mod	Marginal	37	24	47
1785	GRAVES CT	HIGH ST	CLAUDE CT	LOCAL	39	709	154	3,226	40	69	60	Mod	Fair	34	26	49
1353	HERMOSA CT	WELLINGTON ST	HERMOSA CT (CDS)	LOCAL	39	513	111	2,336	65	58	60	Mod	Good	18	17	62
1385	HERMOSA CT	HERMOSA CT (CDS)	PATTERSON CT	LOCAL	38	858	181	3,802	64	63	60	Mod	Good	14	22	95
1352	HERMOSA CT (CDS)	HERMOSA CT	SW END	LOCAL	90	80	40	836	51	37	80	Strng	Marginal	2	47	46
1793	HIGH ST	GRAVES CT	E 113TH AVE	LOCAL	39	315	68	1,432	69	54	80	Strng	Good	10	21	63
1810	HIGH ST	E 113TH AVE	E 113TH PL	LOCAL	39	286	62	1,301	70	73	80	Strng	V Good	2	27	71
1827	HIGH ST	E 113TH PL	E 114TH AVE	LOCAL	39	250	54	1,138	60	67	60	Mod	Good	15	26	61
1841	HIGH ST	E 114TH AVE	E 114TH PL	LOCAL	38	286	60	1,266	65	76	60	Mod	Good	10	25	68
1851	HIGH ST	E 114TH PL	E 115TH AVE	LOCAL	39	324	70	1,472	72	54	80	Strng	Good	3	25	65
1947	HIGH ST	PHILLIPS DR	TRUDA DR	LOCAL	39	261	57	1,188	60	46	80	Strng	Fair	16	24	55
1968	HIGH ST	TRUDA DR	WILLIAMS WAY	LOCAL	39	457	99	2,077	46	62	60	Mod	Fair	28	26	51
1982	HIGH ST	WILLIAMS WAY	WILLIAMS WAY	LOCAL	39	258	56	1,176	48	75	60	Mod	Fair	27	25	56
1991	HIGH ST	WILLIAMS WAY	E 119TH PL	LOCAL	39	439	95	1,997	54	59	80	Strng	Fair	19	27	55
1484	HIGHLINE DR	E 112TH PL	E 113TH PL	LOCAL	34	942	178	3,738	89	62	60	Mod	V Good	9	3	79



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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)
1496	HIGHLINE DR	E 113TH PL	MALLEY DR	LOCAL	33	607	111	2,336	100	63	60	Mod	Excellent	0	0	87
1965	HUMBOLDT DR	KEOUGH DR	LAFAYETTE ST	LOCAL	39	332	72	1,512	69	53	80	Strng	Good	9	22	63
1978	HUMBOLDT DR	LAFAYETTE ST	IRMA WAY	LOCAL	39	461	100	2,099	72	62	80	Strng	Good	2	25	69
2006	HUMBOLDT DR	IRMA WAY	E 119TH PL	LOCAL	39	609	132	2,772	65	59	80	Strng	Good	9	26	62
2068	HUMBOLDT ST	LAFAYETTE ST	E 105TH PL	LOCAL	39	782	169	3,558	53	55	60	Mod	Fair	23	23	53
1826	HUMBOLDT ST	E 112TH PL	FRANKLIN ST	LOCAL	39	1,132	245	5,152	62	76	80	Strng	Good	8	29	66
1896	HUMBOLDT ST	MALLEY DR	FOWLER DR	LOCAL	38	899	190	3,987	39	55	60	Mod	Marginal	38	23	44
1015	HURON ST	SOUTH END	FRED DR	ARTERIAL	63	171	60	1,256	22	56	60	Mod	Poor	46	25	33
1028	HURON ST	FRED DR	W 98TH AVE	ARTERIAL	62	306	106	2,216	18	49	60	Mod	Poor	55	24	27
1037	HURON ST	W 98TH AVE	MELODY DR	ARTERIAL	63	325	114	2,392	8	46	60	Mod	V Poor	66	24	20
1044	HURON ST	MELODY DR	W 99TH AVE	ARTERIAL	63	443	155	3,257	12	59	60	Mod	Poor	64	21	27
1057	HURON ST	W 99TH AVE	NIVER AVE	ARTERIAL	62	365	126	2,637	19	56	60	Mod	Poor	52	23	31
1065	HURON ST	NIVER AVE	W 100TH AVE	ARTERIAL	62	360	124	2,605	20	61	60	Mod	Poor	57	21	33
1075	HURON ST	W 100TH AVE	W 100TH PL	ARTERIAL	64	299	106	2,229	20	53	60	Mod	Poor	58	21	30
1085	HURON ST	W 100TH PL	W 101ST AVE	ARTERIAL	62	271	93	1,961	15	48	60	Mod	Poor	63	18	25
1095	HURON ST	W 101ST AVE	W 101ST PL	ARTERIAL	62	268	92	1,937	20	62	60	Mod	Poor	57	20	33
1101	HURON ST	W 101ST PL	W 102ND AVE	ARTERIAL	62	272	94	1,968	24	56	60	Mod	Poor	54	20	34
1106	HURON ST	W 102ND AVE	W 102ND PL	ARTERIAL	63	273	95	2,003	17	60	60	Mod	Poor	59	22	30
1112	HURON ST	W 102ND PL	W 103RD AVE	ARTERIAL	63	268	94	1,971	9	50	60	Mod	V Poor	65	24	22
1121	HURON ST	W 103RD AVE	W 103RD PL	ARTERIAL	63	349	122	2,566	10	49	60	Mod	V Poor	69	19	23
1144	HURON ST	W 103RD PL	W 104TH AVE	ARTERIAL	60	731	244	5,116	16	45	60	Mod	Poor	59	22	25
1205	HURON ST	W 104TH AVE	NAIAD DR	ARTERIAL	62	704	243	5,093	98	66	60	Mod	Excellent	0	0	87
1241	HURON ST	NAIAD DR	W 106TH AVE	ARTERIAL	62	606	209	4,383	100	81	60	Mod	Excellent	0	0	93
1331	HURON ST	W 106TH AVE	KENNEDY DR	ARTERIAL	63	1,539	539	11,312	96	83	60	Mod	Excellent	0	0	91
1388	HURON ST	KENNEDY DR	HURON ST	ARTERIAL	64	1,181	420	8,819	98	83	60	Mod	Excellent	0	0	93
1413	HURON ST	HURON ST	HURON ST	ARTERIAL	25	315	44	918	100	77	60	Mod	Excellent	0	0	92
1412	HURON ST	HURON ST	HURON ST	ARTERIAL	27	314	47	989	91	86	60	Mod	Excellent	0	0	89
1425	HURON ST	HURON ST	PINNACLE CREEK	ARTERIAL	63	129	45	946	95	95	60	Mod	Excellent	0	0	95
1469	HURON ST	PINNACLE CREEK	COMMUNITY CENTER DR	ARTERIAL	63	789	276	5,796	91	79	60	Mod	Excellent	0	0	86
1471	HURON ST	COMMUNITY CENTER DR	W 114TH AVE	ARTERIAL	63	1,301	455	9,561	72	67	60	Mod	V Good	0	0	70
1623	HURON ST	W 114TH AVE	W 115TH AVE	ARTERIAL	64	850	302	6,347	83	75	60	Mod	V Good	0	0	79
1624	HURON ST	W 115TH AVE	W 116TH AVE	ARTERIAL	76	501	212	4,446	84	73	60	Mod	V Good	0	0	80
1625	HURON ST	W 116TH AVE	W 118TH AVE	ARTERIAL	63	1,455	509	10,694	85	74	60	Mod	V Good	0	0	81
1626	HURON ST	W 118TH AVE	NORTH END	ARTERIAL	67	153	57	1,196	71	62	60	Mod	Good	0	0	68
1483	IMMACULATE HEART ACCESS	WEST END	GRANT DR	LOCAL	23	732	94	1,964	57	52	60	Mod	Fair	26	17	55
1210	INCA ST	W 104TH AVE	NAIAD DR	LOCAL	38	883	186	3,913	71	66	60	Mod	Good	15	14	69
2052	IRMA DR	E 104TH AVE	DS@55FT	COLLECTOR	61	55	19	391	33	10	80	Strng	Poor	47	20	25
2106	IRMA DR	DS@55FT	E 105TH PL	COLLECTOR	63	1,138	398	8,363	25	64	60	Mod	Poor	49	26	37
2156	IRMA DR	E 105TH PL	LEROY DR	COLLECTOR	63	920	322	6,759	30	66	60	Mod	Marginal	45	24	41
2232	IRMA DR	LEROY DR	E 108TH AVE	COLLECTOR	63	975	341	7,169	36	72	60	Mod	Marginal	32	30	47
2253	IRMA DR	E 108TH AVE	MURIEL DR	COLLECTOR	62	693	239	5,012	36	76	60	Mod	Marginal	32	31	49
1679	IRMA DR	MURIEL DR	E 110TH PL	COLLECTOR	62	841	290	6,085	35	63	60	Mod	Marginal	37	28	44
1693	IRMA DR	E 110TH PL	LEROY DR	COLLECTOR	62	333	115	2,405	31	61	60	Mod	Marginal	42	27	40

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)		PCI Override (OPCI)
1762	IRMA DR	LEROY DR	E 112TH AVE	COLLECTOR	61	511	173	3,636	40	54	60	Mod	Marginal	33	26		44
1768	IRMA DR	E 112TH AVE	E 112TH PL	COLLECTOR	37	220	45	949	35	36	60	Mod	Poor	42	23		34
1786	IRMA DR	E 112TH PL	GRAVES CT	COLLECTOR	39	255	55	1,158	100	69	60	Mod	Excellent	0	0		89
1819	IRMA DR	GRAVES CT	E 114TH PL	COLLECTOR	39	784	170	3,568	100	81	60	Mod	Excellent	0	0		93
1839	IRMA DR	E 114TH PL	E 115TH AVE	COLLECTOR	39	364	79	1,655	100	81	60	Mod	Excellent	0	0		93
1865	IRMA DR	E 115TH AVE	MALLEY DR	COLLECTOR	38	645	136	2,862	100	81	60	Mod	Excellent	0	0		93
1898	IRMA DR	MALLEY DR	GILPIN ST	COLLECTOR	38	785	166	3,480	95	75	60	Mod	Excellent	3	2		88
1911	IRMA DR	GILPIN ST	BOWMAN PL	COLLECTOR	39	260	56	1,182	100	65	60	Mod	Excellent	0	0		88
1928	IRMA DR	BOWMAN PL	PHILLIPS DR	COLLECTOR	39	260	56	1,184	100	38	60	Mod	V Good	0	0		79
1940	IRMA DR	PHILLIPS DR	TRUDA DR	COLLECTOR	40	260	58	1,213	95	44	60	Mod	V Good	0	6		77
1951	IRMA DR	TRUDA DR	DEAN DR	COLLECTOR	39	244	53	1,111	100	75	60	Mod	Excellent	0	0		91
1960	IRMA DR	DEAN DR	IRMA DR (CDS)	COLLECTOR	39	105	23	479	100	67	60	Mod	Excellent	0	0		88
1972	IRMA DR	IRMA DR (CDS)	IRMA WAY	COLLECTOR	39	346	75	1,575	100	70	60	Mod	Excellent	0	0		90
1985	IRMA DR	IRMA WAY	IRMA DR (CDS)	COLLECTOR	39	282	61	1,282	100	68	60	Mod	Excellent	0	0		89
1987	IRMA DR	IRMA DR (CDS)	E 119TH PL	COLLECTOR	38	280	59	1,241	100	70	60	Mod	Excellent	0	0		89
2008	IRMA DR	E 119TH PL	E 119TH PL	COLLECTOR	38	127	27	562	100	54	60	Mod	V Good	0	0		84
2024	IRMA DR	E 119TH PL	E 120TH AVE	COLLECTOR	39	202	44	920	100	39	60	Mod	V Good	0	0		79
2031	IRMA DR	E 120TH AVE	NORTH END	LOCAL	42	2,524	589	12,368	94	74	60	Mod	Excellent	6	0		87
1961	IRMA DR (CDS)	IRMA DR	EAST END	LOCAL	51	164	47	984	47	41	60	Mod	Marginal	31	22		44
1986	IRMA DR (CDS)	EAST END	IRMA DR	LOCAL	78	90	39	822	41	13	80	Strng	Poor	7	52		31
1979	IRMA WAY	IRMA DR	HUMBOLDT DR	LOCAL	38	265	56	1,174	82	40	80	Strng	Good	1	16		68
1428	JANICE CT	MURIEL DR	GRANT DR	LOCAL	39	545	118	2,479	65	46	80	Strng	Fair	16	19		58
5005	JAYCEE PARK LOT	0	0	PARKING LOT	34	1,345	251	5,278	50	43	60	Mod	Marginal	30	19		47
1661	JOSEPHINE ST	E 109TH DR	E 110TH DR	LOCAL	35	461	90	1,882	63	53	80	Strng	Fair	8	29		59
1697	JOSEPHINE WAY	E 110TH PL	E 111TH DR	LOCAL	35	278	54	1,133	72	39	80	Strng	Good	8	20		60
1746	JOSEPHINE WAY	E 111TH DR	E 111TH DR	LOCAL	35	394	77	1,610	57	56	80	Strng	Fair	16	27		56
1224	KALAMATH ST	SANTA FE ST	NAIAD DR	LOCAL	39	1,095	237	4,982	48	63	60	Mod	Fair	30	23		52
1346	KALAMATH ST	ROSEANNA DR	KENNEDY DR	LOCAL	39	948	205	4,315	44	57	80	Strng	Marginal	24	33		47
1312	KENNEDY DR	ACOMA ST	MELODY DR	LOCAL	38	1,212	256	5,374	100	73	60	Mod	Excellent	0	0		91
1333	KENNEDY DR	MELODY DR	HURON ST	LOCAL	39	1,149	249	5,230	83	63	60	Mod	V Good	11	6		76
1332	KENNEDY DR	HURON ST	ROSEANNA DR	LOCAL	38	206	43	911	98	40	60	Mod	V Good	0	0		78
1340	KENNEDY DR	ROSEANNA DR	SANTA FE ST	LOCAL	39	540	117	2,459	100	69	60	Mod	Excellent	0	0		89
1347	KENNEDY DR	SANTA FE ST	KALAMATH ST	LOCAL	40	283	63	1,321	100	63	60	Mod	Excellent	0	0		87
1357	KENNEDY DR	KALAMATH ST	LIVINGSTON DR	LOCAL	39	294	64	1,339	100	80	60	Mod	Excellent	0	0		93
1375	KENNEDY DR	LIVINGSTON DR	CLAIRE LN	LOCAL	39	1,306	283	5,941	100	82	60	Mod	Excellent	0	0		93
1374	KENNEDY DR	CLAIRE LN	SW END	LOCAL	39	357	77	1,626	45	41	60	Mod	Marginal	35	20		43
1929	KEOUGH DR	DEAN DR	MONTE WAY	LOCAL	39	948	205	4,313	100	82	60	Mod	Excellent	0	0		93
1950	KEOUGH DR	MONTE WAY	TRUDA DR	LOCAL	39	638	138	2,902	100	75	60	Mod	Excellent	0	0		91
1964	KEOUGH DR	TRUDA DR	HUMBOLDT DR	LOCAL	38	430	91	1,906	100	88	60	Mod	Excellent	0	0		95
2002	KEOUGH DR	HUMBOLDT DR	E 119TH PL	LOCAL	38	805	170	3,570	100	79	60	Mod	Excellent	0	0		93
2137	KIMBLEWYCK CIR	FOX RUN PKWY	KIMBLEWYCK CIR (W)	LOCAL	25	143	20	417	64	29	80	Strng	Fair	19	17		52
2153	KIMBLEWYCK CIR	FOX RUN CIR	KIMBLEWYCK CIR (N)	LOCAL	23	320	41	859	44	38	80	Strng	Marginal	27	29		41
2177	KIMBLEWYCK CIR	KIMBLEWYCK CIR (N)	FOX RUN CIR	LOCAL	23	130	17	348	62	22	80	Strng	Marginal	13	24		48

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2144	KIMBLEWYCK CIR	KIMBLEWYCK CIR (E)	FOX RUN CIR	LOCAL	22	129	16	331	66	33	80	Strng	Fair	13	21	54
2134	KIMBLEWYCK CIR	FOX RUN CIR	KIMBLEWYCK CIR (E)	LOCAL	23	305	39	818	19	42	80	Strng	Poor	45	24	26
2142	KIMBLEWYCK CIR (E)	KIMBLEWYCK CIR (S)	KIMBLEWYCK CIR	LOCAL	23	182	23	488	20	24	80	Strng	V Poor	50	31	20
2154	KIMBLEWYCK CIR (E)	KIMBLEWYCK CIR	KIMBLEWYCK CIR (N)	LOCAL	23	175	22	470	17	15	80	Strng	V Poor	52	31	16
2158	KIMBLEWYCK CIR (N)	KIMBLEWYCK CIR	KIMBLEWYCK CIR	LOCAL	23	198	25	532	22	32	60	Mod	Poor	50	29	25
2157	KIMBLEWYCK CIR (N)	KIMBLEWYCK CIR	KIMBLEWYCK CIR (E)	LOCAL	22	324	40	831	28	33	60	Mod	Poor	48	24	29
2136	KIMBLEWYCK CIR (S)	KIMBLEWYCK CIR (W)	KIMBLEWYCK CIR (E)	LOCAL	23	510	65	1,368	40	42	60	Mod	Marginal	37	23	40
2151	KIMBLEWYCK CIR (W)	KIMBLEWYCK CIR	KIMBLEWYCK CIR (N)	LOCAL	23	268	34	719	31	28	80	Strng	Poor	40	24	29
2071	LAFAYETTE ST	HUMBOLDT ST	E 105TH PL	LOCAL	38	792	167	3,513	62	53	60	Mod	Fair	17	21	58
1808	LAFAYETTE ST	E 112TH PL	LAFAYETTE ST (CDS)	LOCAL	39	865	187	3,934	54	66	60	Mod	Fair	21	25	58
1811	LAFAYETTE ST	LAFAYETTE ST (CDS)	MARION ST	LOCAL	38	108	23	480	63	49	80	Strng	Fair	10	27	57
2003	LAFAYETTE ST	HUMBOLDT DR	E 119TH PL	LOCAL	39	855	185	3,890	39	62	80	Strng	Marginal	28	33	46
1364	LAMBERT LN	ROSEANNA DR	LIVINGSTON DR	LOCAL	40	966	215	4,508	45	57	80	Strng	Marginal	24	28	49
1420	LAMBERT LN	LIVINGSTON DR	CLAIRE LN	LOCAL	37	1,083	223	4,675	95	62	60	Mod	V Good	3	3	83
2196	LARRY DR	LEROY DR	E 108TH AVE	LOCAL	38	934	197	4,141	56	57	80	Strng	Fair	12	32	56
2223	LARRY DR	E 108TH AVE	DOWNING CT	LOCAL	39	456	99	2,074	55	53	80	Strng	Fair	18	27	54
2238	LARRY DR	DOWNING CT	E 108TH PL	LOCAL	39	551	119	2,509	61	63	80	Strng	Good	8	31	61
2247	LARRY DR	E 108TH PL	MURIEL DR	LOCAL	38	798	168	3,537	50	64	80	Strng	Fair	19	31	54
1636	LARRY DR	MURIEL DR	LARSON DR	LOCAL	39	828	179	3,767	52	56	80	Strng	Fair	18	30	53
2127	LARSON DR	E 105TH PL	LEROY DR	LOCAL	39	539	117	2,454	100	55	60	Mod	V Good	0	0	85
2159	LARSON DR	LEROY DR	GARLAND DR	LOCAL	38	432	91	1,917	100	73	60	Mod	Excellent	0	0	91
2205	LARSON DR	GARLAND DR	E 108TH AVE	LOCAL	39	556	120	2,529	100	61	60	Mod	Excellent	0	0	87
2228	LARSON DR	E 108TH AVE	MILDRED DR	LOCAL	38	249	53	1,106	100	64	60	Mod	Excellent	0	0	88
2260	LARSON DR	MILDRED DR	MURIEL DR	LOCAL	37	1,503	309	6,488	100	66	60	Mod	Excellent	0	0	88
1635	LARSON DR	MURIEL DR	LARRY DR	LOCAL	38	244	52	1,082	26	48	80	Strng	Poor	37	37	33
1657	LARSON DR	LARRY DR	ROSALIE DR	LOCAL	37	257	53	1,110	29	66	80	Strng	Marginal	31	40	41
1690	LARSON DR	ROSALIE DR	E 110TH PL	LOCAL	38	316	67	1,402	38	65	60	Mod	Marginal	31	32	46
1707	LARSON DR	E 110TH PL	ELMER DR	LOCAL	39	251	54	1,142	36	49	80	Strng	Marginal	22	31	40
1735	LARSON DR	ELMER DR	E 111TH PL	LOCAL	39	249	54	1,133	47	54	60	Mod	Marginal	27	26	49
1764	LARSON DR	E 111TH PL	E 112TH AVE	LOCAL	38	194	41	862	56	30	80	Strng	Marginal	22	22	47
1774	LARSON DR	E 112TH AVE	E 112TH PL	LOCAL	38	211	45	936	41	45	60	Mod	Marginal	33	26	42
1836	LARSON LN	E 112TH PL	MALLEY DR	LOCAL	39	1,345	291	6,121	48	72	60	Mod	Fair	28	24	55
1847	LARSON LN	MALLEY DR	E 114TH PL	LOCAL	39	255	55	1,160	80	60	60	Mod	V Good	11	9	73
1875	LARSON LN	E 114TH PL	E 115TH AVE	LOCAL	39	571	124	2,598	93	82	60	Mod	Excellent	0	7	89
1882	LARSON LN	E 115TH AVE	E 115TH PL	LOCAL	38	250	53	1,106	93	76	60	Mod	Excellent	0	7	87
1893	LARSON LN	E 115TH PL	PHILLIPS DR	LOCAL	38	363	77	1,609	93	72	60	Mod	Excellent	0	7	86
1944	LAVINIA LN	CLAUDE CT	GAYLORD WAY	LOCAL	39	263	57	1,196	51	74	60	Mod	Fair	26	23	58
1970	LAVINIA LN	GAYLORD WAY	MAIDEN WAY	LOCAL	38	967	204	4,287	51	72	60	Mod	Fair	25	24	57
1981	LAVINIA LN	MAIDEN WAY	LAVINIA WAY	LOCAL	39	252	55	1,145	53	73	60	Mod	Good	23	24	59
2012	LAVINIA LN	LAVINIA WAY	E 119TH PL	LOCAL	38	476	101	2,111	61	66	60	Mod	Good	16	23	62
2010	LAVINIA WAY	LAVINIA LN	E 119TH PL	LOCAL	38	737	156	3,267	72	57	60	Mod	Good	10	18	67
1403	LEONARD LN	NORTHGLENN DR	LINDA SUE LN	LOCAL	39	1,269	275	5,776	63	66	60	Mod	Good	14	22	64
1393	LEONARD LN	LINDA SUE LN	GRANT DR	LOCAL	39	253	55	1,151	68	57	60	Mod	Good	11	21	64

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)
1381	LEONARD LN	GRANT DR	PEARL CT	LOCAL	38	700	148	3,104	57	64	60	Mod	Fair	19	24	59
2243	LEONARD LN	PEARL CT	PEARL ST	LOCAL	39	253	55	1,152	62	71	60	Mod	Good	18	20	64
2128	LEROY DR	LARSON DR	LARRY DR	LOCAL	39	256	55	1,163	100	41	60	Mod	V Good	0	0	80
2126	LEROY DR	LARRY DR	OGDEN ST	LOCAL	38	248	52	1,101	100	77	60	Mod	Excellent	0	0	92
2123	LEROY DR	OGDEN ST	CORONA ST	LOCAL	38	245	52	1,088	100	80	60	Mod	Excellent	0	0	93
2120	LEROY DR	CORONA ST	DOWNING ST	LOCAL	38	280	59	1,241	100	82	60	Mod	Excellent	0	0	94
2119	LEROY DR	DOWNING ST	LOREN LN	LOCAL	38	248	52	1,099	100	74	60	Mod	Excellent	0	0	91
2118	LEROY DR	LOREN LN	MARION ST	LOCAL	38	244	51	1,081	100	64	60	Mod	Excellent	0	0	88
2121	LEROY DR	MARION ST	CORING PL	LOCAL	37	899	185	3,880	92	75	60	Mod	Excellent	0	8	86
2135	LEROY DR	CORING PL	ROSALIE DR	LOCAL	38	298	63	1,323	100	77	60	Mod	Excellent	0	0	92
2141	LEROY DR	ROSALIE DR	MURRAY DR	LOCAL	37	229	47	987	96	74	60	Mod	Excellent	0	5	88
2155	LEROY DR	MURRAY DR	IRMA DR	LOCAL	38	409	86	1,813	70	55	60	Mod	Good	21	9	65
2219	LEROY DR	IRMA DR	DS@1046FT	LOCAL	62	1,046	360	7,566	22	42	80	Strng	Poor	39	22	28
2222	LEROY DR	DS@1046FT	DS@1135FT	LOCAL	62	89	31	644	30	62	60	Mod	Marginal	38	32	40
1001	LEROY DR	DS@1135FT	IRMA DR	LOCAL	62	2,639	909	19,087	21	41	80	Strng	Poor	41	18	27
1197	LINCOLN CT	E 104TH PL	E 105TH AVE	LOCAL	37	335	69	1,447	59	61	60	Mod	Fair	16	25	59
1498	LINCOLN DR	E 113TH PL	MALLEY DR	LOCAL	34	616	116	2,442	25	38	60	Mod	Poor	50	25	28
1200	LINCOLN ST	E 104TH PL	E 105TH AVE	LOCAL	38	410	86	1,815	71	47	60	Mod	Good	12	16	63
1249	LINCOLN ST	E 106TH AVE	GARLAND DR	LOCAL	38	237	50	1,050	69	65	60	Mod	Good	17	14	67
1263	LINCOLN ST	GARLAND DR	E 106TH PL	LOCAL	38	250	53	1,109	66	65	60	Mod	Good	19	15	65
1268	LINCOLN ST	E 106TH PL	E 107TH AVE	LOCAL	38	247	52	1,096	74	66	60	Mod	V Good	8	18	71
1292	LINCOLN ST	E 107TH AVE	E 107TH PL	LOCAL	38	251	53	1,111	72	59	60	Mod	Good	12	16	67
1307	LINCOLN ST	E 107TH PL	E 108TH AVE	LOCAL	38	267	56	1,184	65	78	60	Mod	Good	15	20	69
1334	LINCOLN ST	E 108TH AVE	NORTHGLENN DR	LOCAL	39	473	103	2,153	72	57	60	Mod	Good	11	17	67
1543	LINCOLN ST	E 116TH AVE	E 117TH AVE	LOCAL	32	444	79	1,657	51	48	80	Strng	Marginal	22	27	49
1568	LINCOLN ST	E 117TH PL	E 118TH AVE	LOCAL	36	531	106	2,231	44	55	80	Strng	Marginal	26	30	47
1591	LINCOLN ST	E 118TH AVE	E 118TH PL	LOCAL	35	366	71	1,493	40	64	80	Strng	Marginal	22	38	47
1418	LINDA SUE LN	NORTHGLENN DR	LEONARD LN	LOCAL	39	1,562	338	7,106	59	63	60	Mod	Good	17	24	60
1236	LIPAN ST	W 104TH PL	NAIAD DR	LOCAL	40	1,150	256	5,366	88	73	60	Mod	V Good	9	4	82
1183	LIVINGSTON DR	W 104TH AVE	W 104TH PL	LOCAL	38	211	45	937	24	49	80	Strng	Poor	35	26	31
1226	LIVINGSTON DR	W 104TH PL	W 106TH AVE	LOCAL	39	970	210	4,415	38	52	60	Mod	Marginal	32	24	42
1256	LIVINGSTON DR	W 106TH AVE	NAIAD DR	LOCAL	40	466	104	2,176	39	54	80	Strng	Marginal	29	23	44
1279	LIVINGSTON DR	NAIAD DR	ROSEANNA DR	LOCAL	39	558	121	2,540	31	57	60	Mod	Poor	38	26	39
1338	LIVINGSTON DR	ROSEANNA DR	BETH LN	LOCAL	39	812	176	3,694	41	69	60	Mod	Fair	27	30	50
1356	LIVINGSTON DR	BETH LN	KENNEDY DR	LOCAL	38	279	59	1,235	39	48	80	Strng	Marginal	28	33	41
1363	LIVINGSTON DR	KENNEDY DR	LAMBERT LN	LOCAL	39	311	67	1,415	35	50	80	Strng	Marginal	31	34	39
1410	LIVINGSTON DR	LAMBERT LN	CLAIRE LN	LOCAL	37	923	190	3,985	100	64	60	Mod	Excellent	0	0	88
1448	LIVINGSTON DR	W 111TH AVE	W 111TH PL	LOCAL	35	270	53	1,104	37	47	80	Strng	Marginal	33	30	40
1462	LIVINGSTON DR	W 111TH PL	W 112TH AVE	LOCAL	34	206	39	816	44	35	80	Strng	Marginal	29	27	41
1304	LOGAN CT	GARLAND DR	NORTHGLENN DR	LOCAL	38	1,034	218	4,586	63	61	60	Mod	Good	16	21	61
1531	LOGAN ST	SOUTH END	E 116TH AVE	LOCAL	28	164	26	536	73	62	60	Mod	Good	6	22	69
1549	LOGAN ST	E 116TH AVE	E 117TH AVE	LOCAL	29	446	72	1,509	58	50	80	Strng	Fair	13	29	54
1563	LOGAN ST	WEST END	LOGAN ST	LOCAL	42	38	9	185	88	49	60	Mod	V Good	0	12	75

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)			
1564	LOGAN ST	LOGAN ST	EAST END	LOCAL	42	37	9	179	73	57	80	Strng	Good	0	27						67
1567	LOGAN ST	LOGAN ST	E 118TH AVE	LOCAL	35	315	61	1,285	95	42	60	Mod	V Good	0	5						77
2172	LOREN LN	LEROY DR	CORING PL	LOCAL	39	1,318	286	5,997	31	70	80	Strng	Marginal	29	40						44
1010	LOU DR	SWITZER LN	COVEY CT	LOCAL	39	1,219	264	5,548	62	56	80	Strng	Good	12	26						59
1012	LOU DR	COVEY CT	FRED DR	LOCAL	38	318	67	1,408	70	36	80	Strng	Fair	11	19						58
1029	LOU DR	FRED DR	MELODY DR	LOCAL	38	380	80	1,685	51	44	80	Strng	Marginal	21	28						48
1688	LUCILLE CT	E 110TH PL	MURRAY DR	LOCAL	39	921	200	4,193	79	74	60	Mod	V Good	5	16						77
1067	LUNCEFORD CT	NIVER AVE	W 100TH AVE	LOCAL	38	257	54	1,141	49	45	80	Strng	Marginal	25	27	95	9/30/22				95
1041	LUNCEFORD LN	W 98TH AVE	W 99TH AVE	LOCAL	34	770	146	3,056	53	46	80	Strng	Fair	17	30						50
2060	MADISON WAY	ADAMS ST	FOX RUN PKWY	LOCAL	35	460	89	1,877	65	64	60	Mod	Good	17	18						64
2067	MADISON WAY	FOX RUN PKWY	E 105TH CT	LOCAL	35	277	54	1,130	41	44	80	Strng	Marginal	29	30						41
2111	MADISON WAY	E 105TH CT	E 106TH CT	LOCAL	35	395	77	1,611	51	45	60	Mod	Marginal	31	17						49
2124	MADISON WAY	E 106TH CT	E 106TH PL	LOCAL	36	223	45	938	62	68	60	Mod	Good	19	20						63
2169	MADISON WAY	E 107TH AVE	E 107TH CT	LOCAL	35	220	43	900	55	63	60	Mod	Fair	26	19						57
2193	MADISON WAY	E 107TH CT	E 108TH AVE	LOCAL	35	352	68	1,438	59	55	60	Mod	Fair	21	20						57
1969	MAIDEN WAY	TRUDA DR	LAVINIA LN	LOCAL	39	570	124	2,594	43	49	60	Mod	Marginal	35	22	95	9/30/22				95
1501	MALLEY DR	COMMUNITY CENTER DR	LINCOLN DR	COLLECTOR	63	543	190	3,988	98	69	60	Mod	Excellent	0	2						87
1499	MALLEY DR	LINCOLN DR	HIGHLINE DR	COLLECTOR	62	538	185	3,893	100	74	60	Mod	Excellent	0	0						91
1497	MALLEY DR	HIGHLINE DR	HIGHLINE DR	COLLECTOR	48	31	8	175	88	53	60	Mod	V Good	0	12						76
1495	MALLEY DR	HIGHLINE DR	GRANT DR	COLLECTOR	62	653	225	4,726	97	65	60	Mod	Excellent	2	2						86
1825	MALLEY DR	GRANT DR	PEARL ST	COLLECTOR	61	617	209	4,394	87	69	60	Mod	V Good	12	2						80
1824	MALLEY DR	PEARL ST	WASHINGTON ST	COLLECTOR	73	355	144	3,019	100	62	60	Mod	Excellent	0	0						87
1823	MALLEY DR	WASHINGTON ST	CLARKSON ST	LOCAL	43	213	51	1,067	30	36	60	Mod	Poor	45	25						31
1835	MALLEY DR	CLARKSON ST	LARSON LN	LOCAL	40	1,027	228	4,794	28	67	60	Mod	Marginal	50	23						40
1842	MALLEY DR	LARSON LN	OGDEN ST	LOCAL	38	248	52	1,101	31	66	60	Mod	Marginal	44	25						42
1845	MALLEY DR	OGDEN ST	DOWNING ST	LOCAL	39	261	57	1,187	30	74	60	Mod	Marginal	46	24						44
1849	MALLEY DR	DOWNING ST	MARION ST	LOCAL	38	249	53	1,104	39	77	60	Mod	Fair	40	21						51
1852	MALLEY DR	MARION ST	FOWLER DR	LOCAL	38	251	53	1,113	36	76	60	Mod	Marginal	43	21						49
1863	MALLEY DR	FOWLER DR	HUMBOLDT ST	LOCAL	39	260	56	1,183	31	74	60	Mod	Marginal	44	24						45
1864	MALLEY DR	HUMBOLDT ST	IRMA DR	LOCAL	39	265	57	1,206	30	68	60	Mod	Marginal	48	22						42
1366	MANET WAY	ROSEANNA DR	DS@282FT	LOCAL	38	282	59	1,249	47	50	60	Mod	Marginal	32	21						47
1406	MANET WAY	DS@302FT	CLAIRE LN	LOCAL	37	910	187	3,930	53	51	60	Mod	Fair	23	24						52
2051	MARION ST	E 104TH AVE	DS@52FT	LOCAL	44	52	13	268	53	22	80	Strng	Marginal	26	22	95	9/30/22				95
2053	MARION ST	DS@52FT	MARION WAY	LOCAL	45	160	40	839	41	62	80	Strng	Marginal	27	32	95	9/30/22				95
2075	MARION ST	MARION WAY	E 105TH PL	LOCAL	38	704	149	3,123	54	51	80	Strng	Fair	20	27	95	9/30/22				95
2117	MARION ST	E 105TH PL	LEROY DR	LOCAL	39	489	106	2,226	43	41	80	Strng	Marginal	29	28	95	9/30/22				95
1801	MARION ST	E 112TH PL	MARION ST (CDS)	LOCAL	39	768	166	3,494	56	70	60	Mod	Good	19	24						60
1807	MARION ST	MARION ST (CDS)	LAFAYETTE ST	LOCAL	39	108	23	491	57	65	80	Strng	Fair	11	32						59
1894	MARION ST	MALLEY DR	BOWMAN PL	LOCAL	39	1,147	249	5,220	89	66	60	Mod	V Good	9	3						81
1909	MARION ST	BOWMAN PL	PHILLIPS DR	LOCAL	38	263	55	1,164	96	65	60	Mod	Excellent	4	0						85
1806	MARION ST (CDS)	NW END	MARION ST	LOCAL	85	34	16	335	77	25	80	Strng	Fair	0	23						59
2077	MARION WAY	MARION ST	E 105TH PL	LOCAL	39	922	200	4,197	63	56	80	Strng	Good	13	24						60
1966	MCCRUMB DR	SW END	TRUDA DR	LOCAL	44	440	107	2,253	88	55	60	Mod	V Good	5	7						76

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)		PCI Override (OPCI)	OPCI Date					
1996	MCCRUMB DR	TRUDA DR	E 119TH PL	LOCAL	40	800	178	3,732	87	70	60	Mod	V Good	3	10							81	
1034	MELODY CIR	SOUTH END	MELODY DR	LOCAL	83	118	54	1,142	82	58	60	Mod	V Good	3	15								74
1031	MELODY DR	W 99TH AVE	LOU DR	LOCAL	63	399	140	2,931	100	71	60	Mod	Excellent	0	0								90
1035	MELODY DR	LOU DR	MELODY CIR	LOCAL	63	884	309	6,496	95	81	60	Mod	Excellent	3	3								89
1038	MELODY DR	MELODY CIR	HURON ST	LOCAL	62	578	199	4,178	88	65	60	Mod	V Good	6	6								80
1056	MELODY DR	W 99TH AVE	BELFORD DR	LOCAL	63	1,148	402	8,438	89	82	60	Mod	Excellent	1	10								86
1071	MELODY DR	BELFORD DR	BRIGITTE DR	LOCAL	62	596	205	4,311	100	64	60	Mod	Excellent	0	0								87
1078	MELODY DR	BRIGITTE DR	BRUCE LN	LOCAL	62	399	138	2,889	100	83	60	Mod	Excellent	0	0								94
1088	MELODY DR	BRUCE LN	BRIGITTE DR	LOCAL	62	296	102	2,138	100	88	60	Mod	Excellent	0	0								95
1099	MELODY DR	BRIGITTE DR	W 100TH PL	LOCAL	63	348	122	2,557	100	66	60	Mod	Excellent	0	0								88
1123	MELODY DR	W 100TH PL	MONTEREY CIR	LOCAL	64	1,255	446	9,371	98	77	60	Mod	Excellent	0	2								91
1135	MELODY DR	MONTEREY CIR	MONTEREY CIR	LOCAL	62	471	162	3,404	85	86	60	Mod	Excellent	10	5								85
1146	MELODY DR	MONTEREY CIR	W 104TH AVE	LOCAL	62	218	75	1,580	97	38	60	Mod	V Good	3	0								77
1232	MELODY DR	W 104TH AVE	W 106TH AVE	LOCAL	62	1,194	411	8,639	68	62	60	Mod	Good	0	0								66
1306	MELODY DR	W 106TH AVE	MELODY DR	LOCAL	61	1,318	447	9,383	71	67	60	Mod	Good	0	0								69
1325	MELODY DR	KENNEDY DR	WELLINGTON ST	LOCAL	14	275	21	449	74	52	60	Mod	Good	9	17	95	9/30/22						95
1350	MELODY DR	DS@65FT	PATTERSON CT	LOCAL	62	223	77	1,613	28	73	60	Mod	Marginal	39	33	95	9/30/22						95
1384	MELODY DR	PATTERSON CT	BONITA PL	LOCAL	63	812	284	5,969	33	60	60	Mod	Marginal	40	26	95	9/30/22						95
1405	MELODY DR	BONITA PL	PINYON DR	LOCAL	63	344	120	2,530	40	55	60	Mod	Marginal	34	26	95	9/30/22						95
1422	MELODY DR	PINYON DR	ELATI ST	LOCAL	63	291	102	2,136	53	87	60	Mod	Good	22	25	95	9/30/22						95
1461	MELODY DR	ELATI ST	W 112TH AVE	LOCAL	61	969	328	6,897	48	65	60	Mod	Fair	26	26	95	9/30/22						95
1336	MELODY DR	MELODY DR	WELLINGTON ST	LOCAL	56	66	21	431	35	59	60	Mod	Marginal	39	26								43
1327	MELODY DR	WELLINGTON ST	KENNEDY DR	LOCAL	13	276	20	419	56	59	60	Mod	Fair	21	23								56
1314	MELODY DR	KENNEDY DR	MELODY DR	LOCAL	23	60	8	162	100	57	60	Mod	Excellent	0	0								85
1491	MELODY DR	SOUTH END	W 114TH AVE	LOCAL	42	451	105	2,210	54	40	80	Strng	Marginal	23	22								49
1505	MELODY DR	W 114TH AVE	W 114TH PL	LOCAL	34	467	88	1,852	40	56	80	Strng	Marginal	29	31								44
1516	MELODY DR	W 114TH PL	W 114TH WAY	LOCAL	34	260	49	1,033	42	64	60	Mod	Marginal	31	26								49
1520	MELODY DR	W 114TH WAY	W 115TH AVE	LOCAL	35	162	32	662	44	34	80	Strng	Marginal	27	29								40
1538	MELODY DR	W 116TH AVE	W 116TH PL	LOCAL	35	320	62	1,305	54	53	80	Strng	Fair	20	26								53
1558	MELODY DR	W 116TH PL	W 117TH AVE	LOCAL	14	472	37	770	45	43	80	Strng	Marginal	28	27								44
1581	MELODY DR	W 117TH AVE	W 118TH AVE	LOCAL	38	623	131	2,761	35	56	60	Mod	Marginal	36	29								41
1590	MELODY DR	W 118TH AVE	NORTH END	LOCAL	39	131	28	596	63	28	80	Strng	Fair	17	20								51
2227	MILDRED DR	CLARKSON ST	LARSON DR	LOCAL	40	339	75	1,583	72	60	80	Strng	Good	5	23								67
2256	MILDRED DR	LARSON DR	MURIEL DR	LOCAL	38	1,535	324	6,806	57	66	60	Mod	Good	19	23								60
2140	MILWAUKEE CT	E 106TH PL	NE END	LOCAL	46	139	36	751	69	39	80	Strng	Fair	6	26								58
2145	MILWAUKEE ST	E 106TH AVE	E 106TH PL	LOCAL	35	252	49	1,028	48	53	80	Strng	Marginal	23	29								49
2166	MILWAUKEE ST	E 106TH PL	FOX RUN PKWY	LOCAL	34	268	51	1,062	56	30	80	Strng	Marginal	14	30								46
2180	MILWAUKEE ST	FOX RUN PKWY	STEELE ST	LOCAL	34	158	30	626	75	30	80	Strng	Good	0	25								60
2204	MILWAUKEE ST	STEELE ST	E 108TH AVE	LOCAL	35	414	80	1,690	71	58	60	Mod	Good	10	19								66
2229	MILWAUKEE ST	E 108TH AVE	STEELE ST	LOCAL	34	366	69	1,452	78	54	80	Strng	V Good	3	19								69
1914	MONTE WAY	PHILLIPS DR	DEAN DR	LOCAL	39	260	56	1,182	79	33	80	Strng	Good	11	10								63
1930	MONTE WAY	DEAN DR	KEOUGH DR	LOCAL	39	257	56	1,168	74	38	60	Mod	Good	15	11								61
1132	MONTEREY CIR	MELODY DR	FOX CT	LOCAL	39	1,776	385	8,081	53	65	60	Mod	Fair	20	27								56

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1134	MONTEREY CIR	FOX CT	MELODY DR	LOCAL	39	264	57	1,200	49	45	80	Strng	Marginal	25	27						47
1430	MURIEL DR	NORTHGLENN DR	JANICE CT	LOCAL	38	1,006	212	4,459	40	70	60	Mod	Marginal	32	28						49
1423	MURIEL DR	JANICE CT	GRANT DR	LOCAL	39	475	103	2,162	32	53	60	Mod	Poor	43	25						38
1416	MURIEL DR	GRANT DR	PEARL WAY	LOCAL	38	257	54	1,140	33	52	60	Mod	Poor	46	22						39
1672	MURIEL DR	PEARL WAY	PEARL ST	LOCAL	39	541	117	2,464	39	66	60	Mod	Marginal	32	29						47
1673	MURIEL DR	PEARL ST	WASHINGTON ST	LOCAL	39	181	39	825	33	32	80	Strng	Poor	37	30						32
1675	MURIEL DR	WASHINGTON ST	CLARKSON ST	LOCAL	40	222	49	1,035	29	49	60	Mod	Poor	58	14						35
1676	MURIEL DR	CLARKSON ST	CARLILE ST	LOCAL	39	250	54	1,139	37	78	60	Mod	Fair	36	27						50
1674	MURIEL DR	CARLILE ST	EMERSON CIR	LOCAL	38	245	52	1,087	34	58	60	Mod	Marginal	45	21						41
1664	MURIEL DR	EMERSON ST	OGDEN ST	LOCAL	37	244	50	1,052	25	47	60	Mod	Poor	44	31						32
1651	MURIEL DR	OGDEN ST	ALVIN DR	LOCAL	38	248	52	1,097	43	73	30	Weak	Fair	42	15						52
1642	MURIEL DR	ALVIN DR	E 109TH PL	LOCAL	38	248	52	1,100	40	46	80	Strng	Marginal	30	31						41
1634	MURIEL DR	E 109TH PL	LARSON DR	LOCAL	39	256	56	1,166	27	58	80	Strng	Poor	37	35						37
1628	MURIEL DR	LARSON DR	MILDRED DR	LOCAL	39	254	55	1,155	39	51	60	Mod	Marginal	39	21						42
2257	MURIEL DR	MILDRED DR	LARRY DR	LOCAL	39	477	103	2,169	40	76	60	Mod	Fair	40	20						52
2248	MURIEL DR	LARRY DR	ROSALIE DR	LOCAL	38	250	53	1,108	46	56	60	Mod	Marginal	37	17						48
2249	MURIEL DR	ROSALIE DR	MURRAY DR	LOCAL	39	250	54	1,136	40	60	60	Mod	Marginal	32	28						46
2252	MURIEL DR	MURRAY DR	IRMA DR	LOCAL	38	410	86	1,815	36	56	80	Strng	Marginal	31	33						41
2221	MURRAY DR	LEROY DR	E 108TH AVE	LOCAL	38	1,029	217	4,564	72	69	60	Mod	V Good	7	21						70
2250	MURRAY DR	E 108TH AVE	MURIEL DR	LOCAL	39	829	180	3,773	64	58	60	Mod	Good	19	17						61
1650	MURRAY DR	MURIEL DR	LUCILLE CT	LOCAL	38	589	124	2,610	69	61	60	Mod	Good	10	21						66
1677	MURRAY DR	LUCILLE CT	E 110TH PL	LOCAL	39	250	54	1,138	76	62	80	Strng	V Good	3	21						70
1717	MURRAY DR	E 110TH PL	E 111TH PL	LOCAL	39	517	112	2,353	65	59	80	Strng	Good	12	23						62
1211	NAIAD DR	HURON ST	INCA ST	LOCAL	39	357	77	1,624	31	57	60	Mod	Marginal	36	28						39
1219	NAIAD DR	INCA ST	SANTA FE ST	LOCAL	39	280	61	1,272	27	63	60	Mod	Poor	52	21						38
1225	NAIAD DR	SANTA FE ST	KALAMATH ST	LOCAL	38	303	64	1,343	32	60	60	Mod	Marginal	38	30						41
1237	NAIAD DR	KALAMATH ST	LIPAN ST	LOCAL	39	277	60	1,259	34	71	60	Mod	Marginal	34	26						46
1247	NAIAD DR	LIPAN ST	RONALD LN	LOCAL	39	266	58	1,211	41	66	60	Mod	Marginal	33	21						49
1257	NAIAD DR	RONALD LN	LIVINGSTON DR	LOCAL	39	289	63	1,315	32	47	60	Mod	Poor	41	27						36
1446	NAVAJO ST	W 111TH AVE	W 111TH PL	LOCAL	34	492	93	1,952	52	50	60	Mod	Fair	24	24						50
1077	NIVER AVE	W 100TH PL	W 100TH AVE	LOCAL	39	622	135	2,830	73	74	60	Mod	V Good	3	25						73
1063	NIVER AVE	W 100TH AVE	HURON ST	LOCAL	38	603	127	2,672	68	65	60	Mod	Good	9	23						67
1060	NIVER AVE	HURON ST	LUNCEFORD CT	LOCAL	35	765	149	3,125	34	63	60	Mod	Marginal	40	26	95	9/30/22				95
1062	NIVER AVE	LUNCEFORD CT	DARWIN CT	LOCAL	40	329	73	1,537	39	68	60	Mod	Marginal	27	34	95	9/30/22				95
1064	NIVER AVE	DARWIN CT	CROKE DR	LOCAL	40	470	104	2,194	34	56	80	Strng	Marginal	31	35	95	9/30/22				95
1305	NORTHGLENN DR	GARLAND DR	LOGAN CT	LOCAL	39	1,024	222	4,658	52	65	60	Mod	Fair	22	25						56
1318	NORTHGLENN DR	LOGAN CT	GRANT DR	LOCAL	40	249	55	1,164	57	37	80	Strng	Fair	21	22						49
1335	NORTHGLENN DR	GRANT DR	LINCOLN ST	LOCAL	39	617	134	2,809	95	69	60	Mod	Excellent	0	5						86
1341	NORTHGLENN DR	LINCOLN ST	DS@155FT	LOCAL	39	155	34	704	100	77	60	Mod	Excellent	0	0						92
1349	NORTHGLENN DR	DS@155FT	NORTHGLENN DR (CDS)	LOCAL	39	122	26	554	100	70	60	Mod	Excellent	0	0						90
1376	NORTHGLENN DR	NORTHGLENN DR (CDS)	E 109TH AVE	LOCAL	39	595	129	2,707	100	74	60	Mod	Excellent	0	0						91
1394	NORTHGLENN DR	E 109TH AVE	E 109TH PL	LOCAL	38	251	53	1,113	100	84	60	Mod	Excellent	0	0						94
1402	NORTHGLENN DR	E 109TH PL	LEONARD LN	LOCAL	39	250	54	1,139	98	78	60	Mod	Excellent	0	2						91

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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)					
1417	NORTHGLENN DR	LEONARD LN	LINDA SUE LN	LOCAL	39	250	54	1,138	99	80	60	Mod	Excellent	0	1								93
1429	NORTHGLENN DR	LINDA SUE LN	MURIEL DR	LOCAL	39	321	69	1,459	98	77	60	Mod	Excellent	0	2								91
1439	NORTHGLENN DR	MURIEL DR	EMERY RD	LOCAL	39	284	62	1,293	96	74	60	Mod	Excellent	0	4								88
1454	NORTHGLENN DR	EMERY RD	E 111TH PL	LOCAL	39	246	53	1,118	100	51	60	Mod	V Good	0	0								83
1472	NORTHGLENN DR	E 111TH PL	HIGHLINE DR	LOCAL	38	273	58	1,212	100	41	60	Mod	V Good	0	0								80
1348	NORTHGLENN DR (CDS)	NORTHGLENN DR	SW END	LOCAL	74	83	34	713	92	17	80	Strng	Good	0	8								66
5008	NORTHGLENN JUSTICE CENTER	0	0	PARKING LOT	24	3,300	436	9,149	87	44	60	Mod	V Good	6	7								73
5002	GLENN MAINTENANCE AND OPERATIONS	0	0	PARKING LOT	19	1,296	138	2,889	44	33	60	Mod	Marginal	36	21								39
5010	GLENN WASTEWATER TREATMENT PLANT	0	0	PARKING LOT	12	701	46	970	69	44	80	Strng	Good	12	20								60
5009	GLENN WASTEWATER TREATMENT PLANT	0	0	PARKING LOT	14	2,225	168	3,536	68	57	60	Mod	Good	15	17								64
5001	GLENN WATER TREATMENT PLANT	0	0	PARKING LOT	19	1,044	109	2,289	24	28	80	Strng	Poor	43	22								24
5000	GLENN WATER TREATMENT PLANT	0	0	PARKING LOT	24	8,132	1,070	22,468	44	49	60	Mod	Marginal	31	25								45
1436	RTHWEST OPEN SPACE REC AREA	SOUTH END	DS@970FT	LOCAL	25	970	135	2,830	45	59	60	Mod	Marginal	39	16								49
1456	RTHWEST OPEN SPACE REC AREA	DS@970FT	W 112TH AVE	LOCAL	25	387	54	1,128	55	45	80	Strng	Fair	20	24								52
1435	RTHWEST OPEN SPACE REC AREA	NORTHWEST OPEN SPACE REC AREA	WEST END	LOCAL	24	109	14	304	25	44	60	Mod	Poor	52	23								31
1652	OGDEN CIR	EMERSON CIR	MURIEL DR	LOCAL	39	900	195	4,094	88	65	60	Mod	V Good	9	4								80
1789	OGDEN DR	E 112TH PL	CLARKSON ST	LOCAL	38	849	179	3,764	56	56	60	Mod	Fair	21	23								55
2091	OGDEN ST	EMERSON ST	E 105TH PL	LOCAL	39	839	182	3,820	92	72	60	Mod	V Good	6	3								85
2210	OGDEN ST	LEROY DR	REGINA LN	LOCAL	39	1,213	263	5,518	64	56	60	Mod	Good	17	19								61
1709	OGDEN ST	MURIEL DR	ELMER DR	LOCAL	38	673	142	2,982	54	60	80	Strng	Fair	18	28								56
1884	OGDEN ST	MALLEY DR	DOWNING ST	LOCAL	39	1,206	261	5,487	35	59	60	Mod	Marginal	39	25								43
1443	OSAGE ST	W 111TH AVE	W 111TH PL	LOCAL	34	590	112	2,342	55	52	80	Strng	Fair	18	27								54
1240	PALEY ST	POMPEY WAY	W 106TH AVE	LOCAL	39	613	133	2,790	34	50	80	Strng	Poor	35	31								39
1043	PALMER LN	W 98TH AVE	W 99TH AVE	LOCAL	34	769	145	3,051	33	54	60	Mod	Poor	39	28								39
1437	PARK VISTAS DR	W 111TH AVE	W 111TH WAY	LOCAL	35	475	92	1,939	49	62	60	Mod	Fair	22	29								53
1442	PARK VISTAS DR	W 111TH WAY	W 111TH PL	LOCAL	35	181	35	738	65	49	80	Strng	Fair	10	25								59
1386	PATTERSON CT	MELODY DR	HERMOSA CT	LOCAL	39	1,081	234	4,919	51	62	60	Mod	Fair	28	20	95	9/30/22						95
1647	PEARL CIR	PEARL ST	PEARL ST	LOCAL	38	722	152	3,201	63	55	60	Mod	Good	16	21								60
2242	PEARL CT	E 108TH AVE	LEONARD LN	LOCAL	39	735	159	3,344	70	62	60	Mod	Good	13	17								67
2059	PEARL ST	E 104TH PL	PEARL WAY	LOCAL	40	318	71	1,485	72	66	60	Mod	V Good	8	20								69
2084	PEARL ST	PEARL WAY	WASHINGTON WAY	LOCAL	38	365	77	1,618	83	64	60	Mod	V Good	0	17								76
2244	PEARL ST	E 108TH AVE	LEONARD LN	LOCAL	38	734	155	3,256	77	60	80	Strng	V Good	3	20								71
2255	PEARL ST	LEONARD LN	PEARL WAY	LOCAL	39	250	54	1,138	69	44	80	Strng	Good	14	16								60
1633	PEARL ST	PEARL WAY	PEARL CIR	LOCAL	39	249	54	1,133	71	66	60	Mod	Good	11	18								69
1648	PEARL ST	PEARL CIR	PEARL CIR	LOCAL	38	250	53	1,110	70	62	60	Mod	Good	14	16								67
1671	PEARL ST	PEARL CIR	MURIEL DR	LOCAL	39	250	54	1,136	77	42	80	Strng	Good	9	15								65
1713	PEARL ST	MURIEL DR	EMERY RD	LOCAL	39	477	103	2,169	54	61	80	Strng	Fair	14	28								56
1751	PEARL ST	EMERY RD	E 111TH PL	LOCAL	38	273	58	1,208	56	72	60	Mod	Good	14	30								61
1854	PEARL ST	MALLEY DR	E 114TH CT	LOCAL	38	699	148	3,099	27	44	60	Mod	Poor	51	16	95	9/30/22						95
1879	PEARL ST	E 114TH CT	E 115TH AVE	LOCAL	35	551	107	2,249	21	50	80	Strng	Poor	43	36	95	9/30/22						95
1891	PEARL ST	SOUTH END	E 116TH AVE	LOCAL	27	150	23	474	68	54	60	Mod	Good	14	18								63
1916	PEARL ST	E 116TH AVE	E 117TH AVE	LOCAL	28	429	67	1,401	54	43	80	Strng	Fair	22	24								50
2058	PEARL WAY	E 104TH PL	PEARL ST	LOCAL	39	554	120	2,521	78	57	60	Mod	V Good	5	17								71



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									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)		
1668	PEARL WAY	PEARL ST	MURIEL DR	LOCAL	39	1,132	245	5,152	59	58	60	Mod	Fair	19	22					58
1086	PECOS ST	SOUTH END	W 100TH PL	LOCAL	38	250	53	1,106	37	55	80	Strng	Marginal	23	29	95	9/30/22			95
1096	PECOS ST	W 100TH PL	W 101ST AVE	LOCAL	39	271	59	1,235	43	64	60	Mod	Fair	28	28	95	9/30/22			95
1102	PECOS ST	W 101ST AVE	W 101ST PL	LOCAL	39	271	59	1,232	42	68	60	Mod	Fair	27	31	95	9/30/22			95
1107	PECOS ST	W 101ST PL	W 102ND AVE	LOCAL	39	268	58	1,221	38	66	60	Mod	Marginal	31	31	95	9/30/22			95
1114	PECOS ST	W 102ND AVE	W 102ND PL	LOCAL	38	271	57	1,200	25	62	80	Strng	Poor	37	30	95	9/30/22			95
1119	PECOS ST	W 102ND PL	W 103RD AVE	LOCAL	39	270	58	1,228	29	40	80	Strng	Poor	40	31	95	9/30/22			95
1127	PECOS ST	W 103RD AVE	W 103RD PL	LOCAL	39	262	57	1,191	34	63	60	Mod	Marginal	36	31	95	9/30/22			95
1136	PECOS ST	W 103RD PL	W 104TH AVE	LOCAL	39	474	103	2,157	29	43	60	Mod	Poor	41	30	95	9/30/22			95
1296	PECOS ST	URA LN	ROSEANNA DR	LOCAL	38	674	142	2,989	35	45	60	Mod	Poor	48	17					38
1322	PECOS ST	ROSEANNA DR	NORTH END	LOCAL	38	279	59	1,238	41	31	60	Mod	Poor	44	16					37
1003	PELON DR	SOUTH END	SWITZER LN	LOCAL	38	113	24	500	57	39	80	Strng	Fair	17	26					50
1011	PELON DR	SWITZER LN	FRED DR	LOCAL	38	1,428	301	6,330	53	53	80	Strng	Fair	21	26					53
2089	PENNSYLVANIA ST	E 104TH PL	WASHINGTON WAY	LOCAL	39	821	178	3,734	62	53	60	Mod	Fair	17	20					59
1920	PENNSYLVANIA ST	PENNSYLVANIA ST	E 117TH AVE	LOCAL	29	236	38	799	46	40	60	Mod	Marginal	31	23					44
1952	PENNSYLVANIA ST	PENNSYLVANIA ST	E 118TH AVE	LOCAL	35	314	61	1,284	42	37	80	Strng	Marginal	29	29					39
1892	PHILLIPS DR	WASHINGTON ST	LARSON LN	LOCAL	38	936	198	4,149	40	64	60	Mod	Marginal	27	31	95	9/30/22			95
1905	PHILLIPS DR	LARSON LN	MONTE WAY	LOCAL	38	398	84	1,765	44	73	60	Mod	Fair	23	33	95	9/30/22			95
1908	PHILLIPS DR	MONTE WAY	MARION ST	LOCAL	38	185	39	819	40	78	60	Mod	Fair	28	32	95	9/30/22			95
1927	PHILLIPS DR	MARION ST	IRMA DR	LOCAL	39	1,111	241	5,054	48	68	60	Mod	Fair	24	28	95	9/30/22			95
1934	PHILLIPS DR	IRMA DR	WYCO DR	LOCAL	38	821	173	3,641	83	70	60	Mod	V Good	8	9					79
1936	PHILLIPS DR	WYCO DR	HIGH ST	LOCAL	39	344	75	1,565	93	65	60	Mod	V Good	0	7					83
1939	PHILLIPS DR	HIGH ST	E 117TH WAY	LOCAL	39	545	118	2,481	89	87	60	Mod	Excellent	3	8					88
1932	PHILLIPS DR	E 117TH WAY	CLAUDE CT	LOCAL	39	868	188	3,950	80	64	60	Mod	V Good	10	10					75
1378	PIKE ST	E 109TH AVE	GRANT DR	LOCAL	39	1,208	262	5,494	59	61	60	Mod	Fair	21	21					59
1324	PIKE ST	GRANT DR	CARROL LN	LOCAL	39	643	139	2,925	71	54	60	Mod	Good	10	19					65
1463	PINNACLE CREEK	W 112TH AVE	HURON ST	LOCAL	21	1,272	148	3,117	48	43	60	Mod	Marginal	34	19					45
1414	PINYON DR	MELODY DR	CHEROKEE ST	LOCAL	38	380	80	1,687	84	68	60	Mod	V Good	11	5					78
1449	PINYON DR	CHEROKEE ST	PINYON DR (CDS)	LOCAL	35	909	177	3,713	98	73	60	Mod	Excellent	0	2					89
1452	PINYON DR	PINYON DR (CDS)	ACOMA ST	LOCAL	37	296	61	1,279	93	45	60	Mod	V Good	0	7					77
1450	PINYON DR (CDS)	PINYON DR	WEST END	LOCAL	88	64	31	659	100	56	60	Mod	Excellent	0	0					85
1187	POMPEY WAY	QUIVAS ST	POMPEY WAY (CDS)	LOCAL	39	250	54	1,137	33	32	80	Strng	Poor	36	22					32
1206	POMPEY WAY	POMPEY WAY (CDS)	PALEY ST	LOCAL	38	574	121	2,543	76	47	60	Mod	Good	13	11					66
1231	POMPEY WAY	PALEY ST	QUIVAS ST	LOCAL	39	712	154	3,239	87	69	60	Mod	V Good	3	10					81
1963	QUAM DR	SPRING DR	TRUDA DR	LOCAL	39	1,077	233	4,902	100	71	60	Mod	Excellent	0	0					90
1998	QUAM DR	TRUDA DR	E 119TH PL	LOCAL	38	945	199	4,188	92	65	60	Mod	V Good	6	2					83
1179	QUIVAS ST	W 104TH AVE	POMPEY WAY	LOCAL	40	198	44	926	34	44	80	Strng	Poor	31	28	95	9/30/22			95
1199	QUIVAS ST	POMPEY WAY	QUIVAS ST (CDS)	LOCAL	38	452	95	2,004	39	66	60	Mod	Marginal	31	30	95	9/30/22			95
1230	QUIVAS ST	QUIVAS ST (CDS)	POMPEY WAY	LOCAL	39	623	135	2,836	33	64	60	Mod	Marginal	36	26	95	9/30/22			95
1251	QUIVAS ST	POMPEY WAY	W 106TH AVE	LOCAL	39	435	94	1,980	55	61	60	Mod	Fair	24	22	95	9/30/22			95
1261	QUIVAS ST	W 106TH AVE	PECOS ST	LOCAL	38	271	57	1,202	36	40	60	Mod	Poor	44	20	95	9/30/22			95
1198	QUIVAS ST (CDS)	QUIVAS ST	SW END	LOCAL	51	188	54	1,126	54	30	80	Strng	Marginal	21	25	95	9/30/22			95
2032	RACE ST	E 120TH AVE	KEYSTONE APT ACCESS	LOCAL	24	407	54	1,140	100	52	60	Mod	V Good	0	0					84

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2034	RACE ST	KEYSTONE APT ACCESS	KEYSTONE APT ACCESS	LOCAL	42	33	8	163	88	42	80	Strng	V Good	0	13	72
2040	RACE ST	KEYSTONE APT ACCESS	DS@813FT	LOCAL	26	813	117	2,465	99	73	60	Mod	Excellent	1	0	90
2041	RACE ST	DS@813FT	E 123RD AVE	LOCAL	42	546	127	2,674	100	88	60	Mod	Excellent	0	0	95
1047	RAPP LN	W 98TH AVE	W 99TH AVE	LOCAL	33	760	139	2,926	63	56	80	Strng	Good	13	24	60
2225	REGINA LN	DOWNING CT	E 108TH AVE	LOCAL	39	1,082	234	4,923	34	51	60	Mod	Poor	44	22	39
1229	ROMBLON WAY	W 104TH PL	QUIVAS ST	LOCAL	39	1,216	263	5,532	100	57	60	Mod	Excellent	0	0	85
1246	RONALD LN	W 104TH PL	NAIAD DR	LOCAL	39	1,268	275	5,771	38	56	60	Mod	Marginal	36	26	44
2218	ROSALIE DR	LEROY DR	E 108TH AVE	LOCAL	39	1,110	241	5,053	80	73	60	Mod	V Good	9	12	77
2236	ROSALIE DR	E 108TH AVE	E 108TH PL	LOCAL	39	415	90	1,890	63	39	80	Strng	Fair	14	23	54
2246	ROSALIE DR	E 108TH PL	MURIEL DR	LOCAL	39	498	108	2,266	57	47	60	Mod	Fair	24	19	53
1658	ROSALIE DR	MURIEL DR	LARSON DR	LOCAL	39	1,177	255	5,356	65	61	80	Strng	Good	10	24	63
1287	ROSEANNA DR	BREWER DR	CLAIRE LN	LOCAL	37	368	76	1,588	31	57	60	Mod	Poor	39	30	95
1295	ROSEANNA DR	CLAIRE LN	WESTVIEW ELEM ACCESS	LOCAL	38	248	52	1,099	36	72	60	Mod	Marginal	35	29	95
1297	ROSEANNA DR	WESTVIEW ELEM ACCESS	PECOS ST	LOCAL	38	409	86	1,815	36	68	60	Mod	Marginal	34	30	95
1278	ROSEANNA DR	BREWER DR	LIVINGSTON DR	LOCAL	39	295	64	1,341	36	39	80	Strng	Poor	36	27	95
1280	ROSEANNA DR	LIVINGSTON DR	KALAMATH ST	LOCAL	39	294	64	1,339	37	44	60	Mod	Poor	37	26	95
1281	ROSEANNA DR	KALAMATH ST	SANTA FE ST	LOCAL	39	292	63	1,327	25	63	60	Mod	Poor	46	29	95
1283	ROSEANNA DR	SANTA FE ST	WANDA LN	LOCAL	39	309	67	1,405	32	72	60	Mod	Marginal	46	23	95
1330	ROSEANNA DR	WANDA LN	KENNEDY DR	LOCAL	39	910	197	4,142	29	68	60	Mod	Marginal	39	29	95
1358	ROSEANNA DR	KENNEDY DR	LAMBERT LN	LOCAL	39	306	66	1,391	97	65	60	Mod	Excellent	0	3	86
1365	ROSEANNA DR	LAMBERT LN	MANET WAY	LOCAL	38	295	62	1,307	95	82	60	Mod	Excellent	0	5	90
1382	ROSEANNA DR	MANET WAY	ROSEANNA DR (CDS)	LOCAL	37	321	66	1,384	97	74	60	Mod	Excellent	0	4	89
1397	ROSEANNA DR	ROSEANNA DR (CDS)	CLAIRE LN	LOCAL	38	222	47	983	95	70	60	Mod	Excellent	3	3	86
1391	ROSEANNA DR (CDS)	ROSEANNA DR	WEST END	LOCAL	46	215	55	1,165	42	37	60	Mod	Marginal	35	24	40
1218	SANTA FE ST	KALAMATH ST	NAIAD DR	LOCAL	38	922	195	4,086	47	64	60	Mod	Fair	32	21	52
1319	SANTA FE ST	ROSEANNA DR	WANDA LN	LOCAL	39	485	105	2,205	87	64	60	Mod	V Good	5	8	79
1339	SANTA FE ST	WANDA LN	KENNEDY DR	LOCAL	39	334	72	1,520	83	56	60	Mod	V Good	5	12	73
1493	SHERMAN DR	E 113TH PL	NORTH END	LOCAL	38	438	92	1,933	36	54	80	Strng	Marginal	28	33	41
1201	SHERMAN ST	E 104TH PL	E 105TH AVE	LOCAL	39	410	89	1,866	59	47	80	Strng	Fair	19	22	55
1533	SHERMAN ST	EOP	SHERMAN ST	LOCAL	42	33	8	159	42	24	80	Strng	Poor	24	33	36
1532	SHERMAN ST	SHERMAN ST	EOP	LOCAL	42	33	8	160	71	20	80	Strng	Fair	15	14	53
1545	SHERMAN ST	SHERMAN ST	E 117TH AVE	LOCAL	25	237	33	690	64	35	80	Strng	Fair	12	24	54
1554	SHERMAN ST	E 117TH AVE	E 117TH PL	LOCAL	35	153	30	627	32	57	80	Strng	Marginal	33	35	40
1571	SHERMAN ST	E 117TH PL	E 118TH AVE	LOCAL	34	544	103	2,156	39	50	80	Strng	Marginal	30	31	42
1594	SHERMAN ST	E 118TH AVE	E 118TH PL	LOCAL	34	374	71	1,485	39	60	80	Strng	Marginal	27	34	45
1131	SPANGLER DR	CROKE DR	WEST END	LOCAL	38	707	149	3,133	94	65	60	Mod	V Good	0	5	84
1222	SPERRY DR	W 104TH PL	UTRILLO LN	LOCAL	39	965	209	4,389	38	63	60	Mod	Marginal	32	30	46
1260	SPERRY ST	UTRILLO LN	SULLY WAY	LOCAL	39	640	139	2,914	34	81	60	Mod	Marginal	37	29	49
1290	SPERRY ST	SULLY WAY	URA LN	LOCAL	37	620	127	2,677	41	59	60	Mod	Marginal	38	20	46
1962	SPRING DR	QUAM DR	TRUDA DR	LOCAL	38	1,325	280	5,872	100	74	60	Mod	Excellent	0	0	91
2000	SPRING DR	TRUDA DR	E 119TH PL	LOCAL	37	1,090	224	4,704	89	75	60	Mod	V Good	7	4	84
2139	ST PAUL CT	E 106TH PL	NE END	LOCAL	43	204	49	1,025	60	35	80	Strng	Fair	16	24	51
2245	ST PAUL WAY	E 108TH DR	NE END	LOCAL	41	320	72	1,521	68	41	80	Strng	Fair	13	20	58

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Condition Summary

GISID	On Street	From Street	To Street	FunCL	Pavement Width (ft)	Pavement Length (ft)	Add Area (yd2)	Pavement Area (yd2)	Condition Summary									
									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)
1683	STEELE ST	E 109TH AVE	E 110TH DR	LOCAL	33	253	46	974	79	65	80	Strng	V Good	0	21	74		
1692	STEELE ST	E 110TH DR	NORTH END	LOCAL	32	125	22	468	78	89	60	Mod	V Good	0	22	81		
2113	STEELE ST	E 106TH AVE	E 106TH PL	LOCAL	34	247	47	980	57	49	60	Mod	Fair	25	17	54		
2131	STEELE ST	E 106TH PL	FOX RUN PKWY	LOCAL	34	334	63	1,324	54	47	60	Mod	Fair	29	17	51		
2190	STEELE ST	MILWAUKEE ST	E 107TH CT	LOCAL	35	563	109	2,297	77	62	80	Strng	V Good	3	20	71		
2230	STEELE ST	E 107TH CT	MILWAUKEE ST	LOCAL	33	550	101	2,116	72	54	80	Strng	Good	6	22	65		
1288	SULLY WAY	SPERRY ST	URA LN	LOCAL	38	732	155	3,246	41	54	80	Strng	Marginal	29	30	45		
1004	SWITZER LN	DAMON DR	PELON DR	LOCAL	39	285	62	1,298	62	53	60	Mod	Fair	19	19	58		
1006	SWITZER LN	PELON DR	LOU DR	LOCAL	38	290	61	1,284	62	47	80	Strng	Fair	16	22	57		
1976	SYLVIA DR	WASHINGTON ST	CLARKSON CT	LOCAL	39	235	51	1,067	49	46	60	Mod	Marginal	28	24	47		
1980	SYLVIA DR	CLARKSON CT	TRUDA DR	LOCAL	39	427	92	1,941	46	71	60	Mod	Fair	28	26	54		
1994	SYLVIA DR	TRUDA DR	E 119TH PL	LOCAL	39	654	142	2,978	34	65	60	Mod	Marginal	38	27	44		
2021	SYLVIA DR	E 119TH PL	E 120TH AVE	LOCAL	39	199	43	905	44	32	80	Strng	Marginal	26	30	40		
1213	TANCRED ST	W 104TH PL	UTRILLO LN	LOCAL	39	799	173	3,636	89	61	60	Mod	V Good	5	6	79		
1286	TANCRED ST	UTRILLO LN	URA LN	LOCAL	39	1,410	305	6,414	60	53	80	Strng	Fair	14	26	57		
1370	TEAL ST	E 109TH AVE	GRANT DR	LOCAL	39	900	195	4,095	66	57	80	Strng	Good	9	25	62		
2282	TRUDA DR	SYLVIA DR	IRMA DR	LOCAL	39	2,065	447	9,396	95	57	60	Mod	V Good	3	2	82		
1941	TRUDA DR	IRMA DR	GILPIN ST	LOCAL	38	550	116	2,438	78	57	60	Mod	V Good	13	10	70		
1945	TRUDA DR	GILPIN ST	WILLIAMS WAY	LOCAL	38	310	66	1,376	90	86	60	Mod	Excellent	4	7	88		
1946	TRUDA DR	WILLIAMS WAY	HIGH ST	LOCAL	38	256	54	1,133	87	72	60	Mod	V Good	1	12	82		
1949	TRUDA DR	HIGH ST	MAIDEN WAY	LOCAL	39	243	53	1,104	87	71	60	Mod	V Good	0	13	82		
1948	TRUDA DR	MAIDEN WAY	CLAUDE CT	LOCAL	39	1,257	272	5,720	94	67	60	Mod	Excellent	1	5	85		
1274	UPTON ST	UTRILLO LN	URA LN	LOCAL	38	779	164	3,453	37	66	60	Mod	Marginal	35	28	46		
1174	URA LN	W 104TH AVE	W 104TH PL	LOCAL	39	199	43	904	20	38	80	Strng	Poor	48	31	26		
1209	URA LN	W 104TH PL	UTRILLO LN	LOCAL	39	636	138	2,894	24	65	60	Mod	Poor	51	24	37		
1239	URA LN	UTRILLO LN	W 105TH PL	LOCAL	38	632	133	2,803	24	58	60	Mod	Poor	52	24	35		
1282	URA LN	W 105TH PL	URA LN (CDS)	LOCAL	39	843	183	3,835	26	69	60	Mod	Marginal	44	30	40		
1285	URA LN	URA LN (CDS)	TANCRED ST	LOCAL	42	45	11	221	28	49	60	Mod	Poor	44	28	34		
1298	URA LN	TANCRED ST	VARESE LN	LOCAL	39	236	51	1,074	32	62	60	Mod	Marginal	43	25	41		
1267	URA LN	PECOS ST	URA LN (CDS)	LOCAL	39	287	62	1,307	75	52	80	Strng	Good	0	25	67		
1273	URA LN	URA LN (CDS)	UTRILLO LN	LOCAL	38	475	100	2,104	63	58	80	Strng	Good	13	24	61		
1275	URA LN	UTRILLO LN	UPTON ST	LOCAL	38	268	57	1,187	61	65	60	Mod	Good	13	27	61		
1289	URA LN	UPTON ST	SULLY WAY	LOCAL	37	268	55	1,159	70	68	60	Mod	Good	7	23	69		
1291	URA LN	SULLY WAY	SPERRY ST	LOCAL	38	268	57	1,188	54	68	60	Mod	Fair	22	24	58		
1299	URA LN	SPERRY ST	VARESE LN	LOCAL	36	364	73	1,527	70	66	80	Strng	Good	6	24	68		
1284	URA LN (CDS)	NW END	URA LN	LOCAL	85	58	27	573	35	43	60	Mod	Poor	41	24	37		
1266	URA LN (CDS)	URA LN	SW END	LOCAL	49	205	56	1,166	53	36	80	Strng	Marginal	19	28	46		
1212	UTRILLO LN	URA LN	TANCRED ST	LOCAL	38	271	57	1,200	86	48	60	Mod	V Good	8	6	73		
1221	UTRILLO LN	TANCRED ST	SPERRY ST	LOCAL	39	272	59	1,238	95	68	60	Mod	Excellent	0	5	86		
1243	UTRILLO LN	SPERRY ST	UPTON ST	LOCAL	38	433	91	1,918	98	62	60	Mod	Excellent	0	2	86		
1272	UTRILLO LN	UPTON ST	URA LN	LOCAL	38	733	155	3,249	91	70	60	Mod	V Good	7	2	83		
1208	VARESE LN	W 104TH PL	WYANDOT ST	LOCAL	39	644	140	2,930	35	47	60	Mod	Poor	44	20	95	9/30/22	95
1215	VARESE LN	WYANDOT ST	ZUNI ST	LOCAL	39	269	58	1,224	25	56	60	Mod	Poor	55	19	95	9/30/22	95

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GISID	On Street	From Street	To Street	FunCL	Pavement Width (ft)	Pavement Length (ft)	Add Area (yd2)	Pavement Area (yd2)	Condition Summary									
									Surface Distress Index (SDI)	Roughness Index (RI)	Structural Index (SI)	Strength Rating	Condition Rating	Load Assoc Distress Deducts (LADD)	Non-Load Distress Deducts (NLAD)	PCI Override (OPCI)	OPCI Date	Current Segment PCI (CPCI)
1228	VARESE LN	ZUNI ST	W 105TH PL	LOCAL	38	248	52	1,101	30	46	60	Mod	Poor	46	24	95	9/30/22	95
1301	VARESE LN	W 105TH PL	VARESE LN (CDS)	LOCAL	38	1,204	254	5,340	39	52	60	Mod	Marginal	39	22	95	9/30/22	95
1303	VARESE LN	VARESE LN (CDS)	URA LN	LOCAL	38	516	109	2,286	51	42	60	Mod	Marginal	30	19	95	9/30/22	95
1302	VARESE LN (CDS)	NW END	VARESE LN	LOCAL	58	99	32	664	76	47	80	Strng	Good	4	19			66
1354	VERNA LN	ACOMA ST	BANNOCK ST	LOCAL	38	296	62	1,311	32	53	60	Mod	Poor	37	32			38
1387	VERNA LN	BANNOCK ST	BONITA PL	LOCAL	39	1,005	218	4,573	36	62	60	Mod	Marginal	31	33			44
1351	WALLACE ST	ACOMA ST	PATTERSON CT	LOCAL	38	993	210	4,401	47	49	60	Mod	Marginal	30	23	95	9/30/22	95
1320	WANDA LN	ROSEANNA DR	SANTA FE ST	LOCAL	39	700	152	3,184	95	52	60	Mod	V Good	3	2			80
2087	WASHINGTON ST	E 104TH AVE	E 105TH PL	ARTERIAL	68	922	348	7,311	100	96	60	Mod	Excellent	0	0			98
2160	WASHINGTON ST	E 105TH PL	GARLAND DR	ARTERIAL	23	1,082	138	2,903	92	94	60	Mod	Excellent	1	7			92
2265	WASHINGTON ST	GARLAND DR	MURIEL DR	ARTERIAL	61	2,361	800	16,806	100	100	60	Mod	Excellent	0	0			99
1766	WASHINGTON ST	MURIEL DR	E 112TH AVE	ARTERIAL	23	875	112	2,349	100	96	60	Mod	Excellent	0	0			98
1822	WASHINGTON ST	E 112TH AVE	MALLEY DR	ARTERIAL	23	1,233	157	3,307	95	92	60	Mod	Excellent	5	0			93
1889	WASHINGTON ST	MALLEY DR	PHILLIPS DR	ARTERIAL	24	1,550	207	4,341	96	91	60	Mod	Excellent	4	0			94
1975	WASHINGTON ST	PHILLIPS DR	SYLVIA DR	ARTERIAL	27	1,605	241	5,056	100	97	60	Mod	Excellent	0	0			98
2022	WASHINGTON ST	SYLVIA DR	E 120TH AVE	ARTERIAL	34	840	159	3,333	100	89	60	Mod	Excellent	0	0			95
2027	WASHINGTON ST	E 120TH AVE	E 118TH AVE	ARTERIAL	33	1,271	233	4,893	95	92	60	Mod	Excellent	5	0			93
1956	WASHINGTON ST	E 118TH AVE	E 117TH AVE	ARTERIAL	29	643	104	2,177	98	89	60	Mod	Excellent	2	0			94
1925	WASHINGTON ST	E 117TH AVE	E 117TH AVE	ARTERIAL	58	53	17	356	83	56	60	Mod	V Good	14	3			74
1915	WASHINGTON ST	E 117TH AVE	E 115TH AVE	ARTERIAL	26	764	110	2,318	98	93	60	Mod	Excellent	2	0			95
1881	WASHINGTON ST	E 115TH AVE	E 114TH CT	ARTERIAL	25	541	75	1,577	100	92	60	Mod	Excellent	0	0			96
1856	WASHINGTON ST	E 114TH CT	MALLEY DR	ARTERIAL	26	706	102	2,141	98	86	60	Mod	Excellent	2	0			93
1821	WASHINGTON ST	MALLEY DR	MURIEL DR	ARTERIAL	24	2,109	281	5,905	100	93	60	Mod	Excellent	0	0			97
2264	WASHINGTON ST	MURIEL DR	E 108TH AVE	ARTERIAL	24	1,734	231	4,855	100	100	60	Mod	Excellent	0	0			99
2216	WASHINGTON ST	E 108TH AVE	GARLAND DR	ARTERIAL	25	625	87	1,824	100	91	60	Mod	Excellent	0	0			96
2162	WASHINGTON ST	GARLAND DR	WASHINGTON WAY	ARTERIAL	24	1,082	144	3,031	100	86	60	Mod	Excellent	0	0			95
2086	WASHINGTON ST	WASHINGTON WAY	E 104TH AVE	ARTERIAL	62	921	317	6,659	99	94	60	Mod	Excellent	1	0			96
1216	WASHINGTON WAY	E 104TH PL	PENNSYLVANIA ST	LOCAL	38	1,069	226	4,739	71	67	60	Mod	Good	13	16			69
2090	WASHINGTON WAY	PENNSYLVANIA ST	PEARL ST	LOCAL	38	251	53	1,112	81	81	60	Mod	V Good	3	16			81
2085	WASHINGTON WAY	PEARL ST	WASHINGTON ST	LOCAL	39	177	38	804	81	24	80	Strng	Good	0	19			61
2287	WCR 11	N METRO AVE	E 168TH AVE	ARTERIAL	28	5,373	836	17,551	23	52	60	Mod	Poor	43	17			31
1326	WELLINGTON ST	ACOMA ST	MELODY DR	LOCAL	38	1,107	234	4,908	100	57	60	Mod	Excellent	0	0			85
1328	WELLINGTON ST	MELODY DR	MELODY DR	LOCAL	42	30	7	149	26	9	80	Strng	V Poor	45	29			20
1294	WESTVIEW ELEM ACCESS	ROSEANNA DR	CLAIRE LN	LOCAL	28	490	76	1,602	45	34	80	Strng	Marginal	30	22			41
2103	WILLIAMS ST	GILPIN ST	E 105TH PL	LOCAL	38	979	207	4,339	68	61	60	Mod	Good	11	21			65
1967	WILLIAMS WAY	TRUDA DR	HIGH ST	LOCAL	39	666	144	3,031	56	55	60	Mod	Fair	21	23			55
1989	WILLIAMS WAY	HIGH ST	E 119TH PL	LOCAL	39	653	141	2,971	59	58	60	Mod	Fair	19	23			57
1207	WYANDOT ST	W 104TH PL	VARESE LN	LOCAL	39	631	137	2,870	43	54	60	Mod	Marginal	31	26			46
1869	WYCO DR	E 115TH AVE	E 115TH PL	LOCAL	39	262	57	1,190	46	62	60	Mod	Fair	26	28			51
1876	WYCO DR	E 115TH PL	E 116TH AVE	LOCAL	38	248	52	1,098	67	89	60	Mod	V Good	13	20			74
1886	WYCO DR	E 116TH AVE	E 116TH PL	LOCAL	38	298	63	1,321	45	69	60	Mod	Fair	29	26			52
1901	WYCO DR	E 116TH PL	E 117TH WAY	LOCAL	38	259	55	1,149	62	64	60	Mod	Good	24	14			61
1923	WYCO DR	E 117TH WAY	E 117TH CT	LOCAL	39	310	67	1,409	51	83	60	Mod	Good	24	25			61

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1935	WYCO DR	E 117TH CT	PHILLIPS DR	LOCAL	39	260	56	1,182	63	46	80	Strng	Fair	16	21			57
1656	YORK ST	E 109TH DR	E 110TH DR	LOCAL	35	450	88	1,838	53	55	80	Strng	Fair	19	27			53
1759	YORK ST	E 111TH DR	E 112TH AVE	LOCAL	34	167	31	661	61	23	80	Strng	Marginal	20	19			48
1744	YORK WAY	E 110TH PL	E 111TH DR	LOCAL	31	591	102	2,138	64	53	80	Strng	Good	8	28			60
1214	ZUNI ST	W 104TH PL	VARESE LN	LOCAL	39	816	177	3,714	44	61	60	Mod	Marginal	28	28	95	9/30/22	95

**Appendix B**  
**\$1.5M/Yr Rehabilitation Plan by Segment**



GISID	On Street	From Street	To Street	Current Segment PCI (CPCI)	Year of First Rehab	Rehab Activity	Avg Unit Rate (\$/yd2)	Segment Pavement Cost (\$)	Segment Total Cost (\$)	Whole Project Cost (\$)
1553	116TH WY	NW END	W 116TH WAY	43	1	EM/FWM + Moderate Overlay (2.0 - 3.0) + Strctrl Ptch	24.25	6,402	6,402	6,402
1958	118TH AVE	WASHINGTON ST	DS@226FT	24	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	19,360	19,360	46,944
1957	118TH AVE	DS@226FT	DS@533FT	34	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	27,584	27,584	46,944
1578	118TH AVE	GALAPAGO CT	HURON ST	27	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	26,816	26,816	26,816
1587	118TH AVE	ACOMA ST	DELAWARE CT	95	1	Routine Maintenance	0.00	0	0	0
1585	118TH AVE	DELAWARE CT	ELATI CT	95	1	Routine Maintenance	0.00	0	0	0
1582	118TH AVE	ELATI CT	MELODY DR	95	1	Routine Maintenance	0.00	0	0	0
1580	118TH AVE	MELODY DR	GALAPAGO CT	95	1	Routine Maintenance	0.00	0	0	0
2061	FOX RUN PKWY	MADISON WAY	SE END	29	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	33.50	12,797	12,797	12,797
1015	HURON ST	SOUTH END	FRED DR	33	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	43,960	43,960	1,193,990
1028	HURON ST	FRED DR	W 98TH AVE	27	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	77,560	77,560	1,193,990
1037	HURON ST	W 98TH AVE	MELODY DR	20	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	83,720	83,720	1,193,990
1044	HURON ST	MELODY DR	W 99TH AVE	27	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	113,995	113,995	1,193,990
1057	HURON ST	W 99TH AVE	NIVER AVE	31	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	92,295	92,295	1,193,990
1065	HURON ST	NIVER AVE	W 100TH AVE	33	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	91,175	91,175	1,193,990
1075	HURON ST	W 100TH AVE	W 100TH PL	30	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	78,015	78,015	1,193,990
1085	HURON ST	W 100TH PL	W 101ST AVE	25	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	68,635	68,635	1,193,990
1095	HURON ST	W 101ST AVE	W 101ST PL	33	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	67,795	67,795	1,193,990
1101	HURON ST	W 101ST PL	W 102ND AVE	34	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	68,880	68,880	1,193,990
1106	HURON ST	W 102ND AVE	W 102ND PL	30	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	70,105	70,105	1,193,990
1112	HURON ST	W 102ND PL	W 103RD AVE	22	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	68,985	68,985	1,193,990
1121	HURON ST	W 103RD AVE	W 103RD PL	23	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	89,810	89,810	1,193,990
1144	HURON ST	W 103RD PL	W 104TH AVE	25	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	179,060	179,060	1,193,990
2134	KIMBLEWYCK CIR	FOX RUN CIR	KIMBLEWYCK CIR (E)	26	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	26,176	26,176	26,176
2158	KIMBLEWYCK CIR (N)	KIMBLEWYCK CIR	KIMBLEWYCK CIR	25	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	17,024	17,024	43,616
2157	KIMBLEWYCK CIR (N)	KIMBLEWYCK CIR	KIMBLEWYCK CIR (E)	29	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	26,592	26,592	43,616
2151	KIMBLEWYCK CIR (W)	KIMBLEWYCK CIR	KIMBLEWYCK CIR (N)	29	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	23,008	23,008	23,008
1498	LINCOLN DR	E 113TH PL	MALLEY DR	28	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	78,144	78,144	78,144
1435	NORTHWEST OPEN SPACE REC AREA	NORTHWEST OPEN SPACE REC AREA	WEST END	31	1	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	9,728	9,728	9,728
1298	URA LN	TANCRED ST	VARESE LN	41	1	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	29,535	29,535	29,535
1649	110TH DR EXT	SOUTH END	E 110TH DR	61	2	Edge Mill + Thin Overlay (1.5 - 2.0) + Strctrl Ptch	23.75	1,211	1,211	1,211
1485	113TH PL	WEST END	LINCOLN DR	28	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	48,544	48,544	111,136
1486	113TH PL	LINCOLN DR	SHERMAN DR	33	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	34,976	34,976	111,136
1487	113TH PL	SHERMAN DR	HIGHLINE DR	34	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	27,616	27,616	111,136
2279	120TH AVE	WASHINGTON ST	WASHINGTON ST	33	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	10,675	10,675	10,675
2028	120TH AVE	CLAUDE CT	EAST END	46	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.50	61,620	61,620	61,620
2062	FOX RUN PKWY	SE END	MADISON WAY	32	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	33.50	14,070	14,070	14,070
1171	GRANT DR	E 104TH AVE	DS@41FT	45	2	EM/FWM + Moderate Overlay (2.0 - 3.0) + Strctrl Ptch	25.00	5,675	5,675	5,675



GISID	On Street	From Street	To Street	Current Segment PCI (CPCI)	Year of First Rehab	Rehab Activity	Avg Unit Rate (\$/yd2)	Segment Pavement Cost (\$)	Segment Total Cost (\$)	Whole Project Cost (\$)
1986	IRMA DR (CDS)	EAST END	IRMA DR	31	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	26,304	26,304	26,304
2222	LEROY DR	DS@1046FT	DS@1135FT	40	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	20,608	20,608	631,392
1001	LEROY DR	DS@1135FT	IRMA DR	27	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	610,784	610,784	631,392
1920	PENNSYLVANIA ST	PENNSYLVANIA ST	E 117TH AVE	44	2	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	21,973	21,973	21,973
2287	WCR 11	N METRO AVE	E 168TH AVE	31	2	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	614,285	614,285	614,285
1467	112TH AVE	PINNACLE CREEK	LIVINGSTON DR	36	3	FWM + Thick Overlay (> 2.0 - 3.0)	30.50	78,507	78,507	1,114,288
1458	112TH AVE	LIVINGSTON DR	W 111TH WAY	43	3	FWM + Thick Overlay (> 2.0 - 3.0)	30.50	361,456	361,456	1,114,288
2266	112TH AVE	W 111TH WAY	WEST END	51	3	FWM + Thick Overlay (> 2.0 - 3.0)	30.50	674,325	674,325	1,114,288
1576	118TH AVE	DS@533FT	DS@752FT	49	3	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	16,858	16,858	16,858
1620	120TH AVE	DS@583FT	GRANT ST	48	3	FWM + Thick Overlay (> 2.0 - 3.0)	30.50	67,588	67,588	67,588
1622	120TH AVE	GRANT ST	WASHINGTON ST	49	3	FWM + Thick Overlay (> 2.0 - 3.0)	30.50	301,218	301,218	301,218
1066	100TH AVE	NIVER AVE	HURON ST	33	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	82,048	82,048	82,048
1117	103RD AVE	HURON ST	CROKE DR	35	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	147,136	147,136	147,136
1178	104TH PL	QUIVAS ST	ROMBLON WAY	48	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	63,085	63,085	63,085
2195	108TH AVE	ADAMS ST	NW END	57	4	Edge Mill + Thin Overlay (1.5 - 2.0) + Strctrl Ptch	23.75	4,251	4,251	4,251
1781	112TH PL	CLAUDE CT	SE END	36	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	17,312	17,312	17,312
1861	114TH CT	WEST END	PEARL ST	33	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	37,280	37,280	37,280
1149	BANNOCK ST	SOUTH END	W 104TH AVE	33	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	266,112	266,112	266,112
1867	CARLILE ST	E 114TH PL	E 115TH AVE	36	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	99,968	99,968	99,968
1708	ELMER DR	LARSON DR	E 110TH PL	48	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	130,790	130,790	130,790
2070	FRANKLIN WAY	FRANKLIN ST	E 105TH PL	48	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	98,670	98,670	98,670
1250	GARLAND DR	LINCOLN ST	GRANT DR	48	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	124,905	124,905	124,905
2177	KIMBLEWYCK CIR	KIMBLEWYCK CIR (N)	FOX RUN CIR	48	4	EM/FWM + Moderate Overlay (2.0 - 3.0) + Strctrl Ptch	24.25	8,439	8,439	8,439
1735	LARSON DR	ELMER DR	E 111TH PL	49	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	31,158	31,158	31,158
1336	MELODY DR	MELODY DR	WELLINGTON ST	43	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	11,853	11,853	23,376
1327	MELODY DR	WELLINGTON ST	KENNEDY DR	56	4	FWM + Thick Overlay (> 2.0 - 3.0)	27.50	11,523	11,523	23,376
1533	SHERMAN ST	EOP	SHERMAN ST	36	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	5,088	5,088	5,088
1174	URA LN	W 104TH AVE	W 104TH PL	26	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	28,928	28,928	341,024
1209	URA LN	W 104TH PL	UTRILLO LN	37	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	92,608	92,608	341,024
1239	URA LN	UTRILLO LN	W 105TH PL	35	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	89,696	89,696	341,024
1282	URA LN	W 105TH PL	URA LN (CDS)	40	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	122,720	122,720	341,024
1285	URA LN	URA LN (CDS)	TANCRED ST	34	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	7,072	7,072	341,024
1284	URA LN (CDS)	NW END	URA LN	37	4	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	18,336	18,336	18,336
1556	117TH AVE	GALAPAGO CT	WEST END	50	5	EM/FWM + Moderate Overlay (2.0 - 3.0) + Strctrl Ptch	24.25	10,816	10,816	10,816
2108	ADAMS CT	SW END	ADAMS ST	38	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	41,920	41,920	41,920
1910	BOWMAN PL	MARION ST	IRMA DR	38	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	132,320	132,320	132,320
1220	CARMELA LN	W 104TH PL	BREWER DR	38	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	156,128	156,128	156,128
1481	COMMUNITY CENTER DR	HURON ST	I-25	48	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	303,765	303,765	911,890





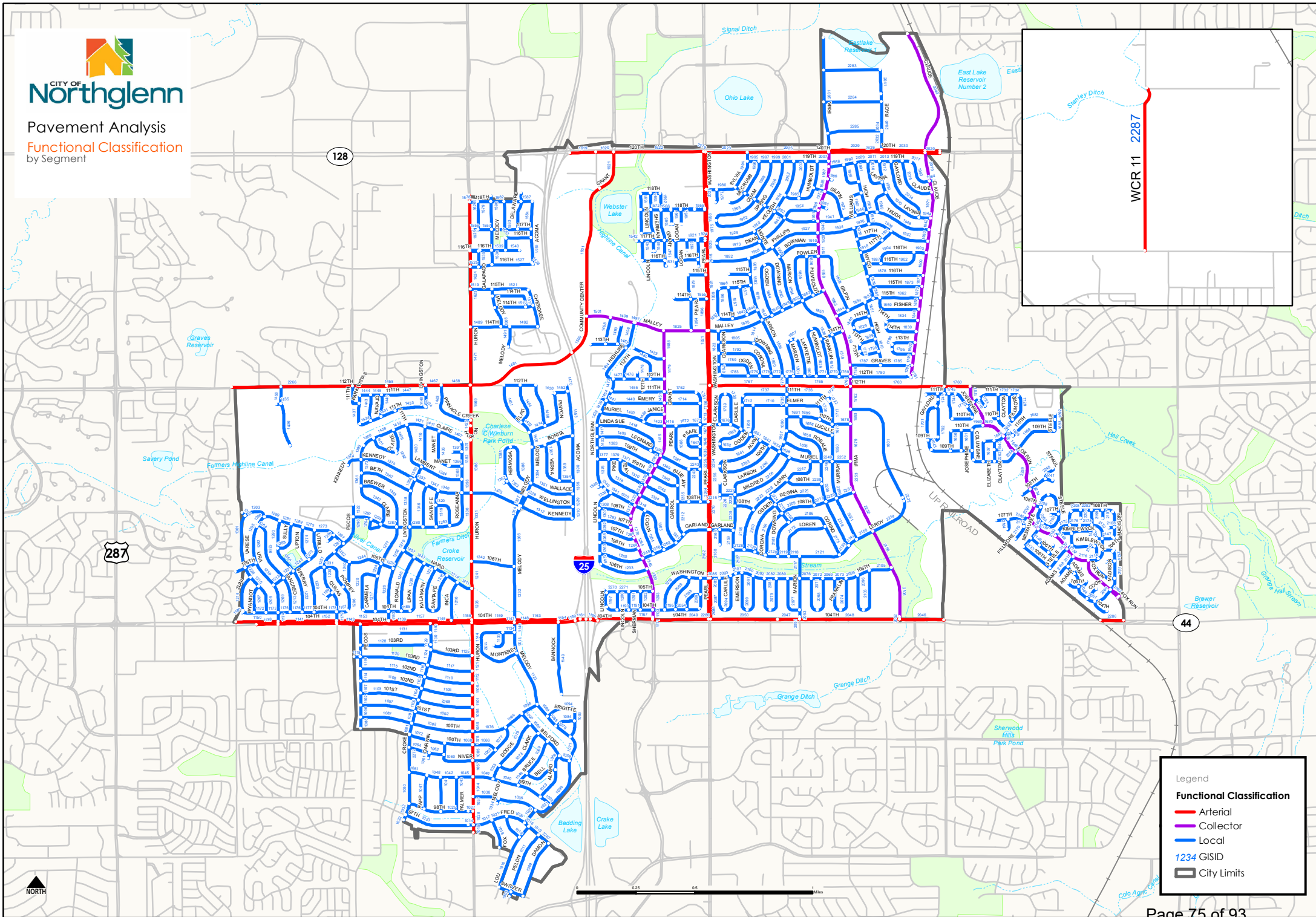
\$1500k/Year Rehabilitation Plan

GISID	On Street	From Street	To Street	Current Segment PCI (CPCI)	Year of First Rehab	Rehab Activity	Avg Unit Rate (\$/yd2)	Segment Pavement Cost (\$)	Segment Total Cost (\$)	Whole Project Cost (\$)
1500	COMMUNITY CENTER DR	I-25	MALLEY DR	40	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	137,130	137,130	911,890
1601	COMMUNITY CENTER DR	MALLEY DR	GRANT ST	35	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	35.00	470,995	470,995	911,890
1814	DOWNING DR (CDS)	DOWNING DR	NORTH END	38	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	29,120	29,120	29,120
2171	FILLMORE WAY (CDS)	FILLMORE WAY	EAST END	65	5	Edge Mill + Thin Overlay (1.5 - 2.0) + Strctrl Ptch	23.75	2,874	2,874	2,874
1423	MURIEL DR	JANICE CT	GRANT DR	38	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	69,184	69,184	69,184
1296	PECOS ST	URA LN	ROSEANNA DR	38	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	95,648	95,648	135,264
1322	PECOS ST	ROSEANNA DR	NORTH END	37	5	FWM + Thick Overlay (> 2.0 - 3.0) + Strctrl Ptch	32.00	39,616	39,616	135,264
1328	WELLINGTON ST	MELODY DR	MELODY DR	20	5	ACP Full Depth Reconstruction	68.50	10,207	10,207	10,207

**Appendix C**  
**Full-Size Maps**



Pavement Analysis  
Functional Classification  
by Segment



Legend

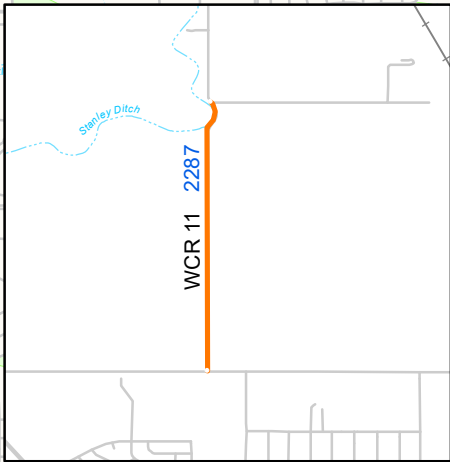
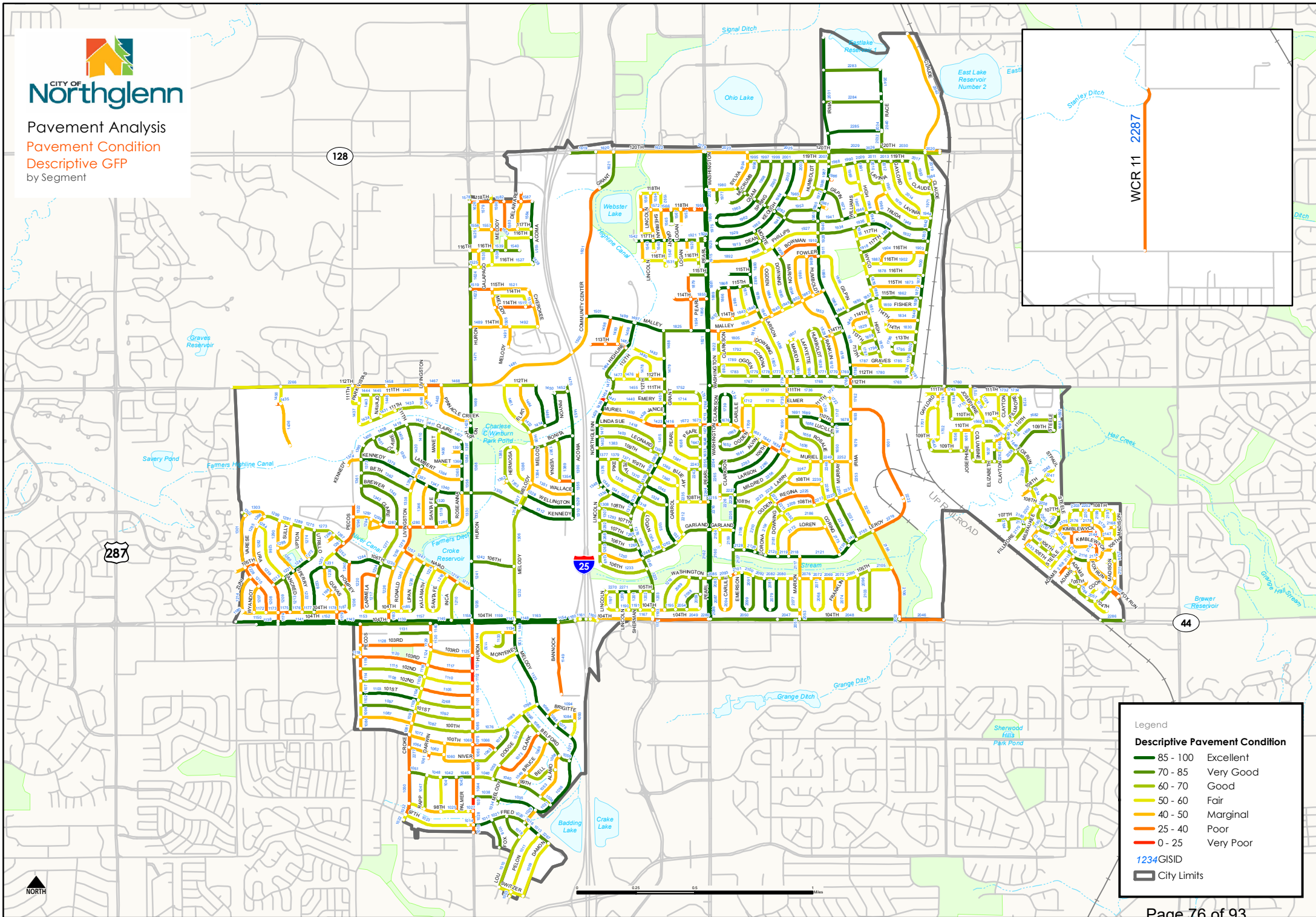
**Functional Classification**

- Arterial
- Collector
- Local
- 1234 GISID
- City Limits

WCR 11 2287



Pavement Analysis  
Pavement Condition  
Descriptive GFP  
by Segment



Legend

**Descriptive Pavement Condition**

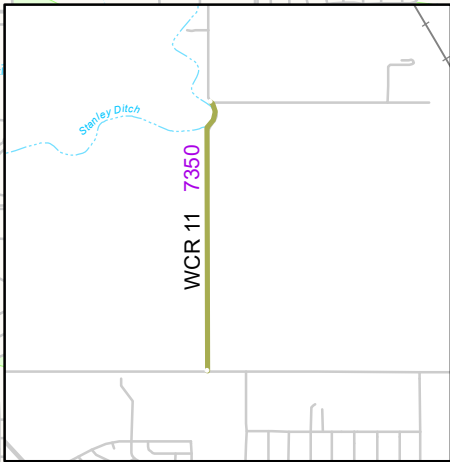
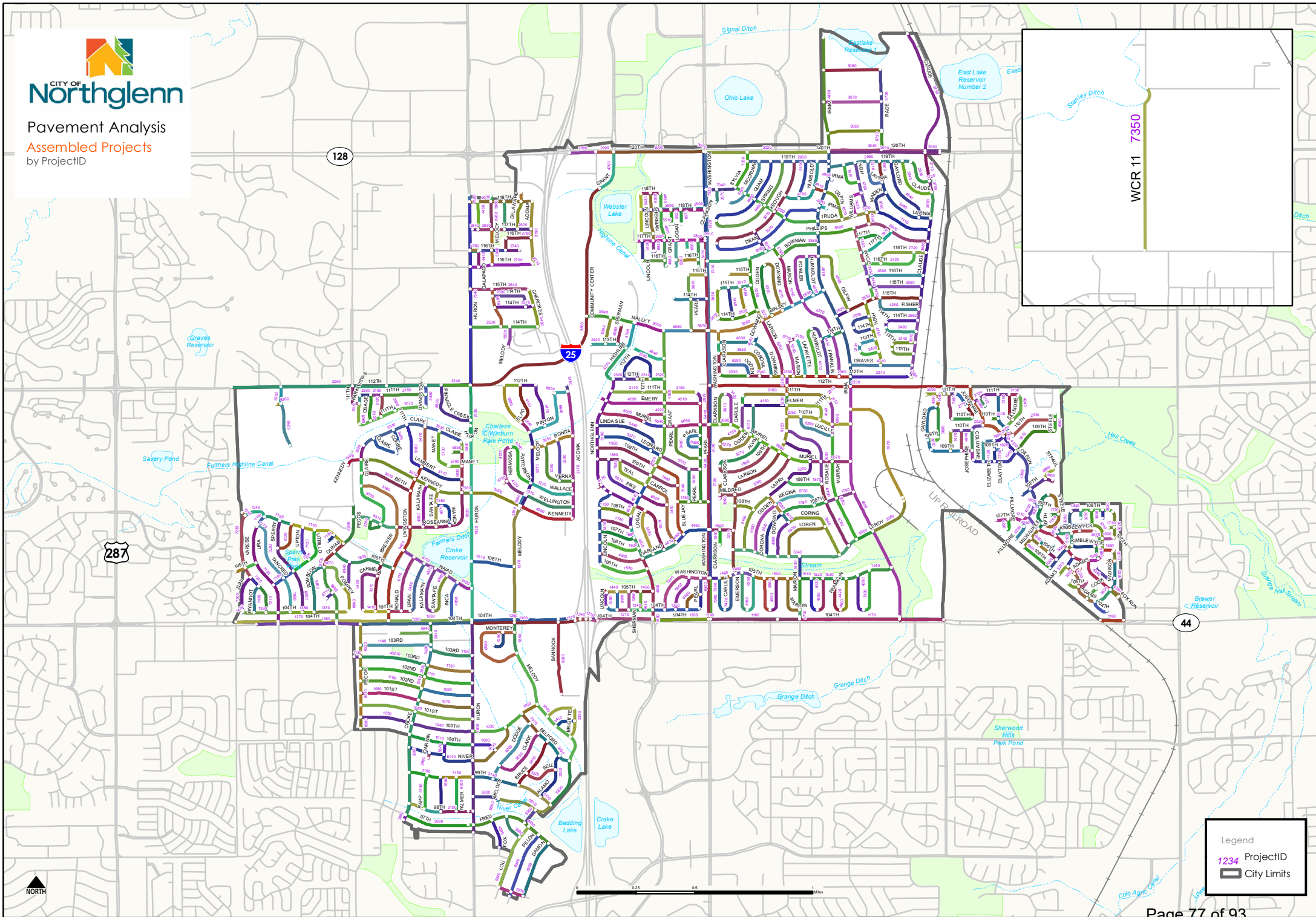
Green	85 - 100	Excellent
Light Green	70 - 85	Very Good
Yellow-Green	60 - 70	Good
Yellow	50 - 60	Fair
Orange	40 - 50	Marginal
Red-Orange	25 - 40	Poor
Red	0 - 25	Very Poor

1234 GISID

City Limits

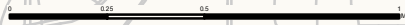


Pavement Analysis  
Assembled Projects  
by ProjectID



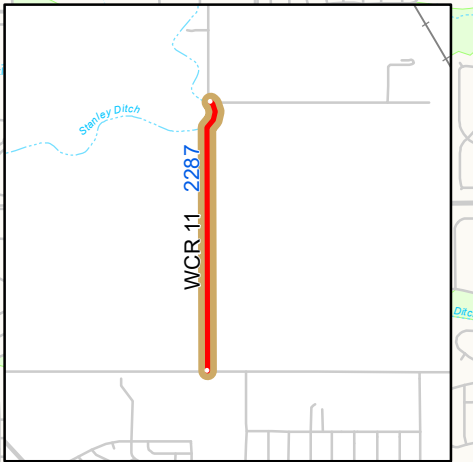
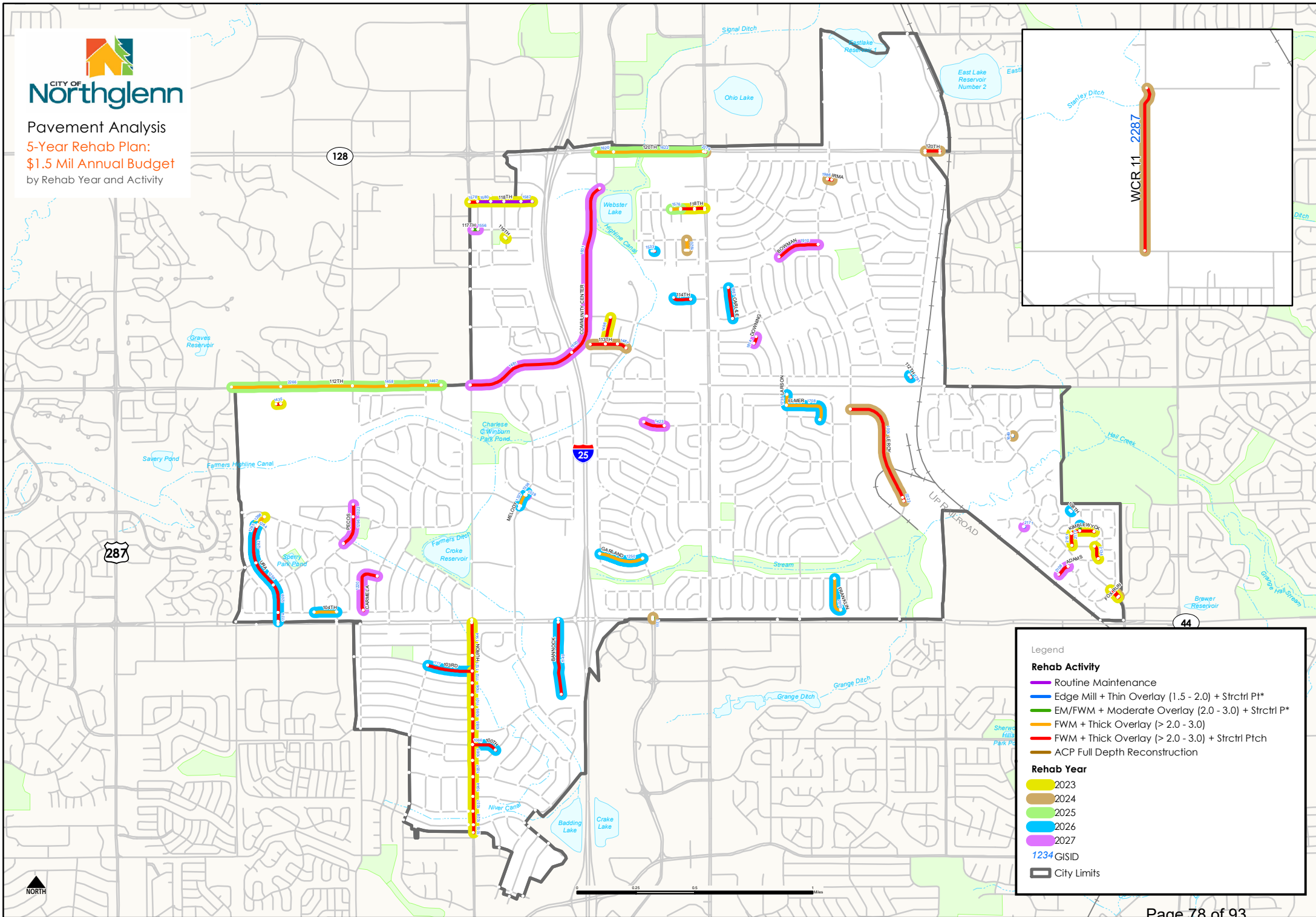
Legend

- 1234 ProjectID
- City Limits





**Pavement Analysis**  
**5-Year Rehab Plan:**  
**\$1.5 Mil Annual Budget**  
 by Rehab Year and Activity



**Legend**

**Rehab Activity**

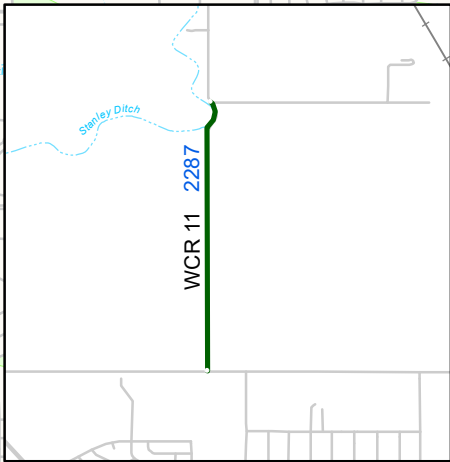
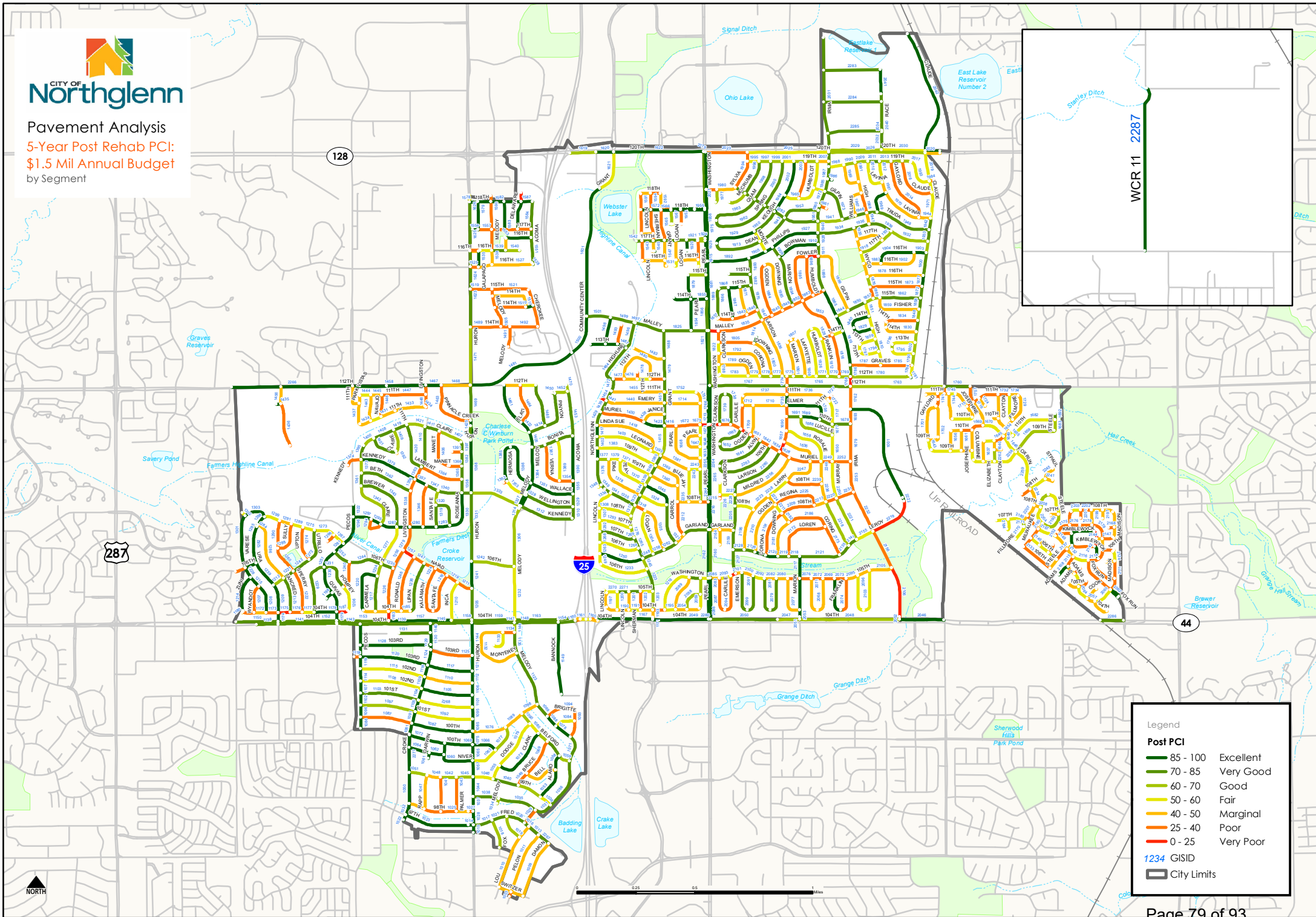
- Routine Maintenance
- Edge Mill + Thin Overlay (1.5 - 2.0) + Strctl Pt\*
- EM/FWM + Moderate Overlay (2.0 - 3.0) + Strctl P\*
- FWM + Thick Overlay (> 2.0 - 3.0)
- FWM + Thick Overlay (> 2.0 - 3.0) + Strctl Ptch
- ACP Full Depth Reconstruction

**Rehab Year**

- 2023
- 2024
- 2025
- 2026
- 2027
- 1234 GISID
- City Limits



**Pavement Analysis**  
**5-Year Post Rehab PCI:**  
**\$1.5 Mil Annual Budget**  
by Segment



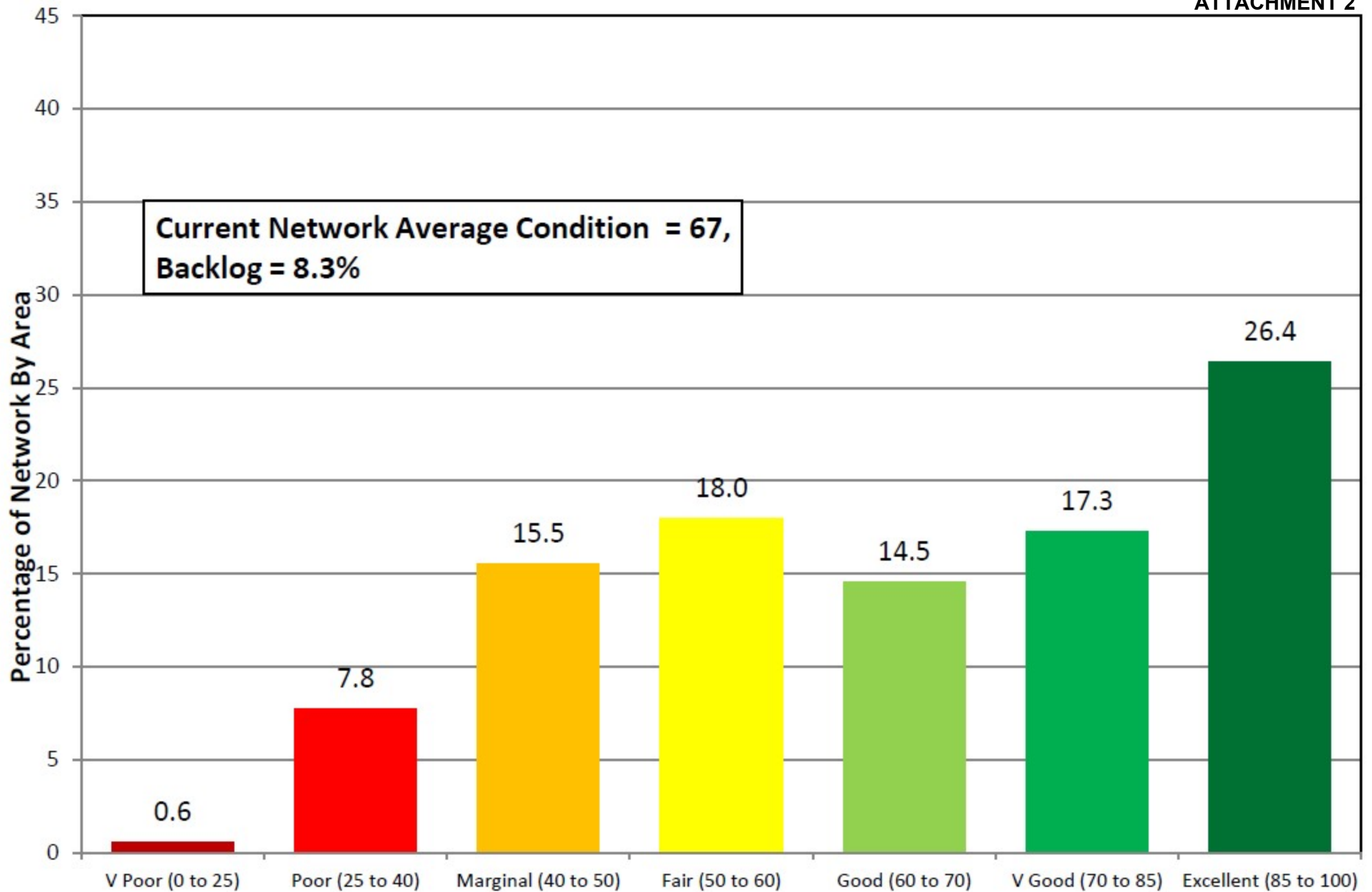
Legend

Post PCI	
85 - 100	Excellent
70 - 85	Very Good
60 - 70	Good
50 - 60	Fair
40 - 50	Marginal
25 - 40	Poor
0 - 25	Very Poor

1234 GISID

City Limits





Current Pavement Condition Using Descriptive Terms



# 2022 PAVEMENT CONDITION ASSESSMENT

**Kent Kisselman**

*Public Works Director*

*303.450.4005*

*kkisselman@northglenn.org*

**Special Meeting**

*March 20, 2023*



CITY OF  
**Northglenn**

# ***PURPOSE***

**To present the 2022 Pavement Condition Assessment, including current total street network conditions and recommendations for future roadway asset management.**



# ***BACKGROUND***

- City is responsible to maintain an average PCI of 70 (Resolution No. 06-92, Series 2006)
- PCI methodology adopted by the City - ASTM D6433
- PCI conducted every 3 years (\$45k)
- IMS Infrastructure completed 2016, 2019 and 2022 PCI using laser technology
- 2019 PCI ranking was 60
- Next PCI scheduled for 2025



# 2022 PCI RESULTS

- Backlog is defined as the percentage of streets with a PCI of 40 or less.
- Percent of Excellent under 15% indicates agency may be struggling to properly fund road network maintenance.
- Generally a very healthy agency will have less than 10% backlog. If backlog reaches 20%, work could become extremely costly.

Northglenn Network	PCI (Average 60 to 65)	Percent of Excellent (min. 15%)	Backlog (less than 10%)
2016	58	14	17
2019	60	7	6.9
2022	67	26.4	8.3



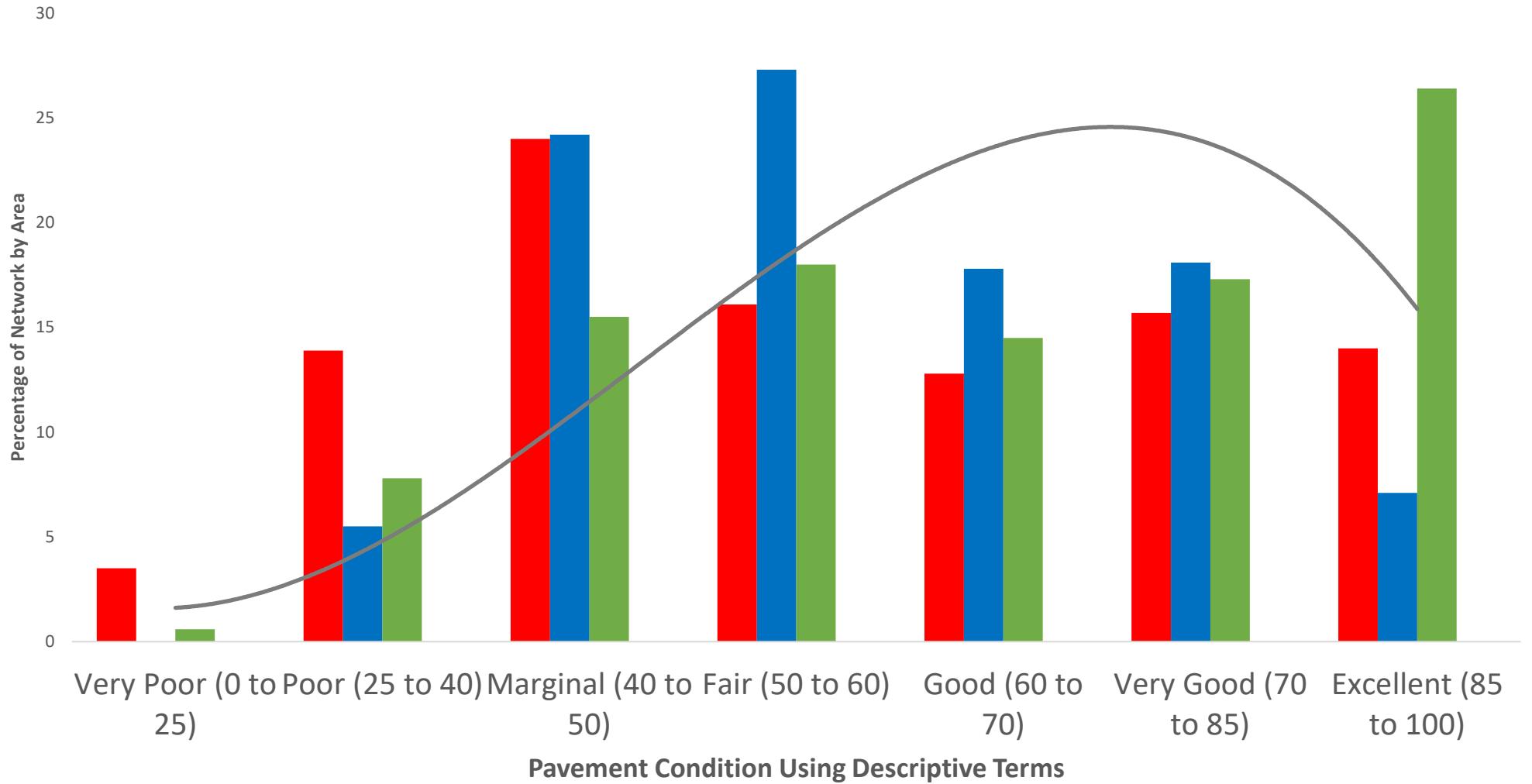
# CONDITION RATING

Condition	Range	Recommended Treatment
Excellent	85 to 100	Crack Sealing
Very Good	70 to 85	MicroSurface / Chip Seal
Good	60 to 70	Edge Mill + Thin Overlay or Chip Seal
Fair	50 to 60	Edge Mill + Thin Overlay (1.5 -2.0 in)
Marginal	40 to 50	Full Width Mill (FWM) + Thick Overlay + Minimal or No Patching
Poor	25 to 40	FWM + Thick Overlay (2.0 – 3.0 in) + Possibly Extensive Patching
Very Poor	0 to 25	Reconstruction



# Northglenn, CO

## As Surveyed Pavement Condition Rating



**PCI  
2016-  
2022**

2016 2019 2022 Poly. (Healthy System)

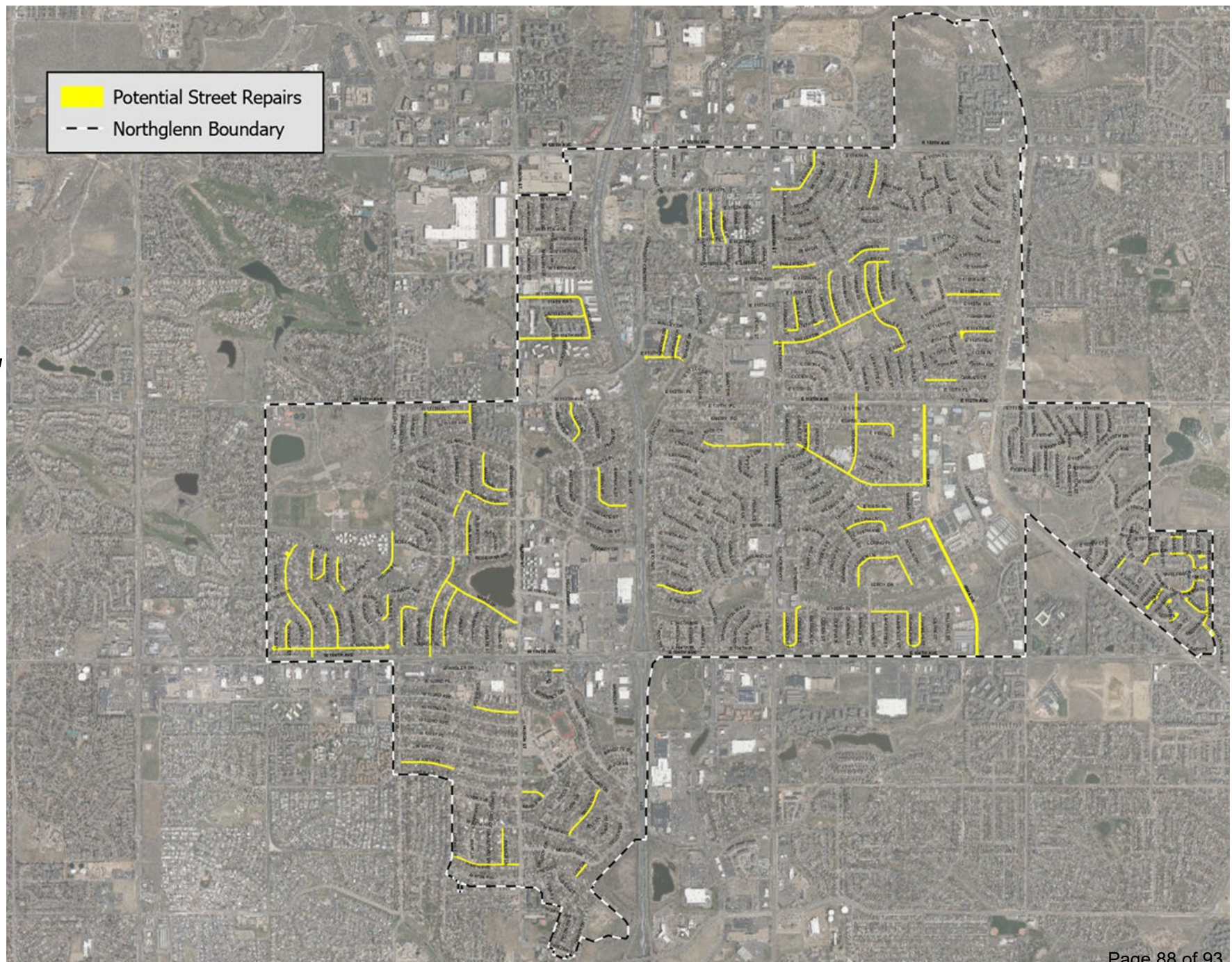


# ***PAVEMENT MANAGEMENT PROGRAM***

- **Streetlogix is a highly customizable, web-based asset management software that enables municipalities to optimize their road budget within a GIS environment.**
  - Provides state of infrastructure
  - Makes maintenance and repair recommendations
  - Prioritizes roadway projects
  - Informs long- and short-term budgeting
  - Further discussion at future planning sessions



# POTENTIAL STREETS FOR REPAIR





# ***PAVEMENT MANAGEMENT PROGRAM***

**CONT'D**

## **Software**

**<https://login.streetlogix.com/>**

## **Story Board**

**<http://t.ly/xNXz5>**



# ***RECOMMENDATION***

**This item is for informational purposes only.**



# QUESTIONS?

**Kent Kisselman**

*Public Works Director*

*303.450.4005*

*[kkisselman@northglenn.org](mailto:kkisselman@northglenn.org)*

**Special Meeting**  
*March 20, 2023*



**CITY OF**  
**Northglenn**

## Potential Streets to be Repaired

On Street	From Street	To Street	PCI
100TH AVE	NIVER AVE	HURON ST	34
100TH PL	CROKE DR	PECOS ST	46
103RD AVE	HURON ST	CROKE DR	36
104TH PL	ZUNI ST	POMPEY WAY (CDS)	54
105TH CT	NW END	MADISON WAY	47
105TH PL	MARION ST	FRANKLIN ST	47
106TH PL	FOX RUN CIR	MADISON WAY	45
107TH AVE	FOX RUN CIR	MADISON WAY	47
107TH CT	WEST END	MADISON WAY	40
108TH AVE	ROSALIE DR	IRMA DR	45
108TH AVE	ADAMS CT	COOK ST	48
108TH PL	LARRY DR	ROSALIE DR	47
111TH PL	LIVINGSTON DR	NAVAJO ST	43
113TH PL	WEST END	HIGHLINE DR	34
114TH AVE	HURON ST	CHEROKEE ST	49
114TH PL	E 114TH PL (CDS)	CLAUDE CT	46
114TH PL	CLARKSON ST	LARSON LN	47
114TH PL	FRANKLIN ST	IRMA DR	47
114TH PL	MELODY DR	CHEROKEE ST	37
114TH PL (CDS)	E 114TH PL	NW END	44
115TH AVE	HURON ST	CHEROKEE ST	42
115TH PL	WYCO DR	CLAUDE CT	47
98TH AVE	CROKE DR	HURON ST	49
ADAMS CIR	ADAMS ST	NE END	45
ADAMS CT	SW END	ADAMS ST	38
ADAMS ST	ADAMS CT	FOX RUN PKWY	43
BOWMAN PL	MARION ST	IRMA DR	39
BRUCE LN	W 99TH AVE	BELFORD DR	44
CARLILE ST	E 114TH PL	E 115TH AVE	37
CARLILE ST	CLARKSON ST	E 105TH PL	42
CARMELA LN	W 104TH PL	BREWER DR	39
CHEROKEE ST	W 114TH AVE	W 115TH AVE	43
CLARKSON ST	CARLILE ST	E 105TH PL	41
CORING PL	LOREN LN	LEROY DR	48
ELATI ST	MELODY DR	W 112TH AVE	45
ELMER DR	LARSON DR	E 110TH PL	48
EMERSON ST	MURIEL DR	ELMER DR	44
FOWLER DR	IRMA DR	E 114TH PL	42
FOX RUN CIR	FOX RUN PKWY	FOX RUN PKWY	45
FOX RUN PKWY	MADISON WAY	SE END	30
FRANKLIN ST	FRANKLIN WAY	E 105TH PL	49
FRANKLIN WAY	FRANKLIN ST	E 105TH PL	49
GARLAND DR	LINCOLN ST	GRANT DR	49
GRANT ST	E 117TH AVE	E 118TH AVE	47

GRAVES CT	IRMA DR	HIGH ST	48
HUMBOLDT ST	MALLEY DR	FOWLER DR	44
IRMA DR	E 112TH AVE	E 104TH AVE	43
KALAMATH ST	ROSEANNA DR	KENNEDY DR	48
LAFAYETTE ST	HUMBOLDT DR	E 119TH PL	46
LAMBERT LN	ROSEANNA DR	LIVINGSTON DR	49
LARSON DR	E 112TH AVE	MURIEL DR	43
LINCOLN DR	E 113TH PL	MALLEY DR	29
LINCOLN ST	E 117TH PL	E 118TH PL	48
LIVINGSTON DR	ROSEANNA DR	W 104TH AVE	40
LIVINGSTON DR	BETH LN	LAMBERT LN	42
LOREN LN	LEROY DR	CORING PL	44
LOU DR	FRED DR	MELODY DR	49
MADISON WAY	E 105TH CT	FOX RUN PKWY	44
MALLEY DR	WASHINGTON ST	IRMA DR	44
MANET WAY	ROSEANNA DR	DS@282FT	48
MILWAUKEE ST	E 106TH PL	FOX RUN PKWY	47
MONTEREY CIR	FOX CT	MELODY DR	47
MURIEL DR	LANCE CT	IRMA DR	42
NAIAD DR	LIVINGSTON DR	HURON ST	41
OGDEN ST	MALLEY DR	DOWNING ST	43
PALEY ST	POMPEY WAY	W 106TH AVE	39
PALMER LN	W 98TH AVE	W 99TH AVE	40
PECOS ST	URA LN	NORTH END	39
PHILLIPS DR	WASHINGTON ST	LARSON LN	48
REGINA LN	DOWNING CT	E 108TH AVE	40
RONALD LN	W 104TH PL	NAIAD DR	44
SHERMAN DR	E 113TH PL	NORTH END	42
SHERMAN ST	E 118TH PL	E 117TH PL	48
SPERRY ST	W 104TH PL	URA LN	40
SULLY WAY	SPERRY ST	URA LN	44
SYLVIA DR	WASHINGTON ST	E 120TH AVE	42
UPTON ST	UTRILLO LN	URA LN	47
URA LN	W 104TH AVE	VARESE LN	37
VERNA LN	ACOMA ST	BONITA PL	42
WYANDOT ST	W 104TH PL	VARESE LN	47