

PLANNING AND DEVELOPMENT DEPARTMENT

MEMORANDUM 17-10

DATE: February 27, 2017

TO: Honorable Mayor Joyce Downing and City Council Members

FROM: James A. Hayes, AICP, City Manager *JH*
Brook Svoboda, Director of Planning and Development *BS*
Becky Smith, AICP, Planning Manager *BS*

SUBJECT: CB-1884 –Rezoning for Certain Real Properties at 11355 York Street – North Metro Line Preliminary and Final Planned Unit Development (PUD)

Purpose

To rezone the existing parcels from Agriculture and C-4 Commercial to Planned Unit Development (PUD) to accommodate the development of a commuter rail station.

Background

Attached to this memo is a proposed ordinance, which if approved, would rezone the existing properties from Agriculture and C-4 Commercial to PUD, located at 11355 York Street. Staff presented the Preliminary and Final PUD application, and Minor Subdivision application combining the two unplatted parcels into one, to the Planning Commission at a Public Hearing held February 7, 2017. A copy of the Planning Commission Resolution and Staff Report with PUD Plan Set are included as **Attachments 1 & 2**.

The Planning Commission basis for the decision for a PUD proposal on the following criteria:

- The proposed development is compatible with the surrounding area;
- The proposed development is not inconsistent with the City's Master Plan;
- The proposed development does not adversely affect the health, safety, and welfare of the inhabitants of the area and the City of Northglenn;
- Adequate circulation exists and traffic movement will not be impeded by the proposed development; and
- Additional municipal service costs will not be incurred.

The Planning Commission Resolution 2017-03, also include the following conditions for final adoption by the Council:

1. RTD and Northglenn will enter into an Agreement for access and on-going maintenance for the Pedestrian Trail. Said agreement shall be considered by the City Council at the time of the Public Hearing for Preliminary PUD.
2. RTD will be required to enter into a Stormwater Facilities Maintenance Agreement.
3. RTD/RRP will be required to pull all necessary permits prior to construction.

Budget Implications

This ordinance amendment request has no budgetary impacts.

Schedule/Time Implications

NA

City Council Options

The City Council makes the final decision regarding Preliminary PUD. The City Council options are as follows:

1. Approve the request, with or without conditions or stipulations;
2. Deny the request for reasons stated; or
3. Table the request for further consideration.

Staff Recommendation

Staff recommends approval of CB-1884

Staff Reference

Brook Svoboda, Director of Planning and Development bsvoboda@northglenn.org or 303.450.8937

Attachments

- Attachment 1 Planning Commission Resolution 2017-03
- Attachment 2 Staff Report
- Attachment 3 Staff Presentation

**RESOLUTION 2017-03
NORTHGLENN PLANNING COMMISSION**

**A RESOLUTION PROVIDING A FAVORABLE
RECOMMENDATION TO THE CITY COUNCIL FOR APPROVAL
OF THE NORTH METRO RAIL LINE PRELIMINARY PLANNED
UNIT DEVELOPMENT (PUD) AND APPROVAL OF FINAL PUD**

WHEREAS, Northglenn Ordinance 11-37-2 requires that the Northglenn Planning Commission review and make recommendation to Council any application for rezoning; and

WHEREAS, the Northglenn Planning Commission therefore desires to make its recommendations to the City Council as required by law.

WHEREAS, the Planning Commission has found that the application satisfies the applicable criteria for rezoning and PUD criteria under 11-37-2 & 11-16-2 for purposes of approving the Final PUD respectively;

BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF NORTHGLENN, COLORADO, THAT:

Section 1. The City of Northglenn Planning Commission hereby provides a favorable recommendation to the City Council for approval of the NORTH METRO RAIL LINE PRELIMINARY PUD and approval of the FINAL PUD in accordance with the proposed Ordinance for adoption.

Section 2. The City of Northglenn Planning Commission hereby recommends that in addition to the conditions spelled out in the proposed adopting ordinance, they shall be reflected, if necessary, on the Final PUD to be recorded with the Adams County Clerk and Recorder, as required by law.

1. RTD and Northglenn will enter into an Agreement for access and on-going maintenance for the Pedestrian Trail. Said agreement shall be considered by the City Council at the time of the Public Hearing for Preliminary PUD.
2. RTD will be required to enter into a Stormwater Facilities Maintenance Agreement.
3. RTD/RRP will be required to pull all necessary permits prior to construction.

DATED this _____ day of _____, 2017

Sonia Di Carlo
Planning Commission Chair

ATTEST:

Rebecca Smith, AICP
Secretary

Case No. Z-2-17 & SUB-1-17

Applicant: Regional Rail Partners (RRP) on
behalf of Regional Transportation
District (RTD)
Location: 11355 York St
Ordinance: 11-16, 11-37-2 and 12-6-3

NORTHGLENN AT 112TH PRELIMINARY & FINAL PLANNED UNIT DEVELOPMENT (PUD) AND MINOR SUBDIVISION STAFF REPORT AND RECOMMENDATION

REQUEST:

The applicant, Regional Rail Partners (RRP) on behalf of the owner the Regional Transportation District (RTD), requests, the Planning Commission hear and recommend to the City Council, a change of zoning for Preliminary Planned Unit Development (PUD) of the parcel located at 11355 York Street, grant approval of the Final PUD (**Exhibit A**), and approve a Minor Subdivision that will combine two (2) existing parcels into one.

REASON FOR REQUEST:

The applicant proposes to combine and rezone the two (2) existing parcels currently zoned C-4 Commercial with Conditional zoning and Agriculture to PUD in order to develop a commuter rail platform, parking lot and driver relief station (DRS) (**EXHIBIT B & C**) to serve the North Metro Rail Line.

BACKGROUND:

In 2018, the Denver area Regional Transportation District (RTD) is scheduled to open the North Metro electric commuter rail line, the culmination of years of planning and inter-governmental cooperation between the agency and the communities along the 18.5-mile line. The North Metro line is focused on serving the rapidly growing northern suburbs of the Denver metro area, including the Cities of Northglenn and Thornton. One of the North Metro line's stations is located at E. 112th Ave., on the east side of the City of Northglenn adjacent to the City of Thornton's municipal limits at E. 112th Ave. and York Street.

The City's Zoning Ordinance does not have a zone district that contemplates or allows for a station platform, parking area and DRS, as a use by right. Therefore, the applicant has requested that the Planning Commission hear and make recommendation on the Preliminary PUD and also hear and approve the Final PUD (contingent upon City Council approving the Preliminary PUD) during this hearing. If the Planning Commission makes a recommendation on the Preliminary PUD and approves the Final PUD, the recommendation will be forwarded to the City Council for approval.

In conjunction with this request, the development will also be subject to Minor Subdivision. Planning Commission has final approval for Minor Subdivisions.

The Planning Commission recommendation of the Preliminary and Final PUD are based on the criteria from Sections 11-16-2 and 11-37-2 of the Northglenn Zoning Ordinance, outlined in the Approval Criteria Section below.

SURROUNDING LAND USES:

North:	Northglenn Moose Lodge, C-4 Commercial
South:	Vacant, Agriculture
West:	RTD Right of Way, Multi-Family Residential, R-1-C
East:	Vacant Properties (City of Thornton)

CASE ANALYSIS:

The analysis of the case was completed with the review of submitted materials for the Preliminary PUD, Final PUD and Minor Subdivision.

MINOR SUBDIVISION (See Attachment C)

Regulatory Criteria

Chapter 12 – Subdivision Regulations offers limited approval criteria for a minor subdivision. Staff has outlined a series of findings of fact at the end of this report that are in context with the intent of the Subdivision Ordinance and requirements of content and form. The parcel's legal description is:

Southeast quarter section 2, Township 2 South, Range 68 West of the 6th P.M. City of Northglenn, County of Adams, State of Colorado.

Lot Layout

To date, these parcels have not been formally subdivided through a City of Northglenn process. To facilitate the development, a minor subdivision is being performed to combine the two parcels into one and create a formal City subdivision. The lot size is 3.988 acres.

Utility Easements

A 20' waterline easement is being dedicated, through the plat, as part of this process. A drainage easement is also being dedicated around the proposed detention pond facility. This allows future access to the detention facility if City maintenance is required.

PLANNED UNIT DEVELOPMENT (See Attachment B)

Land Use / Site Plan

The Northglenn at 112th Station proposal shows development of the commuter rail platform occurring in the RTD right-of-way, development of 315 space parking lot, and driver relief station (DRS). There will be three (3) access points off of York Street, one will be for the bus loop, which only buses will utilize. The other two (2) access points are for vehicles to access the parking lot and station platform.

The site plan shows that a 5'-8' pedestrian path will be provided in the RTD right-of-way (parallel to the tracks). RTD and the City will enter into an agreement to allow the access and determine maintenance for the trail. Staff will be recommending a condition that this agreement be brought to City Council for consideration at that same time as the Preliminary PUD.

The second sheet of the PUD includes the Development Standards proposed for the site. These standards address parking, lighting, and landscaping requirements, as well as site performance standards and architectural standards. Maximum height is proposed at 35' and the setbacks for any building structure are as follows:

- Front: 25 feet
- Side: 10 feet
- Rear: 10 feet

Traffic & Parking

The Environmental Impact Statement (EIS) included an analysis of the traffic impacts due to the station being developed and required mitigation. Sheet X-100 include improvements to York Street, showing the future lane widening to accommodate bus right turn movements. The sheet also shows an extension of the existing sidewalk along 112th Ave. to York St. Sheet X-101 shows the future widening of 112th Avenue on the north side to accommodate right turn movements into York Street and the installation of future intersection improvements including signalization at the intersection of 112th and Fox Run Pkwy.

The Station has been designated as a "kiss and ride", so will not requires as much parking. 315 spaces are proposed in the PUD. Staff has reviewed this and finds that this is sufficient parking for this station.

Landscaping

Total landscape area equates to 15%. This landscaping exceeds the City's standard of 12% required on-site landscaping. The landscape plan is providing tree quantities in accordance with the City's ordinance. No prohibited species are proposed.

Architecture/Elevations

Elevations of the station platform are included with the submittal. The pillars holding up

the canopy and the canopy are intended to represent trees to honor Northglenn's history as a Tree City. The Canopy will have a green tint and the pillars will be covered in a textured metal that is painted brown.

The Northglenn Arts and Humanities Foundation is interested in adding an art program to this station, but has not yet identified specifics for the program. There will be a several of opportunities for art to be integrated into the site, including but not limited to, the windscreens on the platform, the vertical wall, and the ability to add a installation in the bus loop area with the option of lighting. While the budget and scope of an art program have not been determined at this time, approval of the PUD will ensure that these installations could occur a future date.

Utilities

The applicant is connecting to the City of Northglenn water and is showing a looped water line that will be placed in the City of Thornton's right-of-way. The applicant will be required to obtain necessary permits from the City of Northglenn and Thornton to construct the waterline in their right-of-way.

Public utility easements are dedicated to the City through the proposed Plat, including a waterline easement and detention easement. A portion of the utilities will be dedicated to the City as part of the City's mainline infrastructure. The City will operate and maintain those utilities once formally dedicated to the City.

Drainage

The proposal included onsite detention that will mitigate the impacts of this development. Public Works has reviewed and approved the drainage plan. A Stormwater Facilities Maintenance Agreement will be a condition of approval – this will ensure ongoing maintenance of the facility. A Stormwater Facilities Maintenance Agreement shall be executed prior to the issuance of the Certificate of Occupancy.

Administration

In accordance with 11-16-6 Approval Criteria, staff has found that the proposal for the Preliminary and Final PUD is in conformity with the Northglenn Zoning Ordinance and Comprehensive Plan.

The North Metro Fire Rescue District has reviewed the submitted plans and has no objections to the proposal. At time of construction the applicant will be required to meet all code requirements of the North Metro Fire Rescue District and the City of Northglenn's Chief Building Official.

The scheduled public hearing for this application was noticed in the legal section of the Northglenn Sentinel. Property owners surrounding the proposed facility were notified via mail of the proposal, as required by Section 11-41-3(a) of the City's Zoning Ordinance. In addition, the property in question was posted as required by Section 11-41-3(b).

APPROVAL CRITERIA:

Section 11-16-6-Approval Criteria:

- (a) *Prior to granting approval for any preliminary or final planned unit development plan, the Planning Commission and City Council shall give consideration to the purpose and intent of this Article 16 as listed in Section 11-16-2, above, and those criteria for any rezoning request as listed in Section 11-37-2(h) of this Chapter 11 of the Municipal Code. In addition, prior to approval of any final planned unit development plans, the Planning Commission shall find them to be in conformity with the approved preliminary planned unit development plans.*

Section 11-16-2 reads as follows:

Purpose and Intent.

(a) *The purpose and intent of this Article is to create a Planned Unit Development regulation which will maximize the land resources available within the City of Northglenn. More specifically, the purposes are as follows:*

- (1) To ensure that zoning decisions are directed toward achievement of community goals and objectives, as embodied in the adopted Master Development Plan, rather than satisfying arbitrary minimum standards.*
- (2) To recognize the unique nature of each parcel of land and allow flexibility in the planning and design response.*
- (3) To encourage and promote innovation in land development by allowing more variety in type, design, layout, and architectural design of buildings and site amenities.*
- (4) To ensure that proposed rezonings and specific proposals presented to staff and the Planning Commission are constructed as presented and not used merely as a tool to facilitate property sale or some other transfer of real estate to make it more marketable.*
- (5) To encourage economic development to expand the City's tax base and provide employment opportunities.*
- (6) To create a PUD ordinance that streamlines, consolidates and simplifies zoning and subdivision, an ordinance that expedites rather than hinders the development process.*
- (7) To allow for the preservation of natural site amenities such as mature trees, water bodies and channels, exceptional views, etc.*

- (8) To promote the judicious use of open space and landscaped areas with regard to utility and visual impact.*
- (9) To encourage a harmonious and integrated mix of land uses and require compatibility between new and existing development.*
- (10) To provide for the efficient and safe movement of vehicular and pedestrian traffic on both public and private roadways. “*

Additionally;

Section 11-37-2-Zone Changes

(h) Basis for Approval. The Commission and Council shall give consideration to and satisfy themselves to the following:

- (1) That a need exists for the proposal;*
- (2) That this particular parcel of ground is indeed the correct site for the proposed development;*
- (3) That there has been an error in the original zoning; or*
- (4) That there have been significant changes in the area to warrant a zone change;*
- (5) That adequate circulation exists and traffic movement would not be impeded by development; and*
- (6) That additional municipal service costs will not be incurred which the City is not prepared to meet.*

APPROVAL CRITERIA ANALYSIS

- (1) That a need exists for the proposal*

Staff finds that a need exists in that it creates an opportunity for residents of Northglenn and the surrounding area access to commuter rail as a mode of travel.

- (2) That this particular parcel of ground is indeed the correct site for the proposed development;*

Staff finds the proposed development is appropriate for this site based on the configuration of the North Metro rail line and the proximity to existing residential and business development.

- (3) That there has been an error in the original zoning; or*

Not Applicable

- (4) That there have been significant changes in the area to warrant a zone change;*

Staff finds that construction of the North Metro rail line through the City of Northglenn does warrant a zone change to allow for a stop within the city limits.

- (5) That adequate circulation exists and traffic movement would not be impeded by development; and

Traffic and circulation impacts have been studied in an Environmental Impact Statement and will be mitigated accordingly.

- (6) That additional municipal service costs will not be incurred which the City is not prepared to meet.

Staff finds that there will be no additional municipal service costs incurred.

COMMISSION OPTIONS:

The Planning Commission is making a recommendation for Preliminary PUD to the City Council and a final decision regarding the Final PUD and the Minor Subdivision. The Commissions options are as follows:

- 1) Approve the request, with or without conditions or stipulations;
- 2) Deny the request for reasons stated; or
- 3) Table the request for further consideration.

STAFF RECOMMENDATION:

Staff recommends a recommendation of the Preliminary PUD to the City Council and approval of the Final PUD and Minor Subdivision with the following conditions and based on six recommended Findings of Fact per the criteria of Section 11-37-2 of the Northglenn Zoning Ordinance.

RECOMMENDED CONDITIONS OF APPROVAL:

1. RTD and Northglenn will enter into an Agreement for access and on-going maintenance for the Pedestrian Trail. Said agreement shall be considered by the City Council at the time of the Public Hearing for Preliminary PUD.
2. RTD will be required to enter into a Stormwater Facilities Maintenance Agreement.
3. RTD/RRP will be required to pull all necessary permits prior to construction.

RESPECTFULLY SUBMITTED:

Becky Smith, AICP
Planning Manager

WHAT'S NEXT: The City Council will next hear the preliminary PUD portion of this application. If the Commission recommends approval of the preliminary portion and approves the Final PUD portion at the hearing then the rezoning will be finalized.

FUTURE APPLICATIONS:

- | | |
|--|--|
| <i>1. Preliminary PUD</i> | <i>Considered by City Council – TBD</i> |
| <i>2. ROW / Grading Permits</i> | <i>Approved administratively by staff</i> |
| <i>3. Building Permits</i> | <i>Approved administratively by staff</i> |

EXHIBIT A



**RESOLUTION 2017-02
NORTHGLENN PLANNING COMMISSION**

**A RESOLUTION PROVIDING APPROVAL OF THE NORTHGLENN
NORTHGLENN AT 112TH - SUBDIVISION PLAT**

WHEREAS, Northglenn Ordinance 12-6-3 requires that the Northglenn Planning Commission review and approve or deny any application for Minor Subdivision Plat; and

WHEREAS, the Northglenn Planning Commission therefore desires to make its decision as required by law.

WHEREAS, the Planning Commission has found that the application satisfies the applicable criteria for approval of a Minor Subdivision plat;

BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF NORTHGLENN, COLORADO, THAT:

Section 1. The City of Northglenn Planning Commission hereby provides approval of the NORTHGLENN AT 112TH plat in accordance with the applicable Ordinance 12-2-2(a)(3).

Dated at Northglenn, Colorado, this _____ day of _____, 2017.

DATED this _____ day of _____, 2017

Sonia Di Carlo
Planning Commission Chair

ATTEST:

Becky Smith, AICP
Secretary

**RESOLUTION 2017-03
NORTHGLENN PLANNING COMMISSION**

**A RESOLUTION PROVIDING A FAVORABLE
RECOMMENDATION TO THE CITY COUNCIL FOR APPROVAL
OF THE NORTH METRO RAIL LINE PRELIMINARY PLANNED
UNIT DEVELOPMENT (PUD) AND APPROVAL OF FINAL PUD**

WHEREAS, Northglenn Ordinance 11-37-2 requires that the Northglenn Planning Commission review and make recommendation to Council any application for rezoning; and

WHEREAS, the Northglenn Planning Commission therefore desires to make its recommendations to the City Council as required by law.

WHEREAS, the Planning Commission has found that the application satisfies the applicable criteria for rezoning and PUD criteria under 11-37-2 & 11-16-2 for purposes of approving the Final PUD respectively;

BE IT RESOLVED BY THE PLANNING COMMISSION OF THE CITY OF NORTHGLENN, COLORADO, THAT:

Section 1. The City of Northglenn Planning Commission hereby provides a favorable recommendation to the City Council for approval of the NORTH METRO RAIL LINE PRELIMINARY PUD and approval of the FINAL PUD in accordance with the proposed Ordinance for adoption.

Section 2. The City of Northglenn Planning Commission hereby recommends that in addition to the conditions spelled out in the proposed adopting ordinance, they shall be reflected, if necessary, on the Final PUD to be recorded with the Adams County Clerk and Recorder, as required by law.

1. RTD and Northglenn will enter into an Agreement for access and on-going maintenance for the Pedestrian Trail. Said agreement shall be considered by the City Council at the time of the Public Hearing for Preliminary PUD.
2. RTD will be required to enter into a Stormwater Facilities Maintenance Agreement.
3. RTD/RRP will be required to pull all necessary permits prior to construction.

DATED this _____ day of _____, 2017

Sonia Di Carlo
Planning Commission Chair

ATTEST:

Rebecca Smith, AICP
Secretary

LEGAL DESCRIPTION OF PROPERTY

NORTHGLENN AT 112TH OF THE RTD NORTH METRO CORRIDOR COMMUTER RAIL PROJECT, BEING ALL OF THE TRACTS OF LAND DESCRIBED AT RECEPTION NO. 2015000064551, RECORDED ON AUGUST 6, 2015, AND RECEPTION NO. 2015000053437, RECORDED ON JULY 7, 2015, BOTH IN THE ADAMS COUNTY CLERK AND RECORDER'S OFFICE, LOCATED IN THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO, ADDITIONALLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE EAST QUARTER CORNER OF SAID SECTION 2 (A FOUND 3-1/4" ALUMINUM CAP IN RANGE BOX STAMPED "JR ENG T2S R68W 1/4 S2 S1 1998 PLS 24308"; WHENCE THE SOUTHEAST CORNER OF SAID SECTION 2 (A FOUND 2-1/2" ALUMINUM CAP SET ON AN AXLE IN RANGE BOX STAMPED "T2S R68W S2 S1 S11 S12 2000 PLS 28286") BEARS S00°34'18"E A DISTANCE OF 2,627.20 FEET (BASIS OF BEARING - ASSUMED); THENCE N89°09'58"W, COINCIDENT WITH THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 30.01 FEET TO THE WESTERLY RIGHT OF WAY LINE OF YORK STREET; THENCE S00°34'18"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 1,252.97 FEET TO THE NORTHEASTERLY CORNER OF SAID TRACT OF LAND DESCRIBED AT RECEPTION NO. 2015000064551, ALSO BEING THE POINT OF BEGINNING;

THENCE S00°34'18"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 674.65 FEET TO THE SOUTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000053437; THENCE S89°26'30"W, COINCIDENT WITH SAID SOUTHERLY LINE, A DISTANCE OF 305.39 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE RTD NORTH METRO CORRIDOR AS DESCRIBED AT RECEPTION NO. 2009000046216, RECORDED JUNE 26, 2009 IN SAID CLERK AND RECORDER'S OFFICE; THENCE N07°30'24"E, COINCIDENT WITH SAID EASTERLY RIGHT OF WAY LINE, A DISTANCE OF 681.34 FEET TO THE NORTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000064551; THENCE N89°25'42"E, COINCIDENT WITH SAID NORTHERLY LINE, A DISTANCE OF 209.65 FEET TO THE POINT OF BEGINNING.

CONTAINING 173,727 SQUARE FEET, (3.988 ACRES), MORE OR LESS.

Keneth W. Jacobs
Prepared by: 9/30/15
Kenneth W. Jacobs, P.E. 2992
For and on behalf of Jacobs Engineering Group Inc.
707 17th Street, #3400
Denver, CO 80202
303.820.5240

GENERAL NOTES

RTD IS RESPONSIBLE FOR MAINTENANCE OF COMMON AND RIGHT-OF-WAY LANDSCAPE.

ALL WATER AND SANITARY SEWER CONSTRUCTION SHALL CONFORM TO CITY OF NORTHGLENN UTILITY STANDARDS AND SPECIFICATIONS.

INSTALLATION OF ALL UTILITY TAPS SHALL BE OBSERVED BY CITY OF NORTHGLENN PERSONNEL.

CONSTRUCTION OF THE YORK STREET PUBLIC RIGHT-OF-WAY SHALL BE IN CONFORMANCE WITH CITY OF THORNTON STREET STANDARDS AND SPECIFICATIONS. ALL OTHER CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY SHALL BE IN CONFORMANCE WITH CITY OF NORTHGLENN STREET AND DRAINAGE STANDARDS AND SPECIFICATIONS.

ALL CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY SHALL MEET THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT OF 1990.

PRIOR TO ISSUANCE OF A BUILDING PERMIT, THE CITY MUST REVIEW AND APPROVE THE FOLLOWING:

1. GRADING AND DRAINAGE PLAN REPORT
2. EROSION CONTROL PLAN
3. WATER AND SANITARY SEWER UTILITY CONSTRUCTION PLAN
4. LANDSCAPE PLAN
5. IRRIGATION PLAN
6. COMPLETE CONSTRUCTION PLANS FOR ALL REQUIRED PUBLIC IMPROVEMENTS

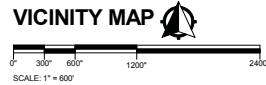
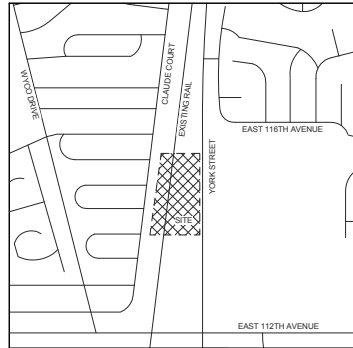
ALL EXISTING METER VAULTS, UTILITY SERVICE LINES APPURTENANCES, STREET CURB, GUTTER, SIDEWALK AND TRAIL ON AND ADJACENT TO THIS SITE SHALL BE REPAIRED AS NECESSARY AND BROUGHT TO CURRENT CITY STANDARDS.

STREET LIGHTS ON PUBLIC STREETS ADJACENT TO THIS SITE, IF NECESSARY, SHALL BE DESIGNED AND INSTALLED BY XCEL ENERGY ACCORDING TO CITY APPROVED STANDARDS AND PAID FOR BY THE DEVELOPER OR PROPERTY OWNER PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY.

DESCRIPTION OF PROPOSED ARCHITECTURAL USE, STYLE, AND MATERIALS

112TH STATION IS A SINGLE SIDE PLATFORM CONFIGURATION WITH A TRANSITION PLAZA AND PARK-N-RIDE INCLUDING 315 PARKING SPACES. THE ARCHITECTURE AND LANDSCAPE APPROACH INCLUDES A TREE LIKE CANOPY STRUCTURE REMINISCENT OF THE MANICURED PARKS OF THE 1960S INGRAINED IN THE SUBURBAN LIFESTYLE AND MODERN AMERICAN LIVING. THE PLAZA INCLUDES TWO LARGE PLANTERS AND A MANICURED TREE LAWN ALONG YORK STREET. THE PLANNING CONCEPT INCLUDES A PALETTE OF DIVERSE, LOW WATER SPECIES TO SUPPORT NORTHGLENN'S WATER CONSERVING MEASURES.

PRELIMINARY AND FINAL P.U.D
NORTH METRO RAIL LINE
P.U.D. 112TH AVENUE STATION, NORTHGLENN, COLORADO
COVER SHEET

**SITE DATA TABLE**

LOT SIZE	173,727 SQ FT	3.99 ACRES	
BUILDING FLOOR AREA	9,500 SQ FT ALLOWABLE PER IBC		
BUILDING CONSTRUCTION TYPE	EXISTING	PROPOSED	
	C-4 & A-1	P.U.D.	
ZONING DISTRICT	ALLOWED	PROPOSED	
LAND USE	35	DRS: 1 STORY, 12' - 7"	
BUILDING HEIGHT	REQUIRED	PROPOSED	
FLOOR AREA RATIO	25'	578.11'	
	EAST	10'	244.06'
	SOUTH	10'	73.06'
	WEST	10'	28.42'
	PARKING SPACES	310	315 TOTAL SPACES
	LOADING SPACES		NONE
OFF-STREET PARKING	HANDICAP SPACES	7	6 REGULAR ADA SPACES, 2 VAN ACCESSIBLE ADA SPACES
LANDSCAPE AREA	15%	56,332 SQ FT	32%

LAND USE TABLE

SURFACE	AREA (SQ. FT.)
STATION PLAZA AND SIDEWALK	33,905
STATION PARKING LOT	106,957
BUS LOOP	19,274
RETENTION BASIN ACCESS ROAD	1,348
LANDSCAPE AREA	45,533
PLATFORM	5,440
DRIVER RELIEF STATION	213

PUBLIC IMPROVEMENTS

PUBLIC IMPROVEMENTS IDENTIFIED BY THE DESIGN OF THIS PROJECT INCLUDE:	
YORK ST. IMPROVEMENTS	\$410,000
8" WATER MAIN IMPROVEMENTS	\$245,200
STATION FRONTAGE IMPROVEMENTS (SIDEWALK, LANDSCAPING)	\$37,500

SHEET INDEX

COVER PAGE
PUD STANDARDS PAGE
CIVIL
CS-6200 - OVERALL SITE PLAN
UB-6100 - OVERALL UTILITY PLAN
DJ-6204 - DRAINAGE BASIN MAP - PROPOSED

LANDSCAPE
LS-6101 - PLANTING PLAN - SOUTH
LS-6102 - PLANTING PLAN - NORTH
LS-6200 - PLANTING NOTES

ARCHITECTURAL
AS-6200 - PLATFORM ELEVATIONS
AS-6201 - STATION ELEVATIONS

ELECTRICAL
ES-6103 - SITE PHOTOMETRIC PLAN - SOUTH
ES-6104 - SITE PHOTOMETRIC PLAN - NORTH
ES-6213 - PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH
ES-6214 - PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER
ES-6215 - PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH
ES-6903 - LUMINAIRE SCHEDULE
ES-1800 - PEDESTRIAN LIGHT POLE DETAILS
ES-1808 - PARKING LOT LIGHT POLE DETAILS

SUPPLEMENTAL
X-100 - 112TH AVE & YORK STREET SIGNING AND STRIPING PLAN
X-101 - 112TH AVE & FOX RUN SIGNING AND STRIPING PLAN

OWNER

RTD
1600 BLAKE STREET
DENVER, CO 80202

ARCHITECT OF RECORD

IRON HORSE ARCHITECTS, INC.
475 17TH ST, #720
DENVER, CO 80202
720-855-7572

CONSTRUCTION SCHEDULE

THIS SITE WILL DEVELOP IN A LOGICAL SEQUENCE BEGINNING WITH STATION PLATFORM WORK, UNDERGROUND UTILITIES, SITE GRADING, HARDSCAPES, THEN LANDSCAPING AND FINISHES.

THE FOLLOWING MILESTONES ARE ANTICIPATED TO OVERLAP WITH A FINAL COMPLETION BY JANUARY 2018:

- A) PLATFORM WORK (3-4 MONTHS): INCLUDES CONCRETE FOUNDATIONS AND UNDERGROUND ELECTRICAL SYSTEMS.
- B) UNDERGROUND UTILITIES (3 MONTHS): INCLUDES PARKING LOT DRAIN PIPING, WATER MAIN EXTENSIONS, AND SEWER SERVICE CONNECTING TO SERVICE IN 112TH AVE.
- C) SITE GRADING (2 MONTHS): INCLUDES BASE COURSE FOR HARDSCAPES AND GRADING OF THE WATER QUALITY POND.
- D) HARDSCAPES (3 MONTHS): INCLUDES CURB AND GUTTER, ASPHALT PAVING, CONCRETE PAVING (BUS LOOP), PLAZA PAVING, AND SIDEWALKS.
- E) LANDSCAPING AND FINISHES (4 MONTHS): INCLUDES IRRIGATION SYSTEMS, LANDSCAPING, SIGNS, LIGHT POLES/FIXTURES, PLATFORM CANOPIES, PLATFORM GUARDRAIL, AND BUS SHELTERS.

ARCHITECT'S CERTIFICATE:

I, VIRGINIA MCALLISTER, A REGISTERED PROFESSIONAL ARCHITECT, DO HEREBY CERTIFY THAT THE PLANS FOR THE P.U.D. NORTH METRO RAIL LINE 112TH AVENUE STATION WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

VIRGINIA MCALLISTER _____ DATE _____

ENGINEER'S CERTIFICATE:

I, ROB PRATT, A REGISTERED PROFESSIONAL ENGINEER, DO HEREBY CERTIFY THAT THE PLANS FOR THE P.U.D. NORTH METRO RAIL LINE 112TH AVENUE STATION WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

ROB PRATT _____ DATE _____

OWNER'S CERTIFICATE:

I, ASHLAND VAUGHN, AS PROJECT MANAGER OF THE NORTH METRO RAIL LINE FOR RTD, OWNER OF THE PROPERTY DESCRIBED HEREIN, KNOWN AS THE NORTHGLENN 112TH SUBDIVISION.

ASHLAND VAUGHN _____ DATE _____

MAYOR ATTEST: CITY CLERK

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE MAYOR OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

MAYOR _____

ATTEST: CITY CLERK _____

PLANNING COMMISSION CHAIRPERSON

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE PLANNING COMMISSION CHAIRPERSON OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

PLANNING COMMISSION, CHAIRPERSON _____

DIRECTOR OF PLANNING AND DEVELOPMENT

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE DIRECTOR OF THE DEPARTMENT OF PLANNING AND DEVELOPMENT OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

PLANNING AND DEVELOPMENT, DIRECTOR _____

DIRECTOR PUBLIC WORKS AND UTILITIES

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE DIRECTOR OF PUBLIC WORKS AND UTILITIES OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

PUBLIC WORKS AND UTILITIES, DIRECTOR _____

ADAMS COUNTY CLERK AND RECORDER

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BYCOUNTY CLERK OF THE CITY OF ADAMS COUNTY OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

CLERK AND RECORDER _____

NOTARY:

STATE OF COLORADO
COUNTY OF ADAMS
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS _____

DAY OF _____, 2016 BY _____

MY COMMISSION EXPIRES _____ DATE _____

PRELIMINARY AND FINAL DEVELOPMENT PERMIT
NORTH METRO RAIL LINE
P.U.D. 112TH AVENUE STATION, NORTHGLENN, COLORADO
PLANNED UNIT DEVELOPMENT STANDARDS

(a) Purpose. The RTD Planned Unit Development serves to provide high-quality, environmentally friendly public transit service and facilities on established corridors. The P.U.D Standards provide accessible, and convenient services capable of attracting and promoting development and investment around stations and along corridors.

(b) All regulations not specified in these Planned Unit Development (P.U.D) standards for the development shall conform to the requirements for each use or most compatible zoning districts as outlined within the City Code in effect at the time of development as determined by the Planning and Development Director. The Planning and Development Director may administratively approve a change to these P.U.D Standards if the requested change is determined by the Planning and Development Director to be a minor amendment to the P.U.D Standards.

(c) The Director of Planning and Development or designee may authorize minor changes or adjustments through an administrative amendment to the approved P.U.D in response to alterations needed when specific applications are submitted. These changes can include (but are not limited to) the reconfiguration, relocation, or reorientation of building pad sites, parking areas and common landscape areas. Minor amendments to the P.U.D Standards must meet the intent of the project.

(d) Main uses permitted.

- (1) Temporary Uses permitted only through the approval of a Temporary Use Permit (TUP).
- Seasonal sales stand
 - Food truck
- Temporary construction trailers and offices are permitted, provided that construction offices and trailers are removed from the site within 30 days of the issuance of a Certificate of Occupancy for the building to which the office is related.

(2) Transportation uses.

(3) All uses necessary for Transit operations including but not limited to:

- Transit passenger shelters and canopies
- Ticket kiosks
- Passenger platforms
- Bus stops and shelters
- Bike lockers
- Parking structures
- Pedestrian bridges or walkways
- Operations support facilities
- Driver's relief facilities
- Surface parking
- Parking Garage

(4) Utility and public service uses.

- Local utilities

(5) Commercial Mobile Radio Service facilities as allowed in the Zoning Ordinance (CMRS)

(6) Accessory uses.

- Accessory outside display
- Accessory outside sales
- Accessory outside storage
- Artwork
- Attached telecommunication antenna to existing structure
- Photovoltaic Systems (PV Power systems)

(e) Maximum height of any structure is 35' feet.

b. Building Structure Setbacks:

- Front: 25 feet
- Side: 10 feet
- Rear: 10 feet

(f) Parking

- a. Setbacks are to be a minimum 15' from collector and arterial streets.

(g) Lighting

1. An overall lighting plan shall be developed in order to coordinate with public street lighting and to establish a hierarchy of illuminated areas.
2. Site lighting shall be even across public spaces, avoiding dark or overly bright areas.
3. Lighting shall be located, oriented, and shielded so as to reduce glare for surrounding buildings, especially residential buildings.
4. Maximum height of pole lights shall not exceed 30' feet in height.
5. All fixtures require full shielding where light emissions are directed downward.

(h) Landscape Requirements

1. Minimum amount of landscaped area per lot is 15%. Plaza areas with landscape elements such as trees in grates, benches, and bike racks qualify towards the 15% landscape area requirement).
2. Along all collector and arterial streets adjacent to or within the P.U.D, there shall be a minimum 15' foot wide landscaped area measured from the curb of the street to any building or parking area. Landscaped width may include paved sidewalks or plaza areas. Along all collector and arterial street right-of-ways.
3. No more than 25 percent of the ground surface area can be exposed gravel or mulch.
4. Within all parking areas, there shall be landscaped end islands and interior islands provided to break up the large expanse of parking area.
5. Landscaping of parking islands shall be at least one tree and five shrubs for each 150 square feet of island unless utilities, traffic sight lines, and heavy anticipated pedestrian traffic make shrubs and trees impractical or as determined through the Development Permit process.
6. All land disturbed by grading shall be re-vegetated with adequate vegetative cover within one year of the disturbance. All finished grades shall emulate natural conditions with all cuts and fill slopes blended back into natural grade with smooth, rounded transitions rather than distinct, angular grade breaks.
7. A change in plant materials over more than five percent (5%) of the total landscaped area shall require submission of an amended landscape plan to the City of Northglenn Planning and Development Department, and such amended landscape plan may be administratively approved by the Director of Planning and Development. Any change over 5% is subject to Planning Commission approval.

(i) Architectural Standards

1. The architectural finish or surface plane, change must be varied along the façade of the building.
2. All buildings shall be designed with the approach of 360 degree architecture. All facades must use the same finish materials, but the percentage of each

type of material used can vary from the front façade. Architectural finishes must be evident on all facades visible from any adjoining property or street.

(j) Site Furnishings

1. Site furnishings shall be designed according to a consistent form, materials, and color. This design shall be coordinated and consistent with the station lighting, and signage.
2. Location of seating shall take into account the needs of transit users and visitors.
3. Benches and bike racks and bike lockers should be provided where appropriate to serve bus stops and other specific needs.

(k) Fences and Retaining Walls not related to transit operations

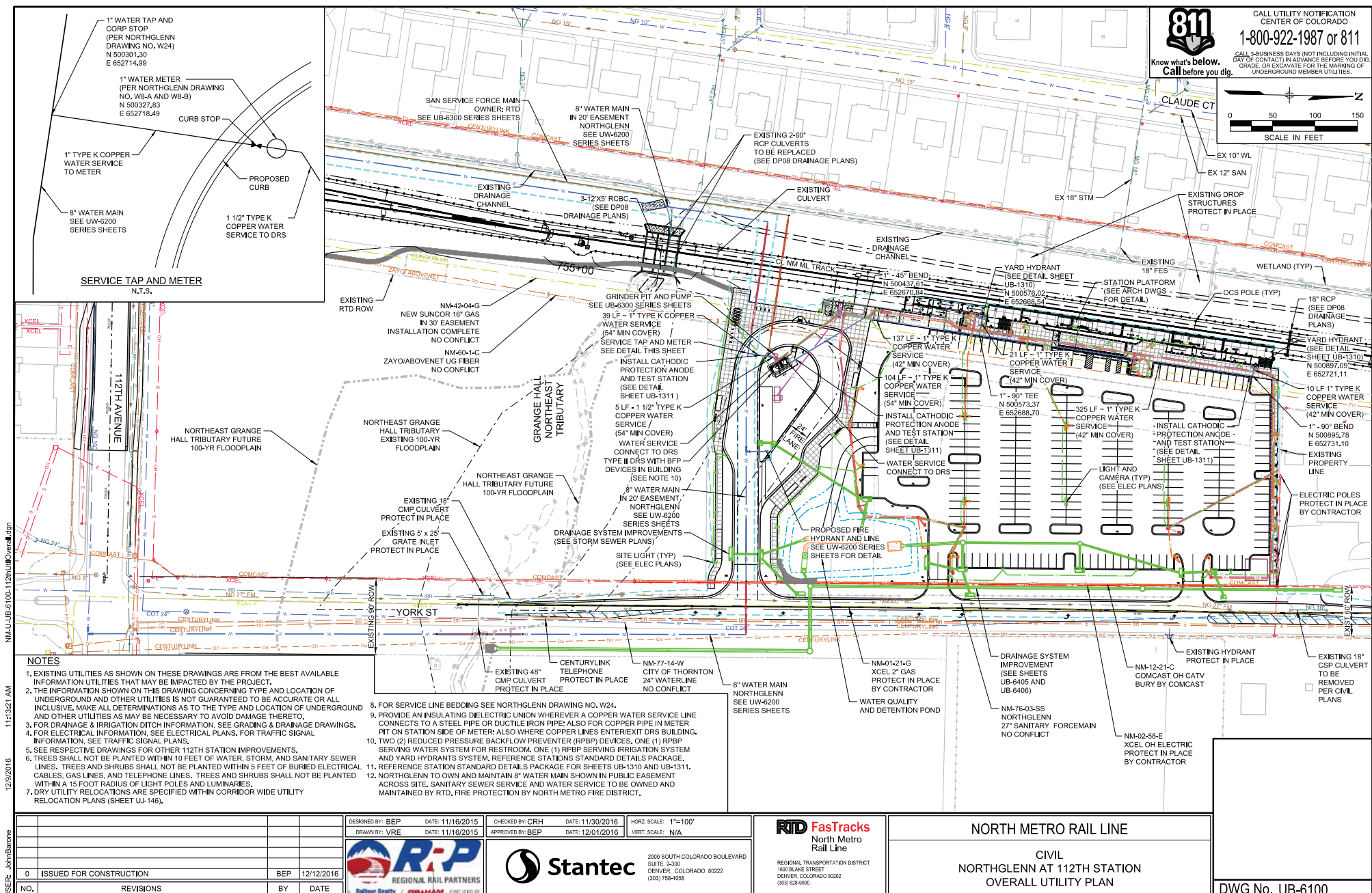
1. Fences and walls should generally contribute to the visual quality of the project or development, while being by design as unobtrusive as possible. A combination of fencing and landscaping should be incorporated wherever practicable.
2. Walls and fencing shall be constructed of materials and finishes that are compatible with, and complementary of, the adjacent building architecture. Fences and walls should be constructed from durable material such as concrete, stone, brick, metal having a dark finish, or any combination approved through the development permit process.


(l) Signs not related to transit operations

1. All other signs are subject to Chapter 21 of the City of Northglenn Municipal Code, the City of Northglenn Sign Code.

(m) General

1. Other issues, requirements or specifications not covered by these standards will be addressed by the general provisions, regulations and standards of the City of Northglenn Municipal Code in effect at the time of building permit application.





CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811

CALL 3-BUSINESS DAYS NOT INCLUDING INITIAL
DATE OF CONTACT IN ADVANCE BEFORE YOU DIG,
GRADE, OR EXCAVATE FOR THE MARKING OF
UNDERGROUND MEMBER UTILITIES.

Know what's below.
Call before you dig.


0 50 100 150
SCALE IN FEET

CLAUDE CT


EX 10" WL
EX 12" SAN
EX 18" STM
EXISTING DROP
STRUCTURES
PROTECT IN PLACE
WETLAND (TYP)
OCS POLE (TYP)
18" RCP
(SEE DP08
DRAINAGE
PLANS)
YARD HYDRANT
(SEE DETAIL
SHEET UB-1310)
N 500897.09
E 652721.11
10 LF 1" TYPE K
COPPER WATER
SERVICE
(42" MIN COVER)
1" - 90° BEND
N 500895.78
E 652731.10
EXISTING
PROPERTY
LINE
ELECTRIC POLES
PROTECT IN PLACE
BY CONTRACTOR

- NOTES**
- EXISTING UTILITIES AS SHOWN ON THESE DRAWINGS ARE FROM THE BEST AVAILABLE INFORMATION UTILITIES THAT MAY BE IMPACTED BY THE PROJECT.
 - THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. MAKE ALL DETERMINATIONS AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.
 - FOR DRAINAGE & IRRIGATION DITCH INFORMATION, SEE GRADING & DRAINAGE DRAWINGS.
 - FOR ELECTRICAL INFORMATION, SEE ELECTRICAL PLANS, FOR TRAFFIC SIGNAL INFORMATION, SEE TRAFFIC SIGNAL PLANS.
 - SEE RESPECTIVE DRAWINGS FOR OTHER 112TH STATION IMPROVEMENTS.
 - TREES SHALL NOT BE PLANTED WITHIN 10 FEET OF WATER, STORM, AND SANITARY SEWER LINES. TREES AND SHRUBS SHALL NOT BE PLANTED WITHIN 5 FEET OF BURIED ELECTRICAL CABLES, GAS LINES, AND TELEPHONE LINES. TREES AND SHRUBS SHALL NOT BE PLANTED WITHIN A 15 FOOT RADIUS OF LIGHT POLES AND LUMINAIRES.
 - DRY UTILITY RELOCATIONS ARE SPECIFIED WITHIN CORRIDOR WIDE UTILITY RELOCATION PLANS (SHEET UJ-146).
 - FOR SERVICE LINE BEDDING SEE NORTHGLENN DRAWING NO. W24.
 - PROVIDE AN INSULATING DIELECTRIC UNION WHEREVER A COPPER WATER SERVICE LINE CONNECTS TO A STEEL PIPE OR DUCTILE IRON PIPE; ALSO FOR COPPER PIPE IN METER PIT ON STATION SIDE OF METER; ALSO WHERE COPPER LINES ENTER/EXIT DRS BUILDING.
 - TWO (2) REDUCED PRESSURE BACKFLOW PREVENTER (RPBP) DEVICES, ONE (1) RPBP SERVING WATER SYSTEM FOR RESTROOM, ONE (1) RPBP SERVING IRRIGATION SYSTEM AND YARD HYDRANTS SYSTEM, REFERENCE STATIONS STANDARD DETAILS PACKAGE.
 - REFERENCE STATION STANDARD DETAILS PACKAGE FOR SHEETS UB-1310 AND UB-1311.
 - NORTHGLENN TO OWN AND MAINTAIN 8" WATER MAIN SHOWN IN PUBLIC EASEMENT ACROSS SITE. SANITARY SEWER SERVICE AND WATER SERVICE TO BE OWNED AND MAINTAINED BY RTD, FIRE PROTECTION BY NORTH METRO FIRE DISTRICT.


DESIGNED BY: BEP	DATE: 11/16/2015	CHECKED BY: CRH	DATE: 11/30/2015	HORIZ. SCALE: 1"=100'
DRAWN BY: VRE	DATE: 11/16/2015	APPROVED BY: BEP	DATE: 12/01/2016	VERT. SCALE: N/A



REGIONAL RAIL PARTNERS



2000 SOUTH COLORADO BOULEVARD
SUITE 2-300
DENVER, COLORADO 80222
(303) 758-4055



North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BLANK STREET
DENVER, COLORADO 80202
(303) 628-0000

NORTH METRO RAIL LINE

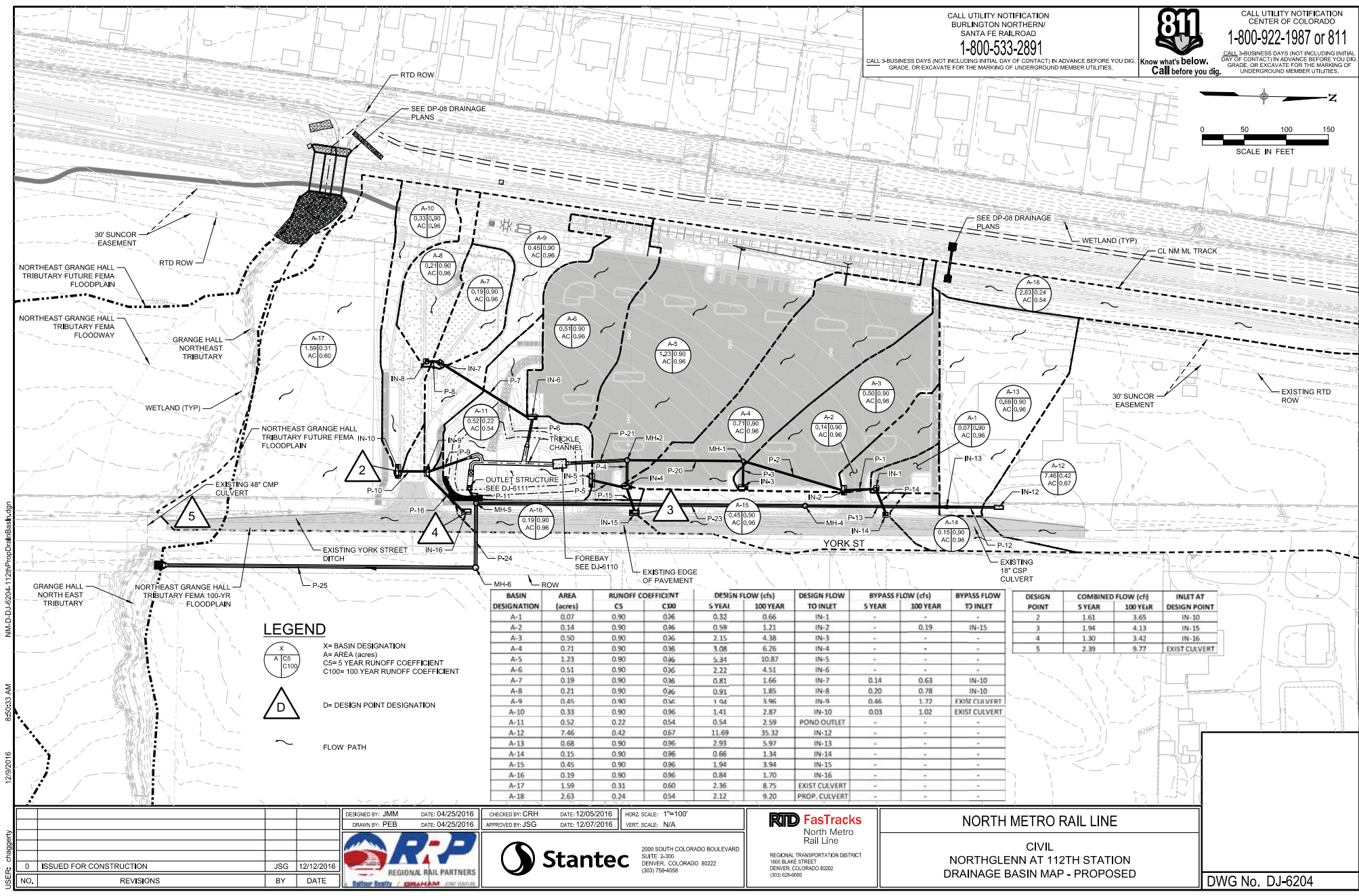
CIVIL

NORTHGLENN AT 112TH STATION

OVERALL UTILITY PLAN

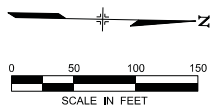
DWG No. UB-6100

USER: JohnBarone
11/15/2016 12:29:20 PM
NML\UB-6100-112th\Overall.dgn



CALL UTILITY NOTIFICATION
BURLINGTON NORTHERN/
SANTA FE RAILROAD
1-800-533-2891
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG,
GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

811
CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG,
GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
Know what's below. Call before you dig.



NM-DJ-6204-1121PropDrainBasin.dgn
850333 AM
12/29/2016
USER: chappery

LEGEND
X = BASIN DESIGNATION
A = AREA (acres)
C5 = 5 YEAR RUNOFF COEFFICIENT
C100 = 100 YEAR RUNOFF COEFFICIENT
D = DESIGN POINT DESIGNATION
FLOW PATH

BASIN DESIGNATION	AREA (acres)	RUNOFF COEFFICIENT C5	RUNOFF COEFFICIENT C100	DESIGN FLOW (cfs) 5 YEAR	DESIGN FLOW (cfs) 100 YEAR	DESIGN FLOW TO INLET	BYPASS FLOW (cfs) 5 YEAR	BYPASS FLOW (cfs) 100 YEAR	BYPASS FLOW TO INLET
A-1	0.07	0.90	0.96	0.32	0.66	IN-1	-	-	-
A-2	0.14	0.90	0.96	0.59	1.21	IN-2	-	0.19	IN-15
A-3	0.50	0.90	0.96	2.15	4.38	IN-3	-	-	-
A-4	0.71	0.90	0.96	3.08	6.26	IN-4	-	-	-
A-5	1.23	0.90	0.96	5.34	10.87	IN-5	-	-	-
A-6	0.51	0.90	0.96	2.22	4.51	IN-6	-	-	-
A-7	0.19	0.90	0.96	0.81	1.66	IN-7	0.14	0.63	IN-10
A-8	0.21	0.90	0.96	0.91	1.85	IN-8	0.20	0.78	IN-10
A-9	0.45	0.90	0.96	1.04	3.96	IN-9	0.46	1.72	EXIST CULVERT
A-10	0.33	0.90	0.96	1.41	2.87	IN-10	0.03	1.02	EXIST CULVERT
A-11	0.52	0.22	0.54	0.54	2.59	POND OUTLET	-	-	-
A-12	7.46	0.42	0.67	11.69	35.32	IN-12	-	-	-
A-13	0.68	0.90	0.96	2.93	5.97	IN-13	-	-	-
A-14	0.15	0.90	0.96	0.66	1.34	IN-14	-	-	-
A-15	0.45	0.90	0.96	1.94	3.94	IN-15	-	-	-
A-16	0.19	0.90	0.96	0.84	1.70	IN-16	-	-	-
A-17	1.59	0.31	0.50	2.36	8.75	EXIST CULVERT	-	-	-
A-18	2.63	0.24	0.54	2.12	9.20	PROP. CULVERT	-	-	-

DESIGN POINT	COMBINED FLOW (cfs) 5 YEAR	COMBINED FLOW (cfs) 100 YEAR	INLET AT DESIGN POINT
2	1.61	3.65	IN-10
3	1.94	4.13	IN-15
4	1.30	3.42	IN-16
5	2.39	9.77	EXIST CULVERT

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	JSG	12/12/2016

DESIGNED BY: JMM DATE: 04/25/2016
DRAWN BY: PEB DATE: 04/25/2016
CHECKED BY: CRH DATE: 12/05/2016
APPROVED BY: JSG DATE: 12/07/2016
HORIZ. SCALE: 1"=100'
VERT. SCALE: N/A

R-P
REGIONAL RAIL PARTNERS
Burlington Northern Santa Fe / CHS

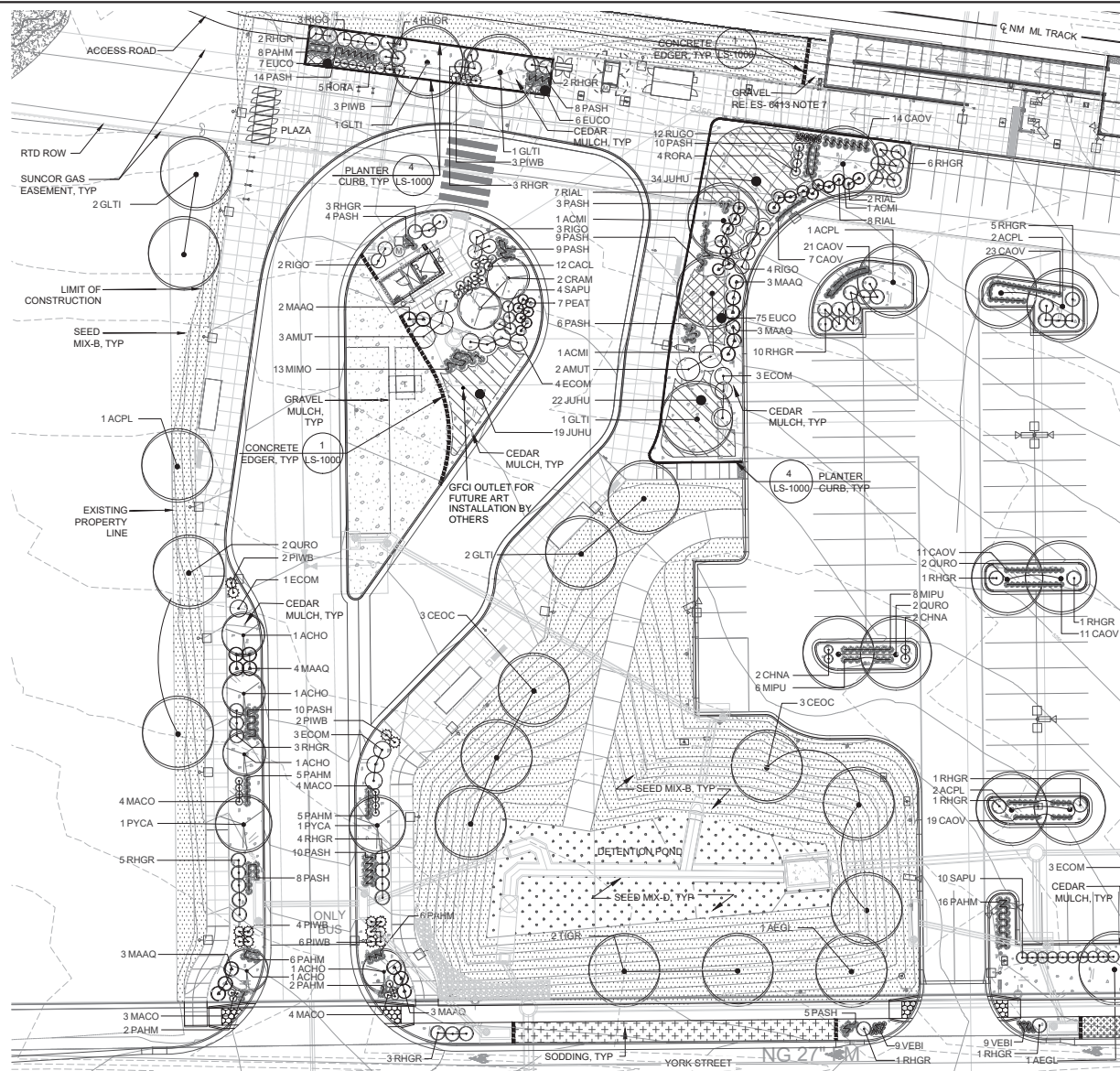
Stantec
2000 SOUTH COLORADO BOULEVARD
SUITE 2-300
DENVER, COLORADO 80222
(303) 758-4055

RTD FastTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLAINE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE
CIVIL
NORTHGLENN AT 112TH STATION
DRAINAGE BASIN MAP - PROPOSED

DWG No. DJ-6204

USER: StaceyS 12/12/2016 6:18 PM NML-LS-6101.dwg



MATCHLINE
SEE DWG NO. LS-6102

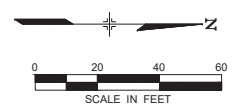


CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG.
Know what's Below. Call before you dig.

GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

LEGEND

- DECIDUOUS TREE
- EVERGREEN TREE
- ORNAMENTAL TREE
- DECIDUOUS SHRUB
- EVERGREEN SHRUB
- ORNAMENTAL GRASS
- PERENNIALS
- GROUNDCOVER
- CONCRETE EDGER
- PLANTER CURB
- SODDING
- SEED MIX - B
- SEED MIX - D
- GRAVEL MULCH
- CEDAR MULCH




PRELIMINARY
NOT FOR
CONSTRUCTION

DWG No. LS-6101

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	SLB	12/12/2016

DESIGNED BY: SES
DRAWN BY: BMK

DATE: 09/01/2014
DATE: 09/16/2014



REGIONAL RAIL PARTNERS

CHECKED BY: SES
APPROVED BY: SLB

DATE: 12/12/2016
DATE: 12/12/2016



VALERIAN
Landscape Architecture | Urban Design | Planning



RID FastTracks
North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE

LANDSCAPE
NORTHGLENN AT 112TH STATION
PLANTING PLAN - SOUTH

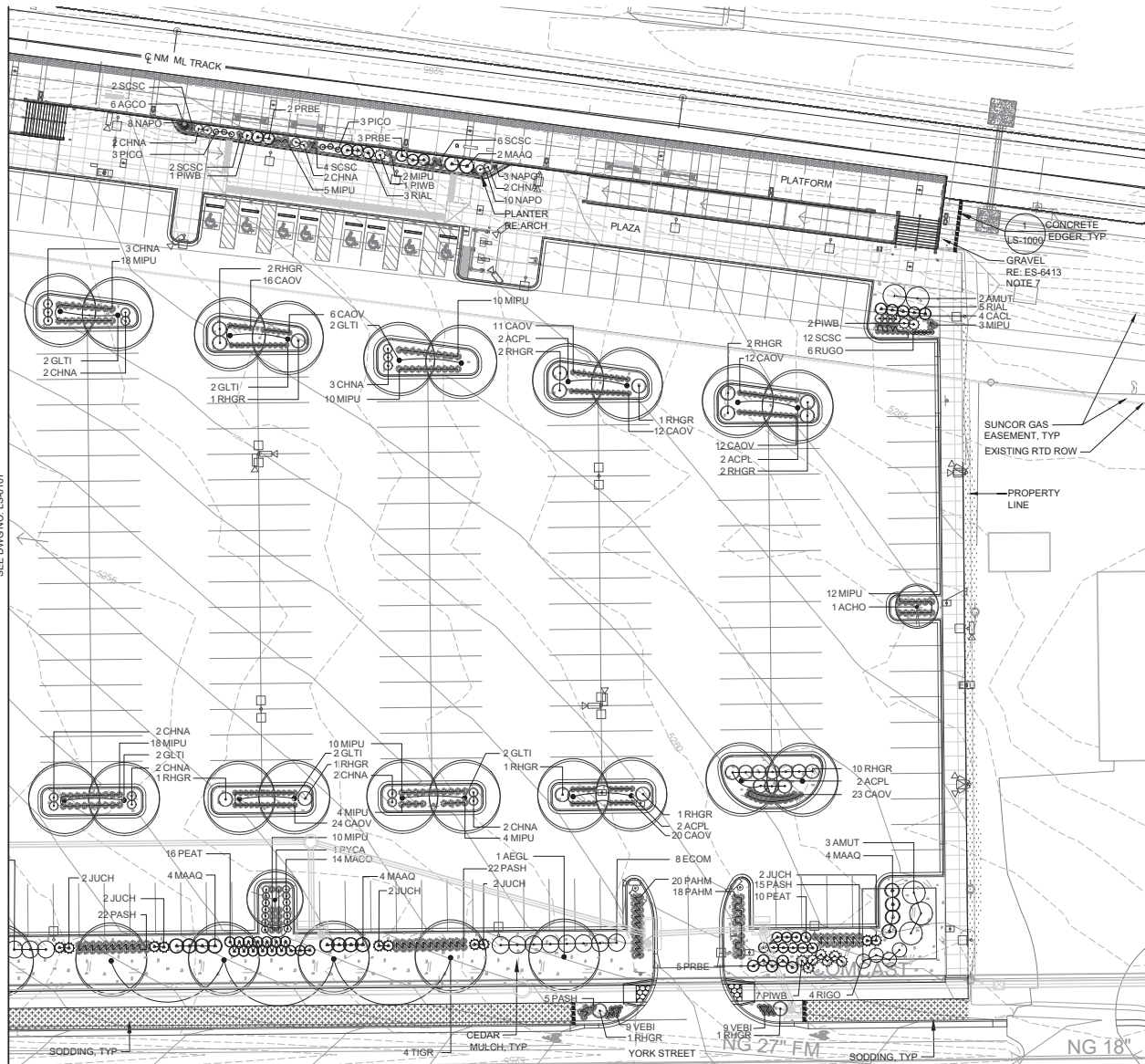
NM-LS-6102.dwg

6:19 PM

12/12/2016

USER: StaceyS

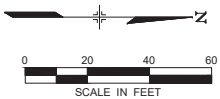
MATCHLINE
SEE DWG NO. LS-6101



811
CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL
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LEGEND

- DECIDUOUS TREE
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- EVERGREEN SHRUB
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- PERENNIALS
- GROUNDCOVER
- CONCRETE EDGER
- PLANTER CURB
- SODDING
- SEED MIX - B
- SEED MIX - D
- GRAVEL MULCH
- CEDAR MULCH



PRELIMINARY
NOT FOR
CONSTRUCTION

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	SLB	12/12/2016

DESIGNED BY: SES DATE: 09/01/2014
DRAWN BY: BMK DATE: 09/16/2014
CHECKED BY: SES DATE: 12/12/2016
APPROVED BY: SLB DATE: 12/12/2016
HORIZ SCALE: 1"=40'
VERT SCALE: NA

R&P
REGIONAL RAIL PARTNERS
RAILROAD PARTNERS / GRAHAM

VALERIAN
Landscape Architecture / Urban Design
Innovation Design / Planning

RID FastTracks
North Metro
Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE
LANDSCAPE
NORTHGLENN AT 112TH STATION
PLANTING PLAN - NORTH

DWG No. LS-6102

NML-LS-6200.dwg

6:19 PM

12/1/2016

USER: StaceyS

PLANT SCHEDULE

DECIDUOUS TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
ACMI	3	ACER MIYABEI 'STATE STREET'	STATE STREET MAPLE	B & B	2" CAL
ACPL	14	ACER PLATANOIDES 'EMERALD QUEEN'	EMERALD QUEEN MAPLE	B & B	2" CAL
AESL	3	AESCULUS GLABRA	OHIO BUCKEYE	B & B	2" CAL
CEOC	6	CELTIS OCCIDENTALIS	COMMON HACKBERRY	B & B	2" CAL
GLTI	19	GLEDITSIA TRIACANTHOS INERMIS 'SKYLINE'	SKYLINE COMMON HONEYLOCUST	B & B	2" CAL
QURO	6	QUERCUS ROBUR	ENGLISH OAK	B & B	2" CAL
TIGR	6	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LITTLELEAF LINDEN	B & B	2" CAL
EVERGREEN TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
PICO	6	PICEA GLAUCA 'CONICA'	DWARF ALBERTA SPRUCE	15 GAL	
PSST	1	PINUS STROBIFORMIS	SOUTHWESTERN WHITE PINE	B & B	6' HT
ORNAMENTAL TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
ACHO	6	ACER TATARICUM 'HOT WINGS'	HOT WINGS TATARIAN MAPLE	B & B	6' CLUMP
CRAM	2	CRATAEGUS AMBIGUA	RUSSIAN HAWTHORN	B & B	6' CLUMP
PYCA	3	PYRUS CALLERYANA 'ARISTOCRAT'	ARISTOCRAT FLOWERING PEAR	B & B	2" CAL
DECIDUOUS SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	CONT	
AMUT	10	AMELANCHIER UTAHENSIS	UTAH SERVICEBERRY	5 GAL	
CACL	16	CARYOPTERIS X CLANDONENSIS 'BLUE MIST'	BLUE MIST SHRUB	5 GAL	
CHNA	26	CHRYSOETHAMNUS NAUSEOSUS NAUSEOSUS	DWARF BLUE RABBITBRUSH	5 GAL	
ECOM	22	EUONYMUS ALATUS 'COMPACTUS'	COMPACT BURNING BUSH	5 GAL	
MAAQ	32	MAHONIA AQUIFOLIUM	OREGON GRAPE	5 GAL	
MACO	29	MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE	5 GAL	
PEAT	33	PEROVSKIA ATRIPLICIFOLIA	RUSSIAN SAGE	5 GAL	
PRBE	10	PRUNUS BESSEYI 'PAWNEE BUTTES'	SAND CHERRY	5 GAL	
RHOR	82	RHUS AROMATICA 'GRO-LOW'	GRO-LOW FRAGRANT SUMAC	5 GAL	
RIAL	25	RIBES ALPINUM	ALPINE CURRANT	5 GAL	
RIGO	16	RIBES AUREUM	GOLDEN CURRANT	5 GAL	
RORA	9	ROSA X 'RADCON'	RADCON KNOCKOUT ROSE	5 GAL	
SAFU	14	SALIX PURPUREA 'NANA'	DWARF ARCTIC WILLOW	5 GAL	
EVERGREEN SHRUB	QTY	BOTANICAL NAME	COMMON NAME	CONT	
JUCH	10	JUNIPERUS CHINENSIS 'ARMSTRONGII'	ARMSTRONG JUNIPER	5 GAL	
JUCO	1	JUNIPERUS SCOPULORUM 'COLOGREEN'	COLOGREEN JUNIPER	15 GAL	
PIMU	1	PINUS MUGO 'MOPS'	MOPS MUGO PINE	5 GAL	
PWIB	31	PINUS MUGO 'WHITEBUD'	MUGO PINE	5 GAL	
ORNAMENTAL GRASSES	QTY	BOTANICAL NAME	COMMON NAME	CONT	
CAOV	242	CALAMAGROSTIS X ACUTIFLORA 'OVERDAM'	OVERDAM FEATHER REED GRASS	1 GAL	
MIMO	13	MISCANTHUS SINENSIS 'MORNING LIGHT'	EULALIA GRASS	1 GAL	
MIPU	120	MISCANTHUS SINENSIS 'PURPURESCENS'	FLAME GRASS	1 GAL	
NAPO	21	NASSELLA TENUISSIMA 'PONY TAILS'	MEXICAN FEATHERGRASS	1 GAL	
PAHM	88	PANICUM VIRGATUM 'HEAVY METAL'	BLUE SWITCH GRASS	1 GAL	
PASH	160	PANICUM VIRGATUM 'SHENENDOAH'	BURGUNDY SWITCH GRASS	1 GAL	
SCSC	26	SCHIZACHYRIUM SCOPARIUM 'THE BLUES'	THE BLUES LITTLE BLUESTEM	1 GAL	
PERENNIALS	QTY	BOTANICAL NAME	COMMON NAME	CONT	
AGCO	6	AGASTACHE X 'CORANADO RED'	ANISE HYSSOP	1 GAL	
RUGO	18	RUDEBECKIA FULGIDA 'GOLDSTURM'	GOLDSTURM BLACK EYED SUSAN	1 GAL	
VEBI	36	VERBENA BIPINNATIFIDA	NATIVE VERBENA	1 GAL	

GROUNDCOVERS	QTY	BOTANICAL NAME	COMMON NAME	CONT @
 - EUCO	88	EUONYMUS COLORATUS	PURPLE WINTERCREEPER	1 GAL
 - JUHU	75	JUNIPERUS HORIZONTALIS 'HUGHES'	HUGHES JUNIPER	5 GAL
SODDING/SEEDING	QTY	BOTANICAL NAME	COMMON NAME	CONT @
 22,989 SF	SEED MIX - B		NATIVE SEED MIX	SEED
 3,013 SF	SEED MIX - D		WET/DRY SEED MIX	SEED
 2,027 SF	SODDING		TEXAS HYBRID BLUEGRASS SOD	SOD

LANDSCAPE NOTES

- SOIL:**
- SOIL PREPARATION FOR LANDSCAPE AREAS TO RECEIVE SOD, TREES, SHRUBS AND ALL PLANTING BEDS SHALL INCLUDE COMPOST ADDED AT A RATE OF FOUR (4) CUBIC YARDS PER ONE THOUSAND SQUARE FEET AND TILLED SIX INCHES INTO THE SOIL.
 - SOIL PREPARATION FOR LANDSCAPE AREAS TO RECEIVE NATIVE SEED SHALL BE AMENDED WITH 1,000 LBS /ACRE BIOSOL. ALL LANDSCAPE AREAS SHALL RECEIVE 4" TOPSOIL AND SOIL PREPARATION AS SPECIFIED.
- MULCH:**
- ALL PLANTING BEDS SHALL HAVE A MINIMUM 3" DEPTH OF CEDAR MULCH.
 - ALL PARKING LOT ISLAND PLANTING BEDS SHALL HAVE A MINIMUM 3" DEPTH OF CEDAR MULCH.
 - TREES AND SHRUBS IN SOD OR NATIVE AREAS - 3" DEPTH CEDAR MULCH AS SPECIFIED ON PLANTING DETAILS.
 - ALL GRAVEL AREAS SHALL HAVE A MINIMUM 3" DEPTH OF 3/4" BUTTER ROCK GRAVEL MULCH WITH GEOTEXTILE FABRIC AS WEED BARRIER.
- IRRIGATION:**
- ALL LANDSCAPE AREAS SHALL BE IRRIGATED BY A PERMANENT, AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
 - TURF AREAS SHALL BE ZONED SEPARATELY FROM BED AREAS.
 - THE CONTROLLER SHALL INCLUDE A RAIN SHUT-OFF.
 - THE SYSTEM SHALL BE PROPERLY ZONED TO SEPARATE PLANT MATERIAL BY WATER REQUIREMENT.
 - IRRIGATION CONSTRUCTION DRAWINGS SHALL BE SUBMITTED PRIOR TO APPROVAL OF THE CIVIL CONSTRUCTION DRAWINGS.
- MAINTENANCE:**
- PROPERTY OWNER SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPING.
 - PROPERTY OWNER SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL ARTERIAL AND COLLECTOR RIGHT-OF-WAY LANDSCAPING.
- PRE-CONSTRUCTION MEETING:**
- A PRE-CONSTRUCTION MEETING SHALL BE REQUIRED BETWEEN THE LANDSCAPE CONTRACTOR, PROPERTY OWNER AND CITY OF NORTHGLENN PERSONNEL BEFORE START OF CONSTRUCTION.
- SOD**
- SOD SHALL BE TEXAS HYBRID BLUEGRASS VARIETY THAT IS LOCALLY GROWN AND SHALL CONFORM TO NORTHGLENN REQUIREMENTS.
- SEED**
- ALL NATIVE GRASS SEED AREAS SHALL BE SEEDED WITH SEED MIXES AND RATES PER THE CITY OF NORTHGLENN STANDARDS AND SPECIFICATIONS.
- PLANTING NOTES:**
- PLANT DECIDUOUS TREES MINIMUM 4" AND SHRUBS 2" OR 1/2" MATURE WIDTH FROM SIDEWALKS, BED EDGES, FENCES AND BUILDINGS.
 - ALL DECIDUOUS TREES SHALL BE BALLED AND BURLAPPED WITH FULL HEADS, STRAIGHT TRUNKS, AND SINGLE LEADERS UNLESS OTHERWISE NOTED.
 - CONTRACTOR TO SELECT TREES THAT HAVE THEIR FIRST BRANCH 6'-0" ABOVE THE TOP OF THE ROOTBALL. IF NEEDED, THE CONTRACTOR SHALL REQUEST THE SUPPLIER TO PRUNE ACCORDINGLY.
 - NO TREE OR SHRUB MORE THAN 30 INCHES HIGHER THAN TOP OF CURB SHALL BE PLANTED NEITHER IN ANY 50 FOOT VISION TRIANGLE AT ANY STREET INTERSECTION NOR IN ANY 25 FOOT VISION TRIANGLE.
 - NO TREE SHALL BE PLANTED WITHIN 10 FEET OF ANY UNDERGROUND UTILITY.
- DETENTION POND NOTES:**
- THE DETENTION POND SHALL BE ON A PERMANENT IRRIGATION SYSTEM ZONED SEPARATE FROM THE SURROUNDING AREA.
 - THE DETENTION POND SHALL BE SEEDED WITH A NATIVE SEED MIX.

SEED MIX SCHEDULE

SEED MIX - B - NATIVE SEED MIX	
40% - BUFFALOGRASS (BUCHLOE DACTYLOIDES)	25.0 LBS/ACRE
40% - BLUE GRAMA (BOUTELLOUA GRACILIS)	5.0 LBS/ACRE
20% - SIDEOTS GRAMA (BOUTELLOUA CURTIPENDULA)	10.0 LBS/ACRE
	40.0 LBS/ACRE
SEED MIX - B - WILDFLOWERS	
20% PURPLE PRAIRIE CLOVER (PETALOSTEMON PURPUREA)	0.6 LBS/ACRE
20% AMERICAN VETCH (VIGIA AMERICANA)	2.3 LBS/ACRE
15% PRAIRIE CONEFLOWER (RATIBIDA COLUMNIFERA)	0.15 LBS/ACRE
10% PERENNIAL GALLARDIA (GALLARDIA ARISTATA)	0.55 LBS/ACRE
10% GOLDEN CROWNBEARD (VERBESINE ENCELLIOIDES)	0.65 LBS/ACRE
10% ROCKY MOUNTAIN BEEPLANT (CLEOME SERRULATA)	1.4 LBS/ACRE
5% FRINGED SAGE (ARTEMESIA FRIGIDA)	0.01 LBS/ACRE
5% NARROWLEAF PENSTEMON (PENSTEMON ANGUSTIFOLIUS)	0.15 LBS/ACRE
5% WESTERN YARROW (ACHILLEA LANULOSA)	0.01 LBS/ACRE
	5.83 LBS/ACRE
SEED MIX - D - WET/DRY SEED MIX	
40% - ALKALIGRASS (PUCCINELLIA DISTANS)	2.00 LBS/ACRE
30% - WESTERN WHEATGRASS (PASCOPYRUM SMITHII)	22.0 LBS/ACRE
30% - SMOOTH BROME (BROMUS INERMIS)	16.0 LBS/ACRE
	40.0 LBS/ACRE

LANDSCAPE TABLE

DESCRIPTION	AREA	PERCENTAGE
SITE AREA (INCLUDING TRACKWAY)	276,276 SF	—
AREA OF ON-SITE LIVING MATERIAL	48,672 SF	17.7%
AREA OF ON-SITE NON-LIVING MATERIAL	45,732 SF	16.7%
AREA OF PAVING AND STRUCTURES	180,108 SF	65.6%
AREA OF LIVING MATERIAL IN ROW	3,067 SF	
AREA OF NON-LIVING MATERIAL IN ROW	6,524 SF	
TOTAL SF OF SPRAY IRRIGATED AREA	26,210 SF	
TOTAL SF OF DRIP IRRIGATED AREA	22,462 SF	

NO.	REVISIONS	BY	DATE	DESIGNED BY: SES	DATE: 09/01/2014	CHECKED BY: SES	DATE: 12/12/2016	HORIZ. SCALE: NA
				DRAWN BY: BMK	DATE: 09/16/2014	APPROVED BY: SLB	DATE: 12/12/2016	VERT. SCALE: NA
								
0	ISSUED FOR CONSTRUCTION	SLB	12/12/2016					

RID FastTracks
North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE

LANDSCAPE
NORTHGLENN AT 112TH STATION
PLANTING NOTES

PRELIMINARY
NOT FOR
CONSTRUCTION

DWG No. LS-6200



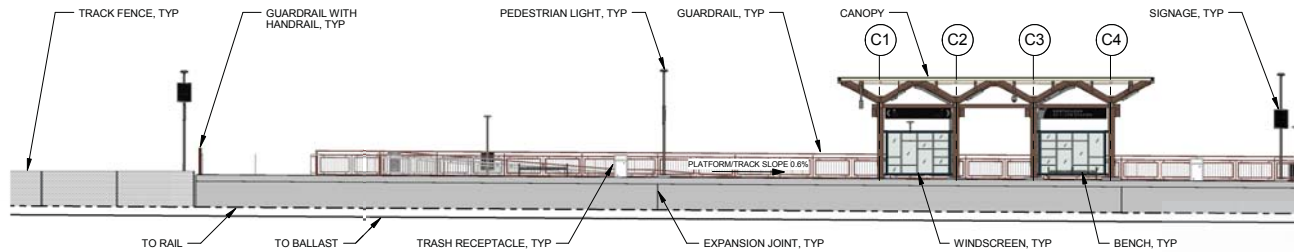
CALL UTILITY NOTIFICATION
CENTER OF COLORADO

1-800-922-1987 or 811

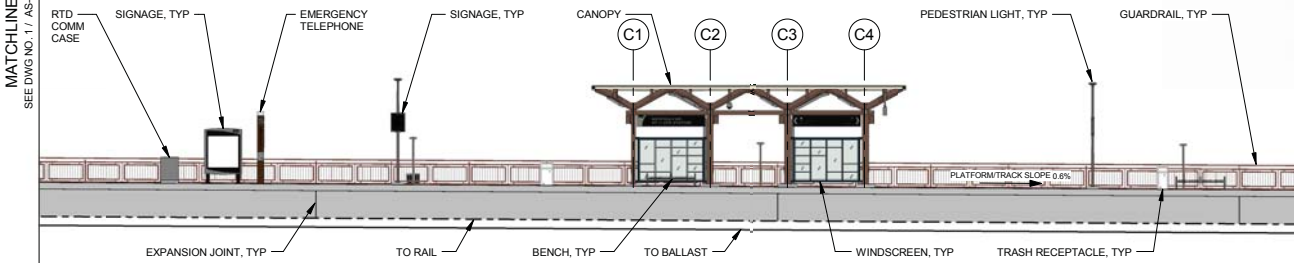
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL
DAY OF CONTACT) IN ADVANCE BEFORE YOU
DIG.

GRADE, OR EXCAVATE FOR THE MARKING OF
UNDERGROUND MEMBER UTILITIES.

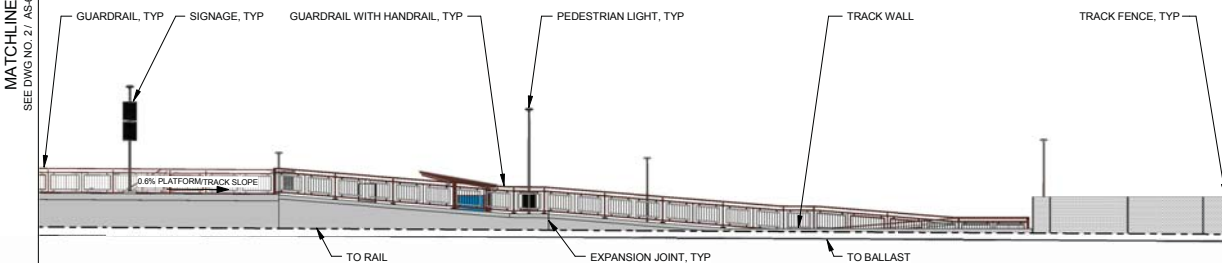
Know what's Below.
Call before you dig.



1 PLATFORM ELEVATION - NORTH



2 PLATFORM ELEVATION - SOUTH



3 PLATFORM ELEVATION - SOUTH RAMP

MATCHLINE
SEE DWG NO. 2 / AS-6200

MATCHLINE
SEE DWG NO. 3 / AS-6200

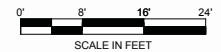
NOTES:

1. FOR STATION SPECIFIC STRUCTURAL DETAILS, SEE DRAWING SERIES SS-6000
2. FOR STATION SPECIFIC MECHANICAL DETAILS, SEE DRAWING SERIES MS-6000
3. FOR STATION SPECIFIC PLUMBING DETAILS, SEE DRAWING SERIES PS-6000
4. FOR STATION SPECIFIC ELECTRICAL DETAILS, SEE DRAWING SERIES ES-6000
5. FOR STATION SPECIFIC CIVIL DETAILS, SEE DRAWING SERIES CS-6000
6. FOR STATION SPECIFIC GRADING DETAILS, SEE DRAWING SERIES DJ-6000
7. FOR STATION SPECIFIC UTILITY DETAILS, SEE DRAWING SERIES UB-6000
8. FOR STATION SPECIFIC LANDSCAPE DETAILS, SEE DRAWING SERIES LS-6000
9. FOR STATIONS STANDARD ARCHITECTURAL DETAILS SEE DRAWING SERIES AS-1000
10. FOR STATIONS STANDARD ARCHITECTURAL STRUCTURAL DETAILS SEE DRAWING SERIES SS-1000

AMENITY	MATERIAL	FINISH COLOR	REMARKS
TICKET VENDING ROOF	GLASS	OLD CASTLE CLEAR WITH ARCTIC SNOW LAMINATE	LAMINATED, TEMPERED
ALL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM B
CANOPY	METAL	TNEMEC - SEE STATION SPECIFIC PLANS FOR COLOR	COATING SYSTEM B, ALL EXPOSED METAL TO BE COATED, UON
ALL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - SEE STATION SPECIFIC PLANS FOR COLOR	COATING SYSTEM B, ALL EXPOSED METAL TO BE COATED, UON
VARIABLE MESSAGE SIGN MOUNTING	METAL	BLACK, RAL 9005	COATING SYSTEM B
LIGHT POLE & FIXTURE	METAL	BLACK, RAL 9005	POWDER COATED
BENCH	METAL	BLACK, RAL 9005	POWDER COATED
PAVING: PLATFORM, RAMPS, STAIRS, LANDINGS, PLAZA, SIDEWALKS	CONCRETE	GRAY	MEDIUM BROOM FINISH
TRASH RECEPTACLE	METAL	BLACK, RAL 9005	PREFINISHED
DETECTABLE WARNING STRIP	-	FEDERAL YELLOW #5353	PER MANUFACTURER
BIKE RACK	METAL	GREEN, RAL 6009	GALVANIZED, PANGARD, POLYESTER POWDER COAT
BIKE LOCKER	FIBERGLASS	TAN	PER MANUFACTURER
RAILINGS: DECORATIVE RAILS & GUARDRAILS	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM C
HANDRAILS	METAL	-	STAINLESS STEEL
HANDRAIL BRACKETS	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM C
BASE PLATE	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM C
PEDESTRIAN BUS SHELTER	METAL	BLACK, RAL 9005	PER MANUFACTURER
CAST IN PLACE CONCRETE RETAINING WALLS AT 12ND, 112TH AND 104TH STATIONS	PAINTED CONCRETE	BROWN, FS 30450	FRACTURED FIN FORMLINER, 1/4" MAX DEPTH, 1-1/2" OC MAX FIN SPACING, EXTERIOR PAINT
OCS POLES AT STATION PLATFORM	STEEL	BLACK, RAL 9005	PER MANUFACTURER
HYD YARDWANT CAGE	STEEL	STAINLESS STEEL	PER MANUFACTURER

CANOPY FINISHES LEGEND

ELEMENT	FINISH	COATING SYSTEM
COLUMN	TNEMEC IRONWOOD 28BR	B
SPINE BEAM	TNEMEC IRONWOOD 28BR	B
OUTRIGGER	TNEMEC IRONWOOD 28BR	B
GLASS PANEL FRAME	TNEMEC BALSAMWOOD 134GN	B
GUTTER	TNEMEC BALSAMWOOD 134GN	B
DOWNSPOUT	TNEMEC IRONWOOD 28BR	B
WINDSCREEN	TNEMEC BALSAMWOOD 134GN	B
GLASS LITES	OLDCASTLE SOLIDEX WITH ARCTIC SNOW LAMINATE	-
SEALANT	CLEAR	B
ACCESS PANEL COVER	MATCH ADJACENT STEEL FINISH	B
JUNCTION BOX	TNEMEC IRONWOOD 28BR	B
STEEL PLATE	TNEMEC IRONWOOD 28BR	B



NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	VAM	09/21/2016

DESIGNED BY: MBB	DATE: 07/14/14	CHECKED BY: TLB	DATE: 09/26/2016
DRAWN BY: T.J.G.	DATE: 07/14/14	APPROVED BY: VAM	DATE: 09/26/2016



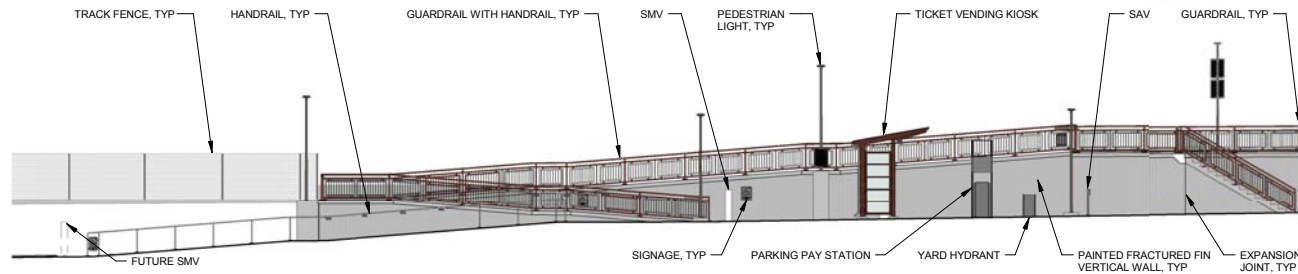
475 17th Street Suite 720
Denver, Colorado 80202
iron-horse-architects.com



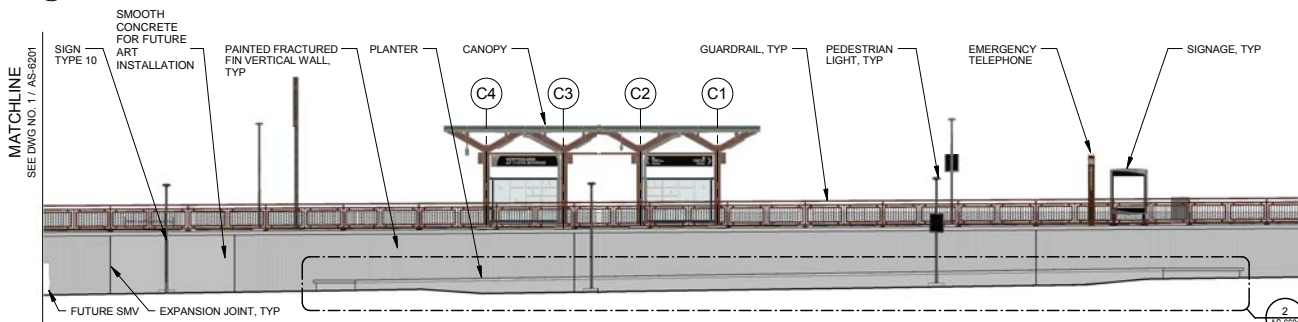
NORTH METRO RAIL LINE

ARCHITECTURAL
NORTHGLENN AT 112TH STATION
STATION ELEVATION - EAST PLATFORM

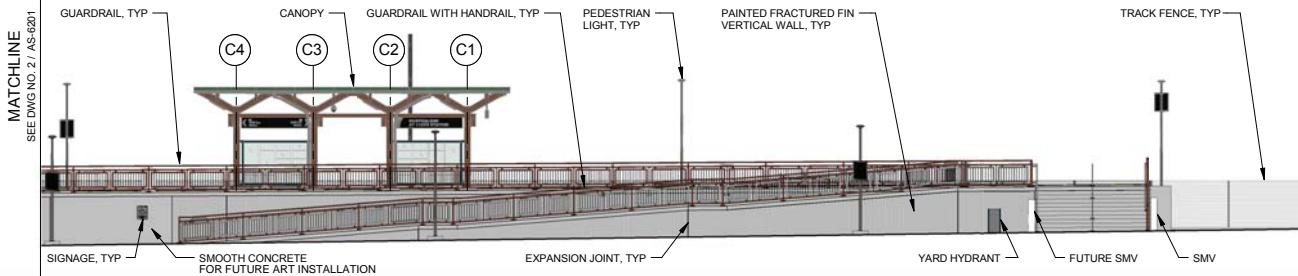
DWG No. AS-6200



1 STATION ELEVATION - SOUTH RAMP



2 STATION ELEVATION - SOUTH



3 STATION ELEVATION - NORTH

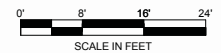
NOTES:

1. FOR STATION SPECIFIC STRUCTURAL DETAILS, SEE DRAWING SERIES SS-6000
2. FOR STATION SPECIFIC MECHANICAL DETAILS, SEE DRAWING SERIES MS-6000
3. FOR STATION SPECIFIC PLUMBING DETAILS, SEE DRAWING SERIES PS-6000
4. FOR STATION SPECIFIC ELECTRICAL DETAILS, SEE DRAWING SERIES ES-6000
5. FOR STATION SPECIFIC CIVIL DETAILS, SEE DRAWING SERIES CS-6000
6. FOR STATION SPECIFIC GRADING DETAILS, SEE DRAWING SERIES GS-6000
7. FOR STATION SPECIFIC UTILITY DETAILS, SEE DRAWING SERIES US-6000
8. FOR STATION SPECIFIC LANDSCAPE DETAILS, SEE DRAWING SERIES LS-6000
9. FOR STATIONS STANDARD ARCHITECTURAL DETAILS SEE DRAWING SERIES AS-1000
10. FOR STATION STANDARD ARCHITECTURAL STRUCTURAL DETAILS SEE DRAWING SERIES SS-1000

AMENITY	MATERIAL	FINISH COLOR	REMARKS
TICKET KIOSK - ROOF	GLASS	OLD CASTLE CLEAR WITH ARCTIC SNOW LAMINATE	LAMINATED, TEMPERED
- ALL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM B
CANOPY	ALL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - SEE CANOPY FINISHES LEGEND
VARIABLE MESSAGE SIGN MOUNTING	METAL	BLACK, RAL 9005	COATING SYSTEM B
LIGHT POLE & FIXTURE	METAL	BLACK, RAL 9005	POWDER COATED
BENCH	METAL	BLACK, RAL 9005	
PAVING - PLATFORM, RAMPS, STAIRS, LANDINGS, PLAZA, SIDEWALKS	CONCRETE	GRAY	MEDIUM BROOM FINISH
TRASH RECEPTACLE	METAL	BLACK, RAL 9005	PREFINISHED
DETECTABLE WARNING STRIP	-	FEDERAL YELLOW #5358	PER MANUFACTURER
BIKE RACK	METAL	GREEN, RAL 6009	GALVANIZED, PANGARD II POLYESTER POWDER COAT
BIKE LOCKER	FIBERGLASS	TAN	PER MANUFACTURER
RAILINGS - DECORATIVE RAILS & GUARDRAILS	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM C
- HANDRAILS	METAL	-	STAINLESS STEEL
- HANDRAIL BRACKETS	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM C
- BASE PLATE	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM C
PEDESTRIAN BUS SHELTER	METAL	BLACK, RAL 9005	PER MANUFACTURER
CAST IN PLACE CONCRETE RETAINING WALLS AT 12ND, 112TH AND 104TH STATIONS	PAINTED CONCRETE	BROWN, FS 30450	FRACTURED FIN FORMLINER, 3/4" MAX DEPTH, 1-1/2" OC MAX FIN SPACING, EXTERIOR PAINT
OCS POLES AT STATION PLATFORM	STEEL	BLACK, RAL 9005	
YARD HYDRANT CAGE	STEEL	STAINLESS STEEL	

CANOPY FINISHES LEGEND

ELEMENT	FINISH	COATING SYSTEM
COLUMN	TNEMEC IRONWOOD 28BR	B
SPIKE BEAM	TNEMEC IRONWOOD 28BR	B
OUTRIGGER	TNEMEC IRONWOOD 28BR	B
GLASS PANEL FRAME	TNEMEC BALSAMWOOD 13AGN	B
GUTTER	TNEMEC BALSAMWOOD 13AGN	B
DOWNSPOUT	TNEMEC IRONWOOD 28BR	B
WINDSCREEN	TNEMEC BALSAMWOOD 13AGN	B
GLASS LITES	OLDCASTLE SOLEXIA WITH ARCTIC SNOW LAMINATE	-
SEALANT	CLEAR	B
ACCESS PANEL COVER	MATCH ADJACENT STEEL FINISH	B
JUNCTION BOX	TNEMEC IRONWOOD 28BR	B
STEEL PLATE	TNEMEC IRONWOOD 28BR	B



NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	VAM	09/21/2016

DESIGNED BY: MBB	DATE: 01/05/15	CHECKED BY: TLB	DATE: 09/26/2016
DRAWN BY: T.J.G.	DATE: 01/05/15	APPROVED BY: VAM	DATE: 09/26/2016

475 17th Street Suite 720 Denver, Colorado 80202 iron-horse-architects.com	

HORZ. SCALE: 1/16" = 1'	VERT. SCALE: N/A
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REGIONAL TRANSPORTATION DISTRICT 1600 BLAKE STREET DENVER, COLORADO 80202 (303) 628-9000

NORTH METRO RAIL LINE
ARCHITECTURAL
NORTHGLENN AT 112TH STATION
STATION ELEVATION - EAST PLATFORM WALL

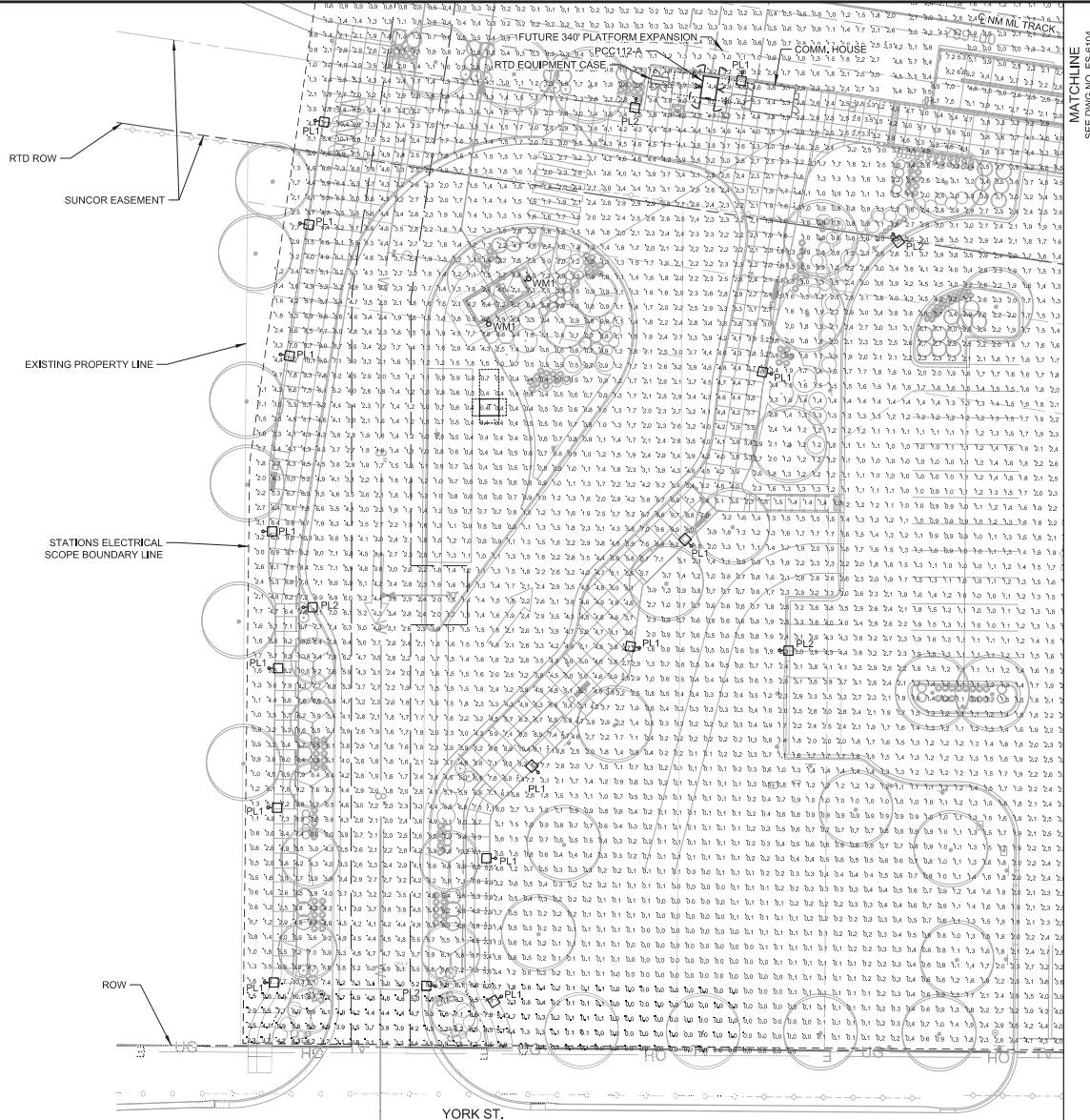
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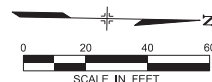
5/12/2016

USER: Adminor



MATCHLINE
SEE DWG NO. ES-604

GENERAL NOTES:
1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.



1 SITE PHOTOMETRIC PLAN - SOUTH

PRELIMINARY
NOT FOR
CONSTRUCTION

NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: S/JGP DATE: 05/05/2016
DRAWN BY: AR/TE DATE: 05/06/2016

R-P
REGIONAL RAIL PARTNERS

CHECKED BY: KAT DATE: 05/09/2016
APPROVED BY: KAT DATE: 05/20/2016

HORIZ. SCALE: 1" = 40'
VERT. SCALE: N/A

SSG MEP, Inc.
3025 S. Parker Road, Suite 1100
Aurora, Co 80014-2950
303.696.2602 fax 303.696.0812
www.ssggroupinc.com

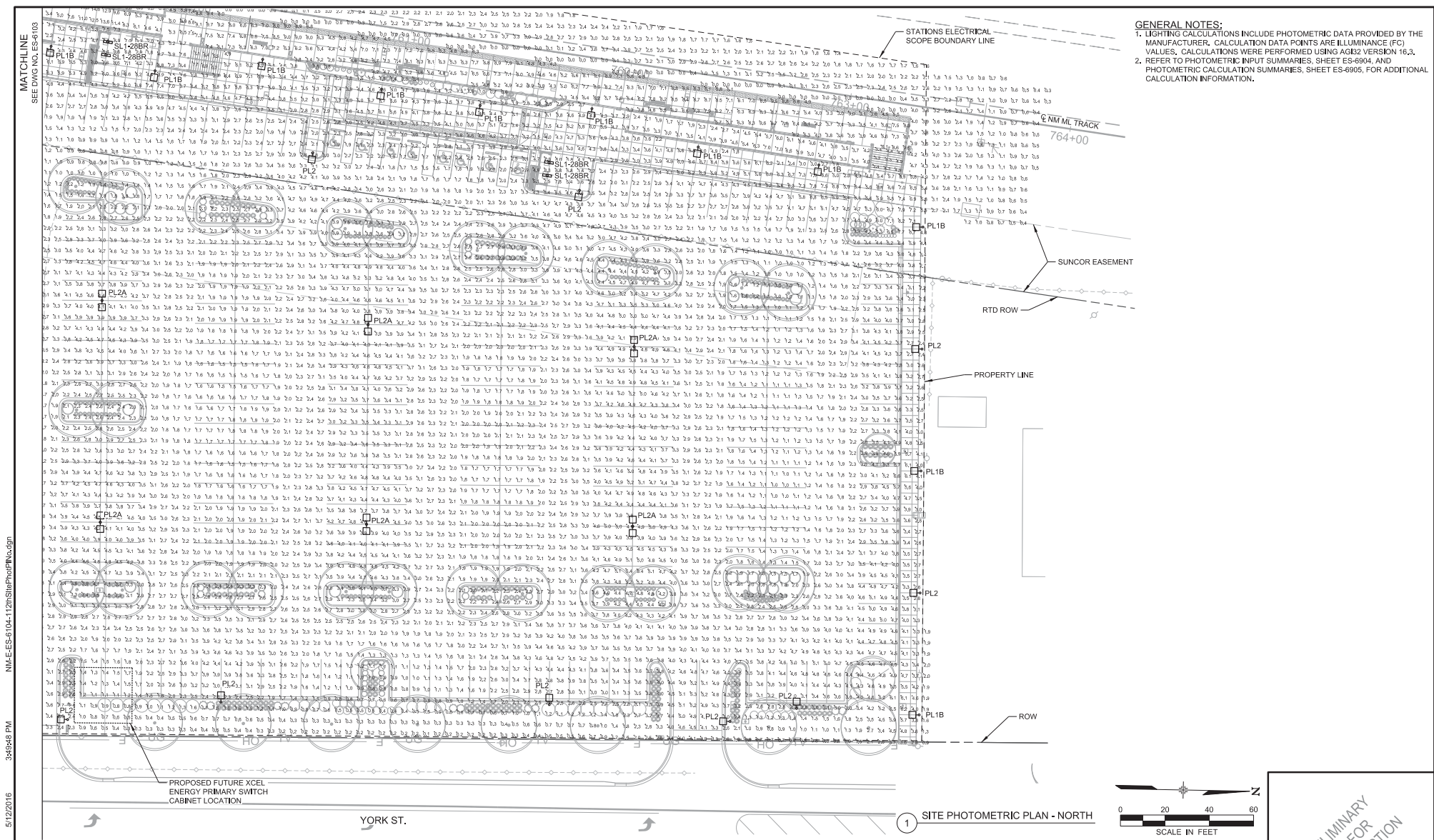
RTD FastTracks
North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BEARS STREET
DENVER, CO 80202 80202
(303) 628-0001

NORTH METRO RAIL LINE

ELECTRICAL
NORTHGLENN AT 112TH STATION
SITE PHOTOMETRIC PLAN - SOUTH

DWG No. ES-6103



GENERAL NOTES:
1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-690A, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-690S, FOR ADDITIONAL CALCULATION INFORMATION.

NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: SJ/GP DATE: 05/05/2016
DRAWN BY: AR/TE DATE: 05/06/2016

RTP
REGIONAL RAIL PARTNERS

CHECKED BY: KAT DATE: 05/09/2016
APPROVED BY: KAT DATE: 05/20/2016

SSG MEP
SSG MEP, Inc.
3025 S. Parker Road, Suite 1100
Aurora, CO 80014-2950
303.696.2602 fax 303.696.0812
www.ssggroupinc.com

RTD FastTracks
North Metro Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BEARS STREET
DENVER, CO 80202
(303) 628-0001

NORTH METRO RAIL LINE

ELECTRICAL
NORTHGLENN AT 112TH STATION
SITE PHOTOMETRIC PLAN - NORTH

PRELIMINARY
NOT FOR
CONSTRUCTION

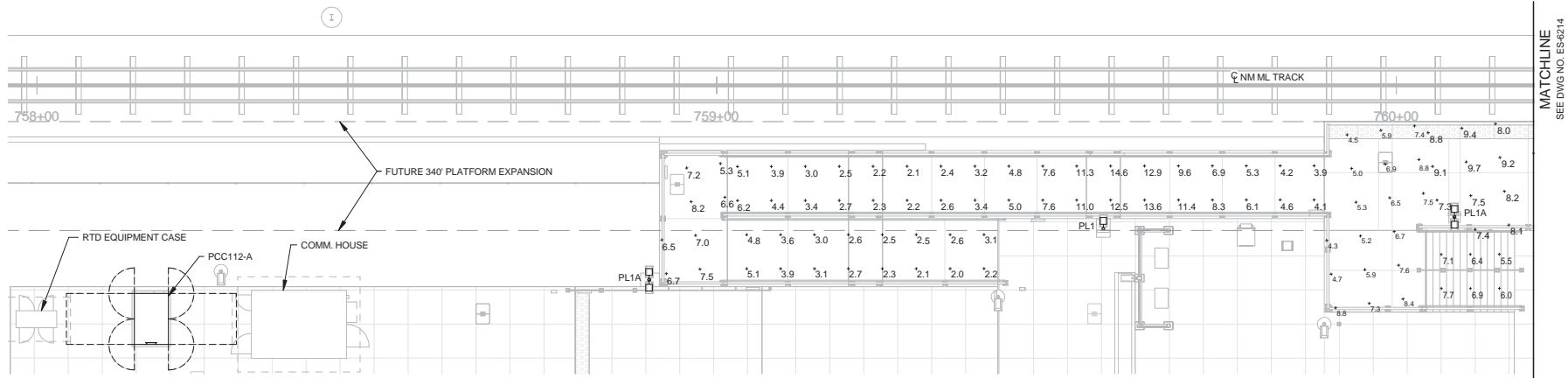
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USER: Adminor

3/4/2016 PM

NMLE-ES-014-112thStPhotPlan.dgn

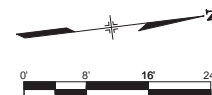
MATCHLINE
SEE DWG NO. ES-6103



1 PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH

GENERAL NOTES:

1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AGI32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.



NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: SRJ DATE: 05/05/2016
 DRAWN BY: AR/TE DATE: 05/06/2016

CHECKED BY: KAT DATE: 05/09/2016
 APPROVED BY: KAT DATE: 05/20/2016

HORIZ. SCALE: 1/16" = 1'
 VERT. SCALE: N/A



SSG MEP, Inc.
 3025 S. Parker Road, Suite 1100
 Aurora, Co 80014-2950
 303.696.2602 fax 303.696.0812
 www.ssggroupinc.com

RTD FasTracks
 North Metro
 Rail Line

REGIONAL TRANSPORTATION DISTRICT
 1600 BLAKE STREET
 DENVER, COLORADO 80202
 (303) 628-8000

NORTH METRO RAIL LINE

ELECTRICAL
NORTHGLENN AT 112TH STATION
PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH

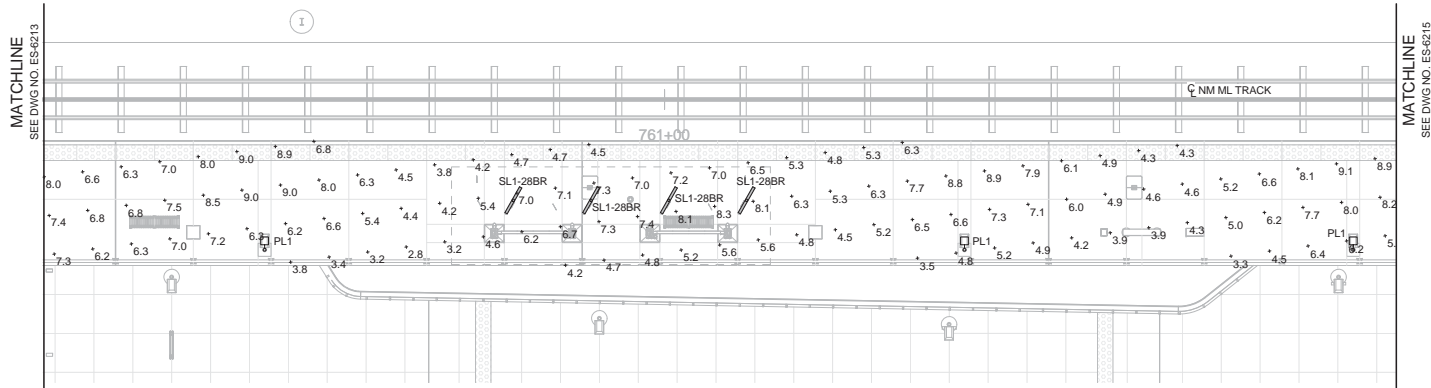
PRELIMINARY
 NOT FOR
 CONSTRUCTION

DWG No. ES-6213

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5/12/2016 4:49:03 PM

5/12/2016 4:49:03 PM



- GENERAL NOTES:**
1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
 2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.

1 PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER

NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: SRJ DATE: 05/05/2016
DRAWN BY: AR/TE DATE: 05/06/2016

CHECKED BY: KAT DATE: 05/09/2016
APPROVED BY: KAT DATE: 05/20/2016

HORIZ. SCALE: 1/16" = 1'
VERT. SCALE: N/A

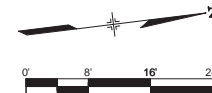


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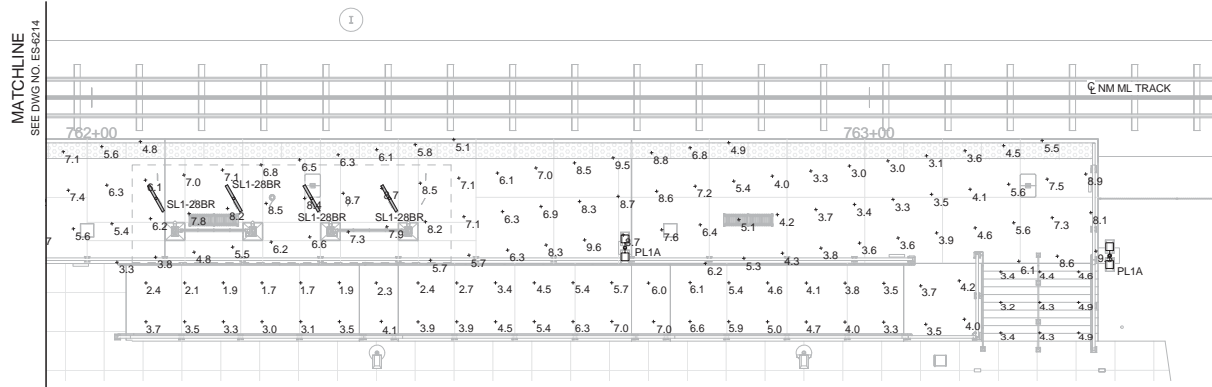
NORTH METRO RAIL LINE

ELECTRICAL
NORTHGLENN AT 112TH STATION
PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER



PRELIMINARY
NOT FOR
CONSTRUCTION

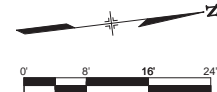
DWG No. ES-6214



1 PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH

GENERAL NOTES:

1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AGI32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.



NO.	REVISIONS	BY	DATE
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B	ISSUED FOR 90% SUBMITTAL CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: SRJ DATE: 05/05/2016
 DRAWN BY: AR/TE DATE: 05/06/2016

RTP
 REGIONAL RAIL PARTNERS
 A Balfour Beatty / GRHAM JACO GROUP

CHECKED BY: KAT DATE: 05/09/2016
 APPROVED BY: KAT DATE: 05/20/2016

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 Rail Line

REGIONAL TRANSPORTATION DISTRICT
 1600 BLAKE STREET
 DENVER, COLORADO 80202
 (303) 628-8000





NORTH METRO RAIL LINE

ELECTRICAL
 NORTHGLENN AT 112TH STATION
 PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH

PRELIMINARY
 NOT FOR
 CONSTRUCTION

DWG No. ES-6215

NORTH METRO RAIL LUMINAIRE SCHEDULE - NORTHGLENN AT 112TH STATION

IMAGE	KEY	LAMPS					DESCRIPTION	FINISH	MOUNTING	MANUFACTURER	CATALOG NUMBER	VOLTAGE	LLF	INPUT WATTS	NOTES
		QTY	TYPE	LUMENS	CR	CCT									
	PL1		LED ARRAY	9,375 LUMENS	70+	4,000 K	SINGLE HEAD PEDESTRIAN LED LUMINAIRE, DIE-CAST ALUMINUM HOUSING WITH 9" ARM LENGTH AND INTEGRAL THERMAL CONTROL SYSTEM, VIBRATION RESISTANT 3G RATING, UL WET LOCATION, FULL CUT-OFF TYPE III OPTICS.	BLACK	7" BASE DIAMETER, 20'-0" ROUND TAPERED STEEL POLE CUT TO 14'-3" WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (4) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 1 - 3 - 90LA - NW - JNV - BLP -OR APPROVED EQUAL- DS210 - R700A140 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - SL - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	89.0	1
	PL1A		LED ARRAY	9,375 LUMENS EACH LUMINAIRE	70+	4,000 K	SAME AS PL1 EXCEPT DOUBLE HEAD.	BLACK	7" BASE DIAMETER, 20'-0" ROUND TAPERED STEEL POLE CUT TO 14'-3" WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (4) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 2 - 3 - 90LA - NW - JNV - BLP -OR APPROVED EQUAL- DS210 - R700A140 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - SL - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	178.0	1
	PL1B		LED ARRAY	5,330 LUMENS	70+	4,000 K	SAME AS PL1 EXCEPT LUMEN OUTPUT.	BLACK	7" BASE DIAMETER, 20'-0" ROUND TAPERED STEEL POLE CUT TO 14'-3" WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (4) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 1 - 3 - 55LA - NW - JNV - BLP -OR APPROVED EQUAL- US210 - K700A140 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - SL - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	54.0	1
	PL2		LED ARRAY	16,732 LUMENS	70+	4,000 K	SINGLE HEAD PARKING LOT LED LUMINAIRE, DIE-CAST ALUMINUM HOUSING WITH 9" ARM LENGTH AND INTEGRAL THERMAL CONTROL SYSTEM, VIBRATION RESISTANT 3G RATING, UL WET LOCATION, FULL CUT-OFF TYPE III OPTICS.	BLACK	7" BASE DIAMETER, 25'-0" ROUND TAPERED STEEL POLE WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (5) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 1 - 3 - 180LA - NW - UNV - 3LP -OR APPROVED EQUAL- DS210 - R700A250 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	177.0	1
	PL2A		LED ARRAY	16,732 LUMENS EACH LUMINAIRE	70+	4,000 K	SAME AS PL2 EXCEPT DOUBLE HEAD.	BLACK	7" BASE DIAMETER, 25'-0" ROUND TAPERED STEEL POLE WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (5) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 2 - 3 - 180LA - NW - UNV - 3LP -OR APPROVED EQUAL- DS210 - R700A250 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	354.0	1
	SL1-28BR		LED ARRAY	2,582 LUMENS	80+	4,000 K	SURFACE MOUNT LINEAR LED LUMINAIRE, 4" LENGTH X 4" WIDTH X 4-5/8" OVERALL HEIGHT, EXTRUDED ALUMINUM HOUSING WITH GASKETTED ENDS AND HIGH IMPACT ACRYLIC LENS, UL WET LOCATION, IP66.	TO MATCH CANOPY FINISH - TNE MEC IRONWOOD 28BR	SURFACE MOUNT	ALW	LIGHTPLANE 3.5 WET LOCATION LP3.5SMB-WL - 4' - HP900-400K - LED - 277 - CUSTOM COLOR - CUSTOM MOUNTING BRACKET -OR APPROVED EQUAL-	277 V	0.87	28.0	1
	WM1		LED ARRAY	3,607 LUMENS	70+	4,000 K	PERFORMANCE LED WALL SCONCE, DIE-CAST ALUMINUM HOUSING AND INTEGRAL THERMAL CONTROL SYSTEM, DIFFUSING GLASS LENS, UL WET LOCATION, FULL CUT-OFF OPTICS.	BRONZE	WALL MOUNT, 8"-0" AFF TO BOTTOM OF FIXTURE	PHILIPS ORDCO	121 LED PERFORMANCE SCONCE 121 - 3 - 35LA-700 - NW - BL2 - DL -OR APPROVED EQUAL-	120 - 277 V	0.87	52.0	1

GENERAL NOTES:

1. CONTRACTOR SHALL BID BASED ON THE LUMINAIRE SCHEDULE ONLY. REFERENCE TO SPECIFICATIONS FOR SUBMITTAL, SUBSTITUTION, AND EQUIPMENT LIST REQUIREMENTS.
2. CONTRACTOR SHALL SUPPLY UNIT PRICES AT TIME OF BID. LOT PRICES ARE INACCEPTABLE.

DETAIL NOTES:

1. FINAL COLOR SELECTION SHALL BE APPROVED BY ARCHITECT AND OWNER PRIOR TO ORDERING.

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A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036,10.06	KAT	10/17/2014

DESIGNED BY: SJ/GP DATE: 05/05/2016
DRAWN BY: AR/TE DATE: 05/06/2016

R&P
REGIONAL RAIL PARTNERS
Balfour Beatty / GRAHAM JOINT VENTURE

CHECKED BY: KAT DATE: 05/09/2016
APPROVED BY: KAT DATE: 05/20/2016

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3025 S. Parker Road, Suite 1100
Aurora, Co 80014-2950
303.696.2602 fax 303.696.0812
www.ssgmep.com

RTD FastTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BEARS STREET
DENVER, CO 80202 80202
(303) 628-8000

NORTH METRO RAIL LINE
ELECTRICAL
NORTHGLENN AT 112TH STATION
LUMINAIRE SCHEDULE

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CONSTRUCTION

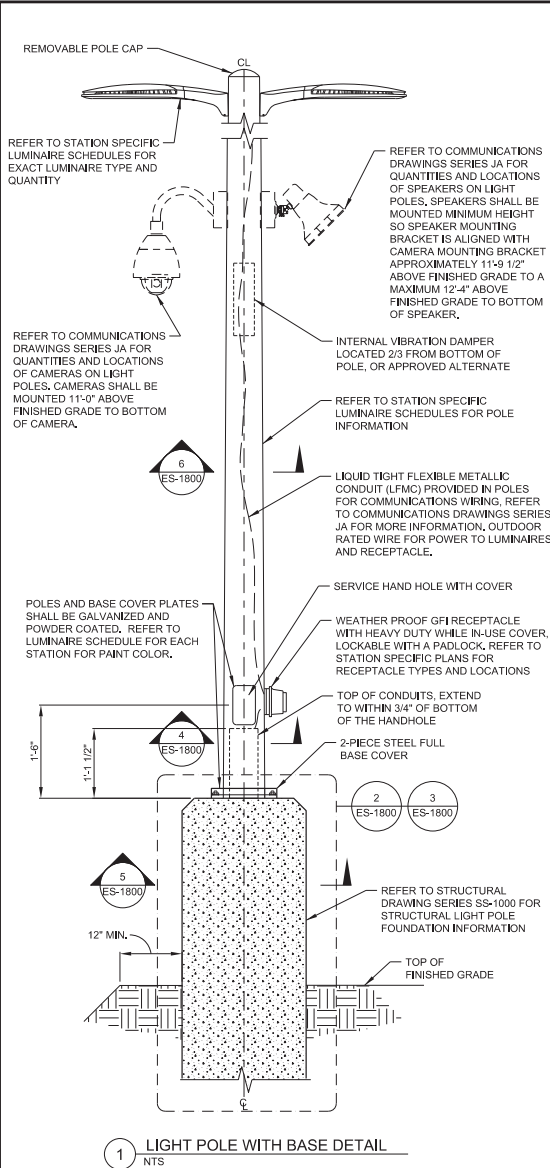
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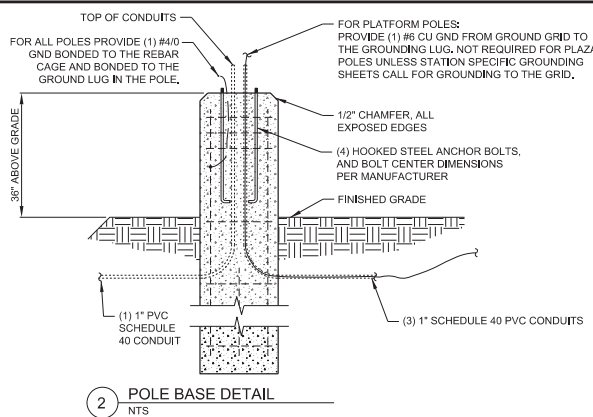
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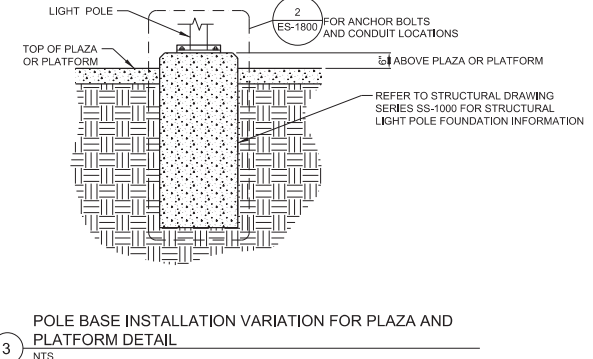
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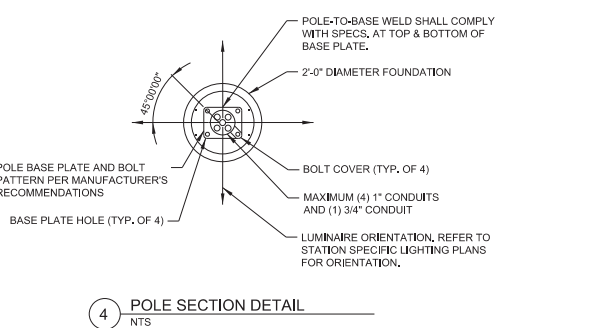
1 LIGHT POLE WITH BASE DETAIL
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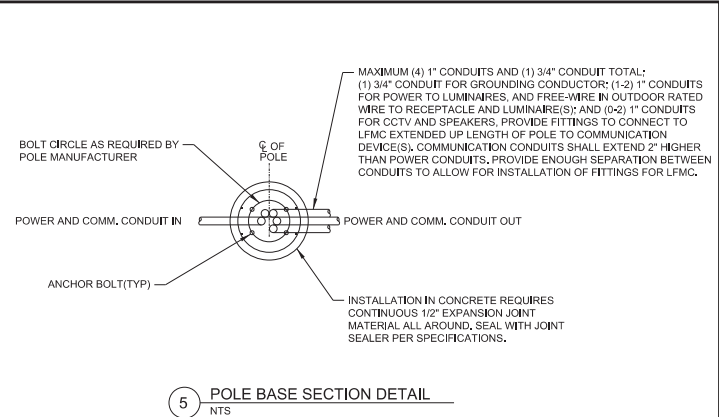
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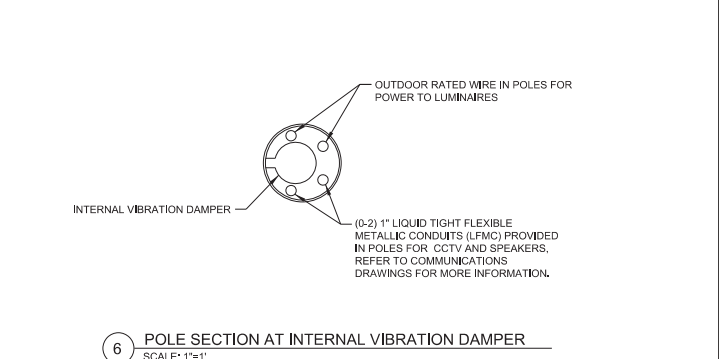
3 POLE BASE INSTALLATION VARIATION FOR PLAZA AND PLATFORM DETAIL
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4 POLE SECTION DETAIL
NTS



5 POLE BASE SECTION DETAIL
NTS



6 POLE SECTION AT INTERNAL VIBRATION DAMPER
SCALE: 1"=1'

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	KAT	11/15/2016

DESIGNED BY: SRJ DATE: 11/02/2015
 DRAWN BY: AR/TE DATE: 11/03/2015
 CHECKED BY: KAT DATE: 11/14/2016
 APPROVED BY: KAT DATE: 11/15/2016

R-P
 REGIONAL RAIL PARTNERS
 RailAmerica / GRAHAM

SSG MEP
 SSG MEP, Inc.
 3025 S. Parker Road, Suite 1100
 Aurora, Co 80014-2950
 303.696.2602 fax 303.696.0812
 www.ssgmep.com

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 North Metro Rail Line
 REGIONAL TRANSPORTATION DISTRICT
 1600 BLAIR STREET
 DENVER, CO 80202
 (303) 628-8000

NORTH METRO RAIL LINE
 ELECTRICAL STATIONS, STANDARD DETAILS
 PEDESTRIAN LIGHT POLE DETAILS

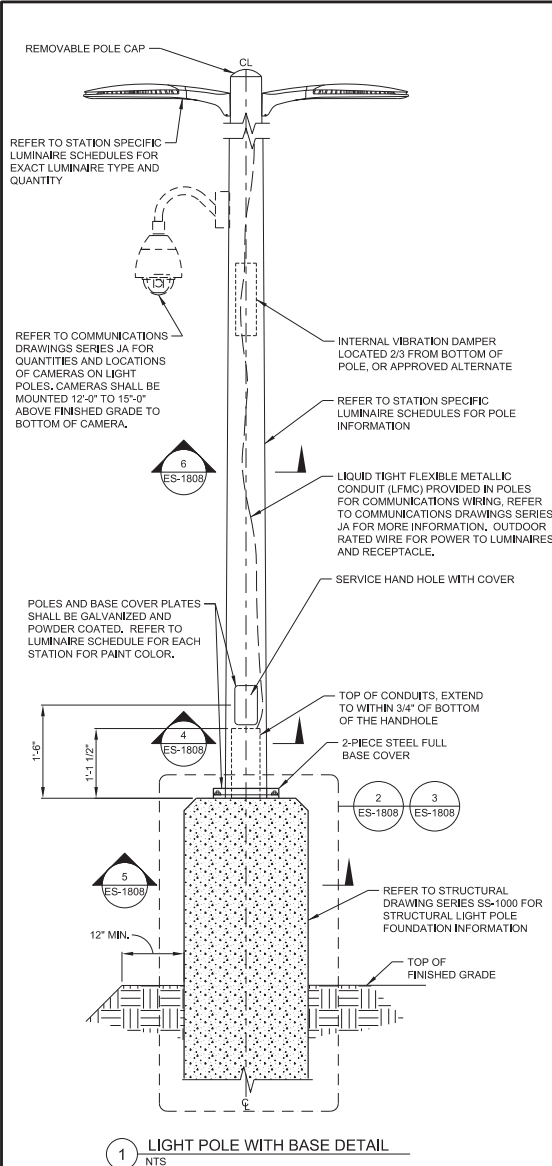
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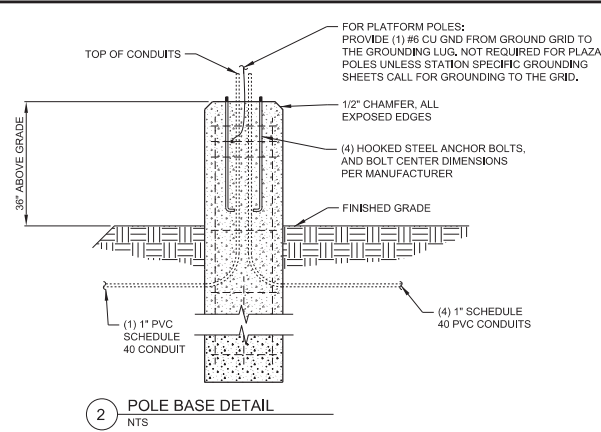
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11/14/2016

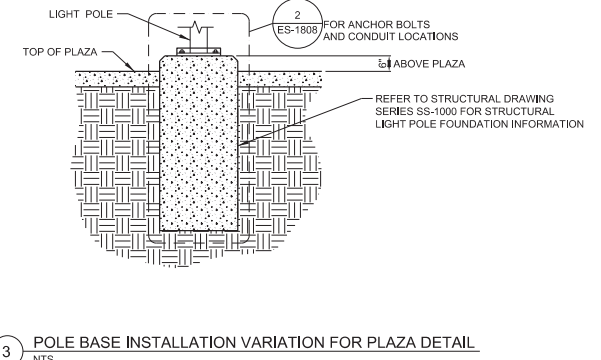
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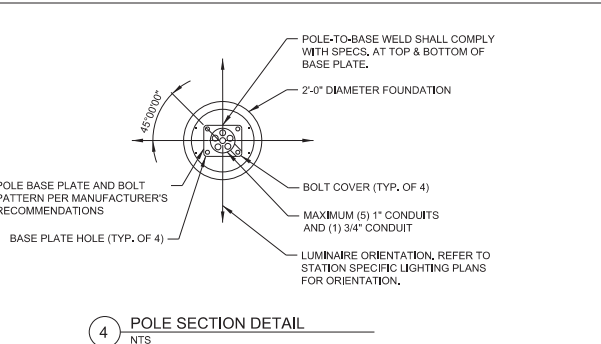
1 LIGHT POLE WITH BASE DETAIL
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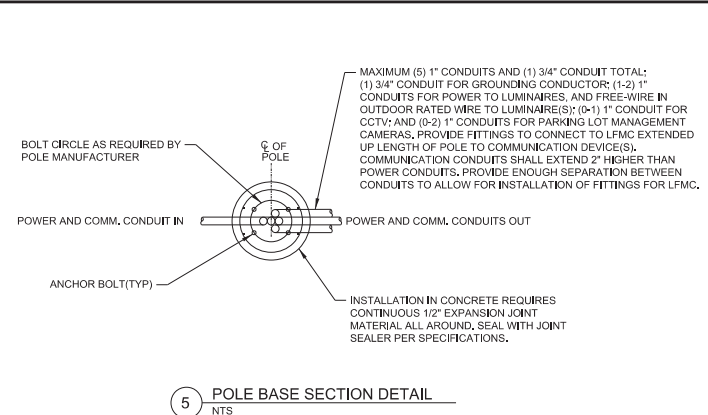
2 POLE BASE DETAIL
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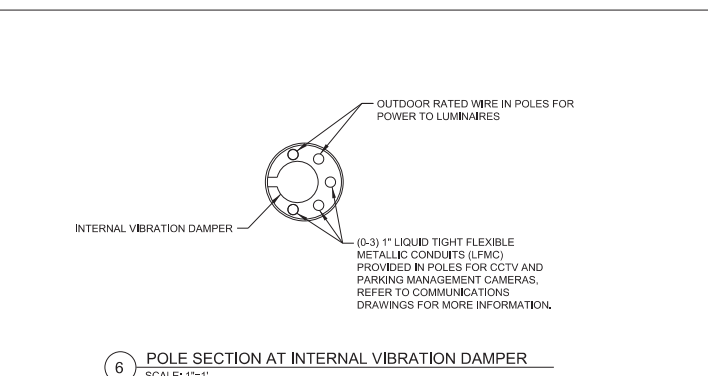
3 POLE BASE INSTALLATION VARIATION FOR PLAZA DETAIL
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4 POLE SECTION DETAIL
NTS



5 POLE BASE SECTION DETAIL
NTS



6 POLE SECTION AT INTERNAL VIBRATION DAMPER
SCALE: 1"=1'

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	KAT	11/15/2016

DESIGNED BY: SRJ DATE: 11/02/2015
DRAWN BY: AR/TE DATE: 11/03/2015
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Railway Partners
REGIONAL RAIL PARTNERS
RAILWAY PARTNERS / GRAHAM JOINT VENTURES

SSG MEP
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DENVER, CO 80202
(303) 628-8000

NORTH METRO RAIL LINE
ELECTRICAL STATIONS, STANDARD DETAILS
PARKING LOT LIGHT POLE DETAILS

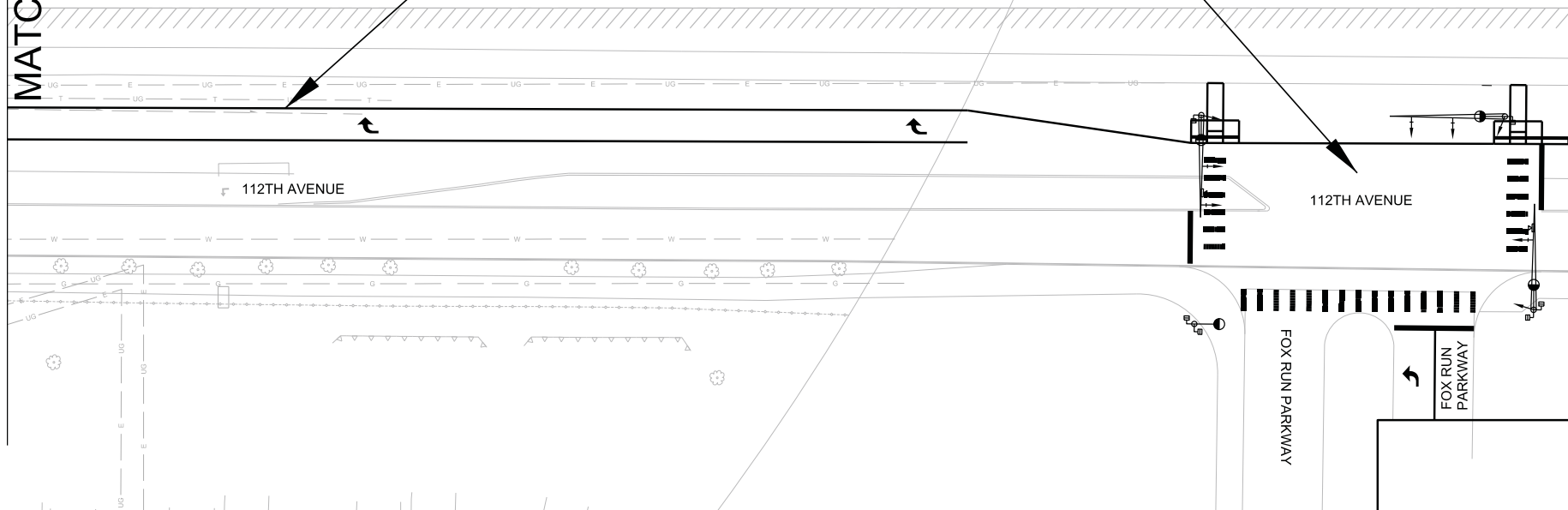
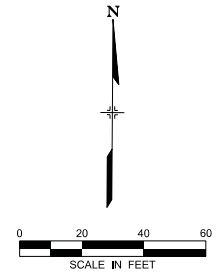
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MATCHLINE SHEET 1 OF 2
MATCHLINE SHEET 2 OF 2

FUTURE INTERSECTION IMPROVEMENTS
BY CITY OF NORTHGLENN

FUTURE LANE WIDENING
BY CITY OF NORTHGLENN



NO.	REVISIONS	BY	DATE

DESIGNED BY:	DATE:	CHECKED BY:	DATE:	HORIZ. SCALE: 1" = 40'
DRAWN BY:	DATE:	APPROVED BY:	DATE:	VERT. SCALE: N/A

RTD FastTracks
North Metro
Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLANS STREET
DENVER, COLORADO 80202
(303) 628-6000

NORTH METRO RAIL LINE
SIGNING AND STRIPING PLAN
112TH AVENUE & YORK STREET (CITY OF NORTHGLENN)
SHEET 2 OF 2

DWG No. X-101

Exhibit C

NORTHGLENN AT 112TH

SOUTHEAST QUARTER SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE 6TH P.M.
CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO

SHEET 1 OF 2

LEGAL DESCRIPTION:

NORTHGLENN AT 112TH OF THE RTD NORTH METRO CORRIDOR COMMUTER RAIL PROJECT, BEING ALL OF THE TRACTS OF LAND DESCRIBED AT RECEPTION NO. 201500064551, RECORDED ON AUGUST 6, 2015, AND RECEPTION NO. 2015000053437, RECORDED ON JULY 7, 2015, BOTH IN THE ADAMS COUNTY CLERK AND RECORDER'S OFFICE, LOCATED IN THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO, ADDITIONALLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE EAST QUARTER CORNER OF SAID SECTION 2 (A FOUND 3-1/4" ALUMINUM CAP IN RANGE BOX STAMPED "JR ENG T2S R68W 1/4 S2 S1 1998 PLS 24308"); WHENCE THE SOUTHEAST CORNER OF SAID SECTION 2 (A FOUND 2-1/2" ALUMINUM CAP SET ATOP AN AXLE IN RANGE BOX STAMPED "T2S R68W S2 S1 S11 S12 2000 PLS 28286") BEARS S00°34'18"E A DISTANCE OF 2,627.20 FEET (BASIS OF BEARING - ASSUMED); THENCE S00°34'18"E, COINCIDENT WITH THE EASTERLY LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 1252.23 FEET; THENCE S89°25'42"W A DISTANCE OF 30.00 FEET TO THE WESTERLY RIGHT OF WAY LINE OF YORK STREET AND THE NORTHEAST CORNER OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000064551, ALSO BEING THE POINT OF BEGINNING;

THENCE S00°34'18"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 674.65 FEET TO THE SOUTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000053437; THENCE S89°26'30"W, COINCIDENT WITH SAID SOUTHERLY LINE, A DISTANCE OF 305.39 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE RTD NORTH METRO CORRIDOR AS DESCRIBED AT RECEPTION NO. 2009000046216, RECORDED JUNE 26, 2009 IN SAID CLERK AND RECORDER'S OFFICE; THENCE N07°30'24"E, COINCIDENT WITH SAID EASTERLY RIGHT OF WAY LINE, A DISTANCE OF 681.34 FEET TO THE NORTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000064551; THENCE N89°25'42"E, COINCIDENT WITH SAID NORTHERLY LINE, A DISTANCE OF 209.65 FEET TO THE POINT OF BEGINNING.

CONTAINING 173,727 SQUARE FEET, (3.988 ACRES), MORE OR LESS.

OWNERSHIP AND DEDICATION:

THE UNDERSIGNED CERTIFIES TO AND FOR THE BENEFIT OF THE CITY COUNCIL OF NORTHGLENN CITY, COLORADO, THAT AS OF THE DATE SET FORTH BELOW, WE THE REGIONAL TRANSPORTATION DISTRICT OF DENVER, BEING THE OWNER OF THE LAND DESCRIBED ABOVE, HAVE GOOD RIGHT AND FULL POWER TO CONVEY, ENCUMBER AND SUBDIVIDE SAME, AND THAT THE PROPERTY IS FREE AND CLEAR OF ALL LIENS, ENCUMBRANCES, EASEMENTS AND RIGHTS OF WAY EXCEPT THE EASEMENTS AND RIGHTS OF WAY DEPICTED ON THIS PLAT. IN THE EVENT OF A DEFECT IN SAID TITLE WHICH BREACHES THE WARRANTIES IN THIS CERTIFICATE, THE UNDERSIGNED, JOINTLY AND SEVERALLY, AGREE(S) TO REMEDY SUCH DEFECT UPON DEMAND BY NORTHGLENN CITY, WHICH REMEDY SHALL NOT BE DEEMED EXCLUSIVE.

KNOW ALL MEN BY THESE PRESENTS, THAT THE UNDERSIGNED, BEING THE OWNER, MORTGAGEE OR LIEN HOLDER OF THE LAND DESCRIBED ABOVE, HAVE CAUSED THE LAND TO BE LAID OUT AND PLATTED UNDER THE NAME OF NORTHGLENN AT 112TH AND DO HEREBY DEDICATE AND GRANT TO THE PUBLIC FOREVER AND IN FEE SIMPLE THE ROADS AND OTHER PUBLIC WAYS AND LANDS SHOWN HEREON, AND DO HEREBY DEDICATE TO NORTHGLENN CITY, AND APPROPRIATE UTILITY COMPANIES AND EMERGENCY ASSISTANCE ENTITIES, THE EASEMENTS AS SHOWN HEREON FOR THE PURPOSES STATED IN COMPLIANCE WITH THE CITY OF NORTHGLENN SUBDIVISION REGULATIONS AND THE LANDOWNERS SHALL BEAR ALL EXPENSE INVOLVED IN PLANNING, DESIGN, AND CONSTRUCTION OF ALL PUBLIC IMPROVEMENTS EXCEPT TO THE EXTENT EXPRESSLY STATED IN ANY CITY-APPROVED AND RECORDED SUBDIVISION IMPROVEMENT AGREEMENT. DEDICATION SHALL BE FINAL UPON ADOPTION BY THE CITY COUNCIL ACCEPTING THE PROPERTY DEDICATED BY THIS PLAT. EXCEPT AS OTHERWISE STATED ON THIS PLAT, THERE SHALL BE NO LIMITATION OR RESTRICTION UPON THE PURPOSE OR PUBLIC USE OF PROPERTY DEDICATED BY THIS PLAT.

IN WITNESS WHEREOF, WE DO HEREUNTO SET OUR HANDS AND SEALS THIS ____ DAY OF _____, 2016.

REGIONAL TRANSPORTATION DISTRICT

(IF BY CORPORATION, PRESIDENT SIGNS, SECRETARY ATTESTS AND CORPORATE SEAL IS AFFIXED)

STATE OF _____)
) SS.

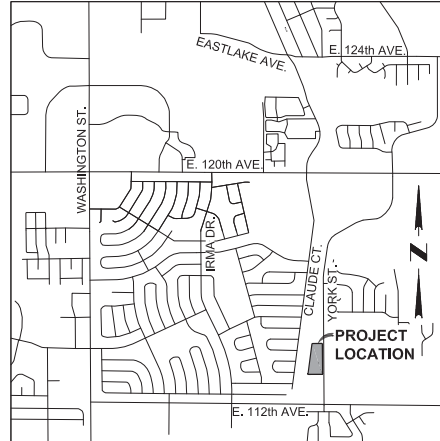
COUNTY OF _____)

ACKNOWLEDGED BEFORE ME THIS ____ DAY OF _____, ____ BY

WITNESS MY HAND AND OFFICIAL SEAL.

NOTARY PUBLIC

MY COMMISSION EXPIRES: _____



VICINITY MAP
SCALE: 1" = 2000'

STANDARD NOTES:

- STREET MAINTENANCE. IT IS MUTUALLY AGREED BY THE SUBDIVIDER AND THE CITY THAT THE DEDICATED PUBLIC WAYS, INCLUDING BUT NOT LIMITED TO STREETS, ROADS, DRIVES AND ALLEYS, SHOWN ON THIS PLAT, WILL NOT BE ACCEPTED FINALLY FOR MAINTENANCE BY THE CITY UNTIL AND UNLESS THE SUBDIVIDER CONSTRUCTS THE SAME IN ACCORDANCE WITH THE SUBDIVISION IMPROVEMENT AGREEMENT AND SUBDIVISION REGULATIONS IN EFFECT AT THE DATE OF RECORDING THIS PLAT AND APPROVAL OF THE CITY HAS ISSUED TO THAT EFFECT. YORK STREET EAST OF THE EAST LINE OF THE SOUTH HALF OF SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE SIXTH PRINCIPAL MERIDIAN LIES WITHIN THE CITY OF THORNTON. YORK STREET WEST OF SAID EAST LINE LIES WITHIN THE CITY OF NORTHGLENN.
- DRAINAGE MAINTENANCE. THE OWNER, ITS LEGAL REPRESENTATIVES, HEIRS, EXECUTORS, ADMINISTRATORS, SUCCESSORS IN INTEREST AND ASSIGNS SHALL BE JOINTLY AND SEVERALLY LIABLE AND RESPONSIBLE FOR MAINTAINING THE STRUCTURAL INTEGRITY AND OPERATIONAL FUNCTIONS OF ALL DRAINAGE FACILITIES LOCATED ON THE PROPERTY SHOWN HEREON UNLESS OTHERWISE SPECIFIED HEREIN, INCLUDING BUT NOT LIMITED TO, PRIVATE DRAINAGE FACILITIES AND PUBLIC AND PRIVATE DRAINAGE EASEMENTS. DRAINAGE IMPROVEMENTS ARE SUBJECT TO SECTION 16-17-13. POST-CONSTRUCTION REQUIREMENT OF PERMANENT BMPS, AS AMENDED.
- VEHICULAR ACCESS CONTROL. VEHICULAR ACCESS TO PUBLIC STREETS IN THIS SUBDIVISION SHALL BE SOLELY BY WAY OF DRIVEWAYS SPECIFICALLY APPROVED BY THE CITY OF NORTHGLENN.
- UNDERGROUND UTILITIES. ALL TELEPHONE LINES, ELECTRIC LINES, CABLE TELEVISION LINES AND OTHER LIKE UTILITY SERVICES SHALL BE PLACED UNDERGROUND. TRANSFORMER, SWITCHING BOXES, TERMINAL BOXES, METER CABINETS, PEDESTALS, DUCTS AND OTHER FACILITIES NECESSARILY APPURTENANT TO SUCH UNDERGROUND UTILITIES MAY BE PLACED ABOVE GROUND.

SURVEYOR'S NOTES:

- THE LINEAL UNIT USED IN THE PREPARATION OF THIS PLAT IS THE UNITED STATES SURVEY FOOT AS DEFINED BY THE U.S. NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION'S NATIONAL GEODETIC SURVEY.
- ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER FIRST DISCOVERY OF SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.

SURVEYOR'S CERTIFICATE:

I, KENNETH W. CARLSON, DO HEREBY CERTIFY THAT THE SURVEY OF THE BOUNDARY OF NORTHGLENN AT 112TH WAS MADE UNDER MY SUPERVISION AND THE ACCOMPANYING PLAT ACCURATELY REPRESENTS SAID SURVEY.

THE EAST LINE OF THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE SIXTH PRINCIPAL MERIDIAN, CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO IS ASSUMED TO BEAR SOUTH 00°34'18" EAST. MONUMENTS FOUND FOR THE EAST QUARTER CORNER AND THE SOUTHEAST CORNER OF SAID SECTION 2 ARE SHOWN ON THE ACCOMPANYING PLAT. THIS SUBDIVISION PLAT COMPLIES WITH COLORADO REVISED STATUTE 38-51-105.

KENNETH W. CARLSON
COLORADO REGISTRATION NO. 24942
FOR AND ON BEHALF OF JACOBS ENGINEERING GROUP INC.

TITLE INSURANCE NOTE:

THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY JACOBS ENGINEERING GROUP, INC. TO DETERMINE OWNERSHIP, RIGHTS OF WAY, OR EASEMENTS OF RECORD. FOR ALL INFORMATION REGARDING EASEMENTS, RIGHTS OF WAY AND EASEMENTS OF RECORD JACOBS ENGINEERING GROUP, INC. RELIED UPON TITLE COMMITMENT NUMBER 063662015, EFFECTIVE DATE FEBRUARY 17, 2016, PREPARED BY WESTCOR LAND TITLE INSURANCE COMPANY DATED FEBRUARY 17, 2016, AS ISSUED BY H.C. PECK & ASSOCIATES, TO DELINEATE THE AFORESAID INFORMATION.

PLANNING COMMISSION APPROVAL:

THIS PLAT WAS RECOMMENDED FOR APPROVAL BY THE CITY OF NORTHGLENN, COLORADO, THIS ____ DAY OF _____, 2016.

BY _____ CHAIRPERSON

PLANNING AND DEVELOPMENT APPROVAL:

THIS MINOR SUBDIVISION HAS BEEN APPROVED BY THE DIRECTOR OF THE DEPARTMENT OF PLANNING AND DEVELOPMENT OF THE CITY OF NORTHGLENN, COLORADO ON THE ____ DAY OF _____, 2016.

PLANNING AND DEVELOPMENT, DIRECTOR

PUBLIC WORKS AND UTILITIES APPROVAL:

THIS MINOR SUBDIVISION HAS BEEN APPROVED BY THE DIRECTOR OF THE DEPARTMENT OF PUBLIC WORKS AND UTILITIES OF THE CITY OF NORTHGLENN, COLORADO ON THE ____ DAY OF _____, 2016.

PUBLIC WORKS AND UTILITIES, DIRECTOR

CITY APPROVAL:

THIS PLAT IS APPROVED FOR FILING AND THE CITY HEREBY ACCEPTS THE DEDICATION OF THE PUBLIC WAYS SHOWN HEREON, INCLUDING BUT NOT LIMITED TO, THE STREETS, ROADS, DRIVEWAYS AND ALLEYS FOR PUBLIC USE SUBJECT TO THE PROVISIONS CONTAINED IN THE STREET MAINTENANCE NOTE HEREIN, THE DEDICATION OF PUBLIC LANDS SHOWN HEREON, AND THE DEDICATION OF THE EASEMENTS SHOWN HEREON. SIGNED THIS ____ DAY OF _____, 2016

BY _____ MAYOR

ATTEST: BY _____ CITY CLERK

CERTIFICATE OF CLERK AND RECORDER:

THIS PLAT WAS FILED FOR RECORD IN THE OFFICE OF ADAMS COUNTY CLERK AND RECORDER, IN THE STATE OF COLORADO, AT ____ M. ON THE ____ DAY OF _____, A.D. 2016.

DEPUTY: _____ COUNTY CLERK AND RECORDER

RECEPTION NO. _____

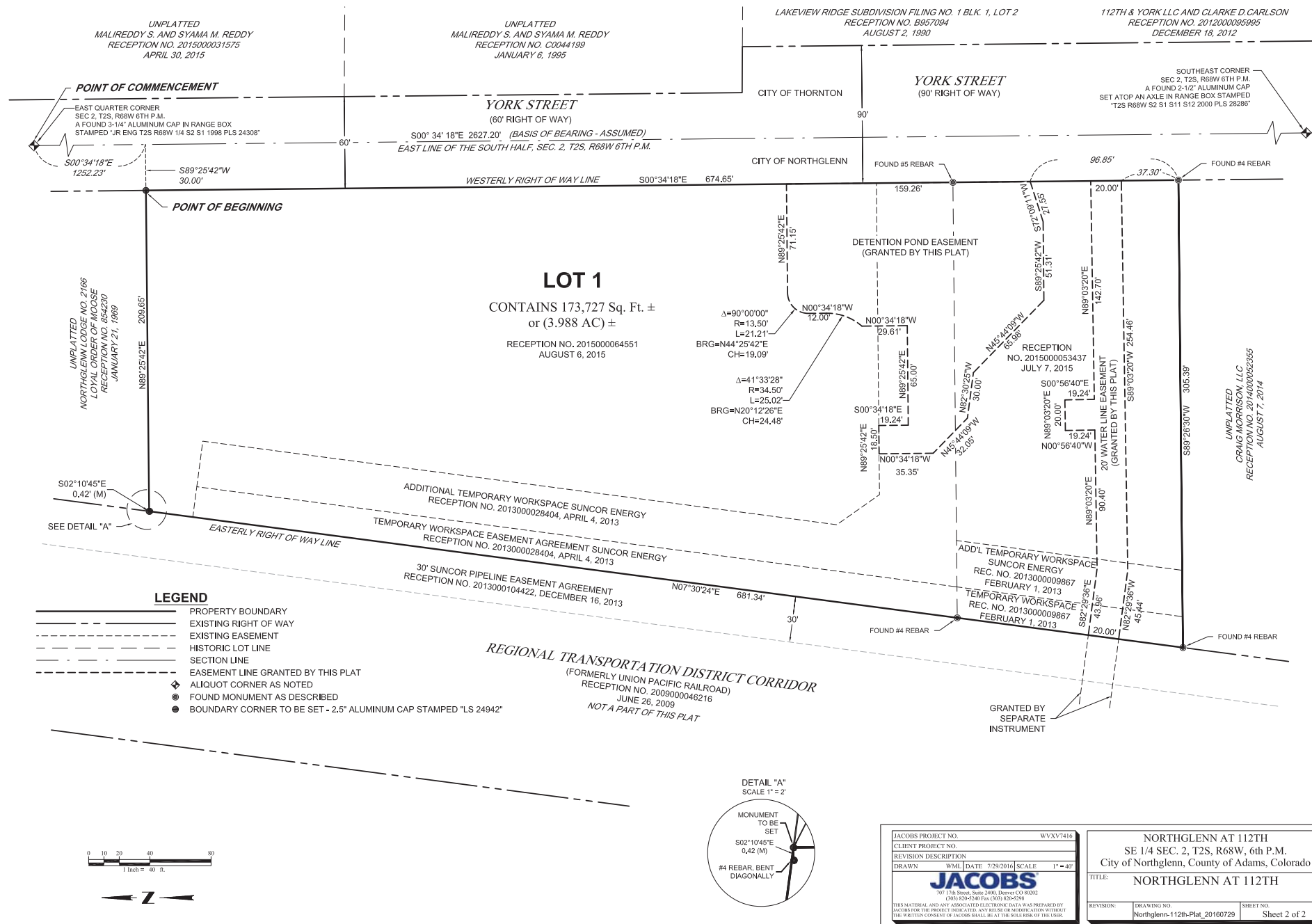
JACOBS PROJECT NO. WVXV7416		NORTHGLENN AT 112TH	
CLIENT PROJECT NO.		SE 1/4 SEC. 2, T2S, R68W, 6TH P.M.	
REVISION DESCRIPTION		City of Northglenn, County of Adams, Colorado	
DRAWN	DATE 7/29/2016	SCALE 1" = 40'	TITLE: NORTHGLENN AT 112TH
<small>707 F 15th Street, Suite 2400, Denver CO 80202 (303) 425-2240 Fax (303) 425-2296 THIS MATERIAL AND ANY ASSOCIATED ELECTRONIC DATA WAS PREPARED BY JACOBS FOR THE PROJECT INDICATED. ANY REUSE OR MODIFICATION WITHOUT THE WRITTEN CONSENT OF JACOBS SHALL BE AT THE SOLE RISK OF THE USER.</small>			
REVISION:	DRAWING NO. Northglenn-112th-Plat_20160729	SHEET NO.	Sheet 1 of 2

NORTHGLENN AT 112TH

BEING A PORTION OF THE SOUTHEAST QUARTER SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE 6TH P.M.

CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO

SHEET 2 OF 2





Northglenn-112th station
Planned Unit Development (PUD)
Case # Z-2-17

Presented to:
The Northglenn City Council:
March 13th, 2017



INTRODUCTION

Location - 11355 York St

Request - Rezoning to Preliminary and Final PUD

Proposal - Commuter Rail Station and Parking Lot

Approval Criteria – PUD

- That a need exists for the proposal;
- That this particular parcel of ground is indeed the correct site for the proposed development;
- That there has been an error in the original zoning; or
- That there have been significant changes in the area to warrant a zone change;
- That adequate circulation exists and traffic movement would not be impeded by development; and
- That additional municipal service costs will not be incurred which the City is not prepared to meet.



OVERVIEW

- Preliminary and Final PUD approval requested. City Council will need to approve the Preliminary PUD rezoning.

BACKGROUND

- Currently vacant lot(s)
- RTD North Metro commuter rail line to open in 2018
- Current Zoning:
 - C-4 Commercial with Conditional Zoning
 - AG Agriculture
- Applicant proposes Preliminary and Final PUD to allow facilities associated with a commuter rail stop

VICINITY MAP



LAND USE MAP





MINOR SUBDIVISION

- Lot Layout
 - Combine the 2 parcels into one
 - Proposed lot size is 3.988 acres
- Utilities
 - 20' waterline easement dedicated to City
 - Drainage easement dedicated to City

UNPLATTED
MALIREDDY S. AND SYAMA M. REDDY
RECEPTION NO. 2015000031575
APRIL 30, 2015

UNPLATTED
MALIREDDY S. AND SYAMA M. REDDY
RECEPTION NO. C0044199
JANUARY 6, 1995

LAKEVIEW RIDGE SUBDIVISION FILING NO. 1 BLK. 1, LOT 2
RECEPTION NO. B957094
AUGUST 2 1990

112TH & YORK LLC AND CLARKE D. CARLSON
RECEPTION NO. 2012000095895
DECEMBER 18, 2012

- POINT OF COMMENCEMENT

EAST QUARTER CORNER
SEC 2, T2S, R68W 6TH P.M.
A FOUND 3-1/4" ALUMINUM CAP IN RANGE BOX
STAMPED "JR ENG T2S R68W 1/4 S2 S1 1998 PLS 24308"

YORK STREET
(60' RIGHT OF WAY)

S00° 34' 18"E 2627.20' (BASIS OF BEARING - ASSUMED)
EAST LINE OF THE SOUTH HALF, SEC. 2, T2S, R68W 6TH P.M.

CITY OF THORNTON

YORK STREET
(90' RIGHT OF WAY)

SOUTHEAST CORNER
SEC 2, T2S, R88W 6TH P.M.
A FOUND 2-1/2" ALUMINUM CAP
SET ATOP AN AXLE IN RANGE BOX STAMPED
"T2S R88W S2 S1 S11 S12 2000 PLS 28286"

- POINT OF BEGINNING

UNPLATTED
NORTHGLENN LODGE NO. 2188
LOYAL ORDER OF MOOSE
RECEPTION NO. 854230
JANUARY 21, 1969

LOT 1

**CONTAINS 173,727 Sq. Ft. ±
or (3.988 AC) ±**

RECEPTION NO. 2015000064551
AUGUST 6, 2015

$\Delta=90^{\circ}00'00''$
 $R=13.50'$
 $L=21.21'$

RG=N44°25'42"E
CH=19.09'

 $\Delta=41^{\circ}33'$
 $R=34$

BRG=N20°12'26"

RECEPTION
NO. 2015000053437

JULY 7, 2015
S00°56'40"E

19.24

9°03'2
20.00

UNPLATTED
CRAIG MORRISON, LLC
RECEPTION NO. 2014000052

LEGEND

- PROPERTY BOUNDARY
- EXISTING RIGHT OF WAY
- EXISTING EASEMENT
- HISTORIC LOT LINE
- SECTION LINE
- EASEMENT LINE GRANTED BY THIS PLAT
- ◆ ALIQUOT CORNER AS NOTED
- FOUND MONUMENT AS DESCRIBED
- BOUNDARY CORNER TO BE SET - 2.5" AL

RECEIVED
RECEPTION NO. 2013000028404, APRIL 4, 2013

TEMPORARY WORKSPACE EASEMENT AGREEMENT SUNCOR ENERGY
RECEPTION NO. 2013000028404, APRIL 4, 2013

30' SUNCOR PIPELINE EASEMENT AGREEMENT
RECEPTION NO. 2013000104422, DECEMBER 16, 2013

REGIONAL TRANSPORTATION DISTRICT CORRIDOR
(FORMERLY UNION PACIFIC RAILROAD)
RECEPTION NO. 2008000046216
JUNE 28, 2009
NOT A PART OF THIS PLAT

GRANTED BY
SEPARATE
INSTRUMENT

DETAIL "A"
SCALE 1" = 2'

MONUMENT

TO BE SET

S02°10'45"E
0.42 (M)

REBAR, BENT
DIAGONALLY

—

JACOBS

LEGAL DESCRIPTION OF PROPERTY

NORTHGLENN AT 112TH OF THE RTD NORTH METRO CORRIDOR COMMUTER RAIL PROJECT, BEING ALL OF THE TRACTS OF LAND DESCRIBED AT RECEPTION NO. 201000004651, RECORDED ON AUGUST 6, 2015, AND RECEPTION NO. 201500003437, RECORDED ON JULY 7, 2015, BOTH IN THE ADAMS COUNTY CLERK AND RECORDER'S OFFICE, LOCATED IN THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 2 SOUTH, RANGE 86 WEST OF THE 8TH PRINCIPAL MERIDIAN, CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO, ADDITIONALLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE EAST QUARTER CORNER OF SAID SECTION 2 (A FOUND 3-1/4" ALUMINUM CAP IN RANGE BOX STAMPED "JL ENG 128 R86W 14 S2 S1 1886 PLS 24308"); WHENCE THE SOUTHEAST CORNER OF SAID SECTION 2 (A FOUND 2-1/2" ALUMINUM CAP SET ON AN AXLE IN RANGE BOX STAMPED "128 R86W S2 S1 S12 2000 PLS 28286") BEARS S89°34'16"E A DISTANCE OF 2,627.20 FEET (BASIS OF BEARING - ASSUMED); THENCE N89°09'26"W, COINCIDENT WITH THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 30.01 FEET TO THE WESTERLY RIGHT OF WAY LINE OF YORK STREET;

THENCE S89°34'16"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 1,252.97 FEET TO THE NORTHEASTERLY CORNER OF SAID TRACT OF LAND DESCRIBED AT RECEPTION NO. 201500004651, ALSO BEING THE POINT OF BEGINNING;

THENCE S89°34'16"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 874.86 FEET TO THE SOUTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 201500003437;

THENCE S89°34'16"W, COINCIDENT WITH SAID SOUTHERLY LINE, A DISTANCE OF 305.39 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE RTD NORTH METRO CORRIDOR AS DESCRIBED AT RECEPTION NO. 2009000046216, RECORDED JUNE 26, 2009 IN SAID CLERK AND RECORDER'S OFFICE;

THENCE N07°02'24"E, COINCIDENT WITH SAID EASTERLY RIGHT OF WAY LINE, A DISTANCE OF 681.34 FEET TO THE NORTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 201500004651;

THENCE N89°25'42"E, COINCIDENT WITH SAID NORTHERLY LINE, A DISTANCE OF 209.85 FEET TO THE POINT OF BEGINNING.

CONTAINING 175,727 SQUARE FEET, (4.066 ACRES), MORE OR LESS.

Prepared by: *Kenneth W. Carlson*
Kenneth W. Carlson P.E. 24912
For and on behalf of Jacobs Engineering Group Inc.
707 17th Street, Suite 3000
Denver, CO 80202
303.820.5240

GENERAL NOTES

RTD IS RESPONSIBLE FOR MAINTENANCE OF COMMON AND RIGHT-OF-WAY LANDSCAPE.

ALL WATER AND SANITARY SEWER CONSTRUCTION SHALL CONFORM TO CITY OF NORTHGLENN UTILITY STANDARDS AND SPECIFICATIONS.

INSTALLATION OF ALL UTILITY TAPS SHALL BE OBSERVED BY CITY OF NORTHGLENN PERSONNEL.

CONSTRUCTION OF THE YORK STREET PUBLIC RIGHT-OF-WAY SHALL BE IN CONFORMANCE WITH CITY OF THORNTON STREET STANDARDS AND SPECIFICATIONS. ALL OTHER CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY SHALL BE IN CONFORMANCE WITH CITY OF NORTHGLENN STREET AND DRAINAGE STANDARDS AND SPECIFICATIONS.

ALL CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY SHALL MEET THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT OF 1990.

PRIOR TO ISSUANCE OF A BUILDING PERMIT, THE CITY MUST REVIEW AND APPROVE THE FOLLOWING:

1. GRADING AND DRAINAGE PLAN REPORT
2. EROSION CONTROL PLAN
3. WATER AND SANITARY SEWER UTILITY CONSTRUCTION PLAN
4. LANDSCAPE PLAN
5. IRRIGATION PLAN
6. COMPLETE CONSTRUCTION PLANS FOR ALL REQUIRED PUBLIC IMPROVEMENTS

ALL EXISTING METER VAULTS, UTILITY SERVICE LINES APPURTENANCES, STREET CURB, GUTTER, SIDEWALK AND TRAIL ON AND ADJACENT TO THIS SITE SHALL BE REPAIRED AS NECESSARY AND BROUGHT TO CURRENT CITY STANDARDS.

STREET LIGHTS ON PUBLIC STREETS ADJACENT TO THIS SITE, IF NECESSARY, SHALL BE DESIGNED AND INSTALLED BY XCEL ENERGY ACCORDING TO CITY APPROVED STANDARDS AND PAID FOR BY THE DEVELOPER OR PROPERTY OWNER PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY.

DESCRIPTION OF PROPOSED ARCHITECTURAL USE, STYLE, AND MATERIALS

112TH STATION IS A SINGLE-SIDE PLATFORM CONFIGURATION WITH A TRANSITION PLAZA AND PARKING INCLUDING 316 PARKING SPACES. THE ARCHITECTURE AND LANDSCAPE APPROACH INCLUDES A TREE LIKE CANOPY STRUCTURE REMINISCENT OF THE MANICURED PARKS OF THE 1960S INCORPORATED IN THE SUBURBAN LIFESTYLE AND MODERN AMERICAN LIVING. THE PLAZA INCLUDES TWO LARGE PLANTERS AND A MANICURED TREE LAWN ALONG YORK STREET. THE PLANNING CONCEPT INCLUDES A PALETTE OF DIVERSE, LOW WATER SPECIES TO SUPPORT NORTHGLENN'S WATER CONSERVING MEASURES.

PRELIMINARY AND FINAL P.U.D NORTH METRO RAIL LINE P.U.D. 112TH AVENUE STATION, NORTHGLENN, COLORADO COVER SHEET



SITE DATA TABLE

LOT SIZE	173,727 SQ FT	3.96 ACRES
BUILDING FLOOR AREA	9,880 SQ FT ALLOWABLE PERIC	219,118
BUILDING CONSTRUCTION TYPE	EXISTING	PROPOSED
ZONING DISTRICT	C-48 A1	P.U.D.
LAND USE	ALLOWED	PROPOSED
BUILDING HEIGHT	35'	ONE 1 STORY, 12' - 7'
FLOOR AREA RATIO	REQUIRED	PROPOSED
	NORTH	58.11
	EAST	24
	SOUTH	10
	WEST	10
	PARKING SPACES	316
	LANDSCAPE SPACES	316 TOTAL SPACES
OFF-STREET PARKING	LANDSCAPE SPACES	7
	LANDSCAPE SPACES	6 REGULAR ADA SPACES, 3 VAN ACCESSIBLE ADA SPACES
LANDSCAPE AREA	16%	66,333 SQ FT

LAND USE TABLE

SURFACE	AREA (SQ FT)
STATION PLATFORM AND SERVICE	11,599
STATION PARKING LOT	106,957
BUS LOOP	18,334
STATION FRONTAGE IMPROVEMENTS	1,044
LANDSCAPE AREA	43,333
PLAZA AREA	3,500
OVERALL TOTAL	185,767

PUBLIC IMPROVEMENTS

PUBLIC IMPROVEMENTS IDENTIFIED BY THE DESIGN OF THIS PROJECT INCLUDE:	
YORK ST IMPROVEMENTS	\$410,000
1" WATER MAIN IMPROVEMENTS	\$240,300
STATION FRONTAGE IMPROVEMENTS (SIDEWALK, LANDSCAPING)	\$27,500

SHEET INDEX

COVER PAGE
PUD STANDARDS PAGE
CIVIL
CB-6200 - OVERALL SITE PLAN
UB-6100 - OVERALL UTILITY PLAN
DJ-6204 - DRAINAGE BASIN MAP - PROPOSED
LANDSCAPE
LS-6101 - PLANTING PLAN - SOUTH
LS-6102 - PLANTING PLAN - NORTH
LS-6200 - PLANTING NOTES

ARCHITECTURAL
AS-6200 - PLATFORM ELEVATIONS
AS-6201 - STATION ELEVATIONS

ELECTRICAL
ES-6103 - SITE PHOTOMETRIC PLAN - SOUTH
ES-6104 - SITE PHOTOMETRIC PLAN - NORTH
ES-6213 - PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH
ES-6214 - PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER
ES-6215 - PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH
ES-6903 - LUMINAIRE SCHEDULE
ES-1000 - PEDESTRIAN LIGHT POLE DETAILS
ES-1008 - PARKING LOT LIGHT POLE DETAILS
SUPPLEMENTAL
X-100 - 112TH AVE & YORK STREET SIGNING AND STRIPING PLAN
X-101 - 112TH AVE & FOX RUN SIGNING AND STRIPING PLAN

OWNER

RTD
1920 BLAKE STREET
DENVER, CO 80202

ARCHITECT OF RECORD

IRON HORSE ARCHITECTS, INC.
475 17TH ST, #720
DENVER, CO 80202
720-855-7572

CONSTRUCTION SCHEDULE

THIS SITE WILL DEVELOP IN A LOGICAL SEQUENCE BEGINNING WITH STATION PLATFORM WORK, UNDERGROUND UTILITIES, SITE GRADING, HARDSCAPES, THEN LANDSCAPING AND FINISHES.

THE FOLLOWING MILESTONES ARE ANTICIPATED TO OVERLAP WITH A FINAL COMPLETION BY JANUARY 2018:
A) PLATFORM WORK (3-4 MONTHS): INCLUDES CONCRETE FOUNDATIONS AND UNDERGROUND ELECTRICAL SYSTEMS.
B) UNDERGROUND UTILITIES (3 MONTHS): INCLUDES PARKING LOT DRAIN PIPING, WATER MAIN EXTENSIONS, AND SEWER SERVICE CONNECTING TO SERVICE IN 112TH AVE.
C) SITE GRADING (2 MONTHS): INCLUDES BASE COURSE FOR HARDSCAPES AND GRADING OF THE WATER QUALITY POND.
D) HARDSCAPES (3 MONTHS): INCLUDES CURB AND GUTTER, ASPHALT PAVING, CONCRETE PAVING (BUS LOOP), PLAZA PAVING, AND SIDEWALKS.
E) LANDSCAPING AND FINISHES (4 MONTHS): INCLUDES IRRIGATION SYSTEMS, LANDSCAPING, SIGNS, LIGHT POLES/FITURES, PLATFORM CANOPIES, PLATFORM GUARDRAIL, AND BUS SHELTERS.

ARCHITECT'S CERTIFICATE:

I, VIRGINIA MCALLISTER, A REGISTERED PROFESSIONAL ARCHITECT, DO HEREBY CERTIFY THAT THE PLANS FOR THE P.U.D. NORTH METRO RAIL LINE 112TH AVENUE STATION WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

VIRGINIA MCALLISTER DATE

ENGINEER'S CERTIFICATE:

I, ROB PRATT, A REGISTERED PROFESSIONAL ENGINEER, DO HEREBY CERTIFY THAT THE PLANS FOR THE P.U.D. NORTH METRO RAIL LINE 112TH AVENUE STATION WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

ROB PRATT DATE

OWNER'S CERTIFICATE:

I, ASHLAND VAUGHN, AS PROJECT MANAGER OF THE NORTH METRO RAIL LINE FOR RTD, OWNER OF THE PROPERTY DESCRIBED HEREIN, KNOWN AS THE NORTHGLENN 112TH SUBDIVISION.

ASHLAND VAUGHN DATE

MAYOR ATTEST: CITY CLERK

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE MAYOR OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2018.

MAYOR

ATTEST: CITY CLERK

PLANNING COMMISSION CHAIRPERSON

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE PLANNING COMMISSION CHAIRPERSON OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2018.

PLANNING COMMISSION CHAIRPERSON

DIRECTOR OF PLANNING AND DEVELOPMENT

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE DIRECTOR OF THE DEPARTMENT OF PLANNING AND DEVELOPMENT OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2018.

PLANNING AND DEVELOPMENT, DIRECTOR

DIRECTOR PUBLIC WORKS AND UTILITIES

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE DIRECTOR OF PUBLIC WORKS AND UTILITIES OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2018.

PUBLIC WORKS AND UTILITIES, DIRECTOR

ADAMS COUNTY CLERK AND RECORDER

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY COUNTY CLERK OF THE CITY OF ADAMS COUNTY OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2018.

CLERK AND RECORDER

NOTARY:

STATE OF COLORADO
COUNTY OF ADAMS
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS

DAY OF _____, 2018 BY

MY COMMISSION EXPIRES _____ DATE

LAND USE/SITE PLAN

- “Kiss & Ride” Station
- Platform on east side of tracks
- 3 access points off York Street
 - Bus access
 - 2 vehicle access points
- Pedestrian Path
- Development standards:
 - Front: 25 feet
 - Side: 10 feet
 - Rear: 10 feet

PRELIMINARY AND FINAL DEVELOPMENT PERMIT
NORTH METRO RAIL LINE
P.U.D. 112TH AVENUE STATION, NORTHGLENN, COLORADO
PLANNED UNIT DEVELOPMENT STANDARDS

(a) Purpose. The RTD Planned Unit Development serves to provide high-quality, environmentally friendly public transit service and facilities on established corridors. The P.U.D Standards provide accessible, and convenient services capable of attracting and promoting development and investment around stations and along corridors.

(b) All regulations not specified in these Planned Unit Development (P.U.D) standards for the development shall conform to the requirements for each use or most compatible zoning districts as outlined within the City Code in effect at the time of development as determined by the Planning and Development Director. The Planning and Development Director may administratively approve a change to these P.U.D Standards if the requested change is determined by the Planning and Development Director to be a minor amendment to the P.U.D Standards.

(c) The Director of Planning and Development or designee may authorize minor changes or adjustments through an administrative amendment to the approved P.U.D in response to alterations needed when specific applications are submitted. These changes can include (but are not limited to) the reconfiguration, relocation, or reorientation of building pad sites, parking areas and common landscape areas. Minor amendments to the P.U.D Standards must meet the intent of the project.

(d) Main uses permitted.

- (1) Temporary Uses permitted only through the approval of a Temporary Use Permit (TUP).
- Seasonal sales stand
 - Food truck
- Temporary construction trailers and offices are permitted, provided that construction offices and trailers are removed from the site within 30 days of the issuance of a Certificate of Occupancy for the building to which the office is related.

(2) Transportation uses.

(3) All uses necessary for Transit operations including but not limited to:

- Transit passenger shelters and canopies
- Ticket kiosks
- Passenger platforms
- Bus stops and shelters
- Bike lockers
- Parking structures
- Pedestrian bridges or walkways
- Operations support facilities
- Driver's relief facilities
- Surface parking
- Parking Garage

(4) Utility and public service uses.

- Local utilities

(5) Commercial Mobile Radio Service facilities as allowed in the Zoning Ordinance (CMRS)

(6) Accessory uses.

- Accessory outside display
- Accessory outside sales
- Accessory outside storage
- Artwork
- Attached telecommunication antenna to existing structure
- Photovoltaic Systems (PV Power systems)

(e) Maximum height of any structure is 35' feet.

b. Building Structure Setbacks:

- Front: 25 feet
- Side: 10 feet
- Rear: 10 feet

(f) Parking

- a. Setbacks are to be a minimum 15' from collector and arterial streets.

(g) Lighting

1. An overall lighting plan shall be developed in order to coordinate with public street lighting and to establish a hierarchy of illuminated areas.
2. Site lighting shall be even across public spaces, avoiding dark or overly bright areas.
3. Lighting shall be located, oriented, and shielded so as to reduce glare for surrounding buildings, especially residential buildings.
4. Maximum height of pole lights shall not exceed 30' feet in height.
5. All fixtures require full shielding where light emissions are directed downward.

(h) Landscape Requirements

1. Minimum amount of landscaped area per lot is 15%. Plaza areas with landscape elements such as trees in grates, benches, and bike racks qualify towards the 15% landscape area requirement).
2. Along all collector and arterial streets adjacent to or within the P.U.D, there shall be a minimum 15' foot wide landscaped area measured from the curb of the street to any building or parking area. Landscaped width may include paved sidewalks or plaza areas. Along all collector and arterial street right-of-ways.
3. No more than 25 percent of the ground surface area can be exposed gravel or mulch.
4. Within all parking areas, there shall be landscaped end islands and interior islands provided to break up the large expanse of parking area.
5. Landscaping of parking islands shall be at least one tree and five shrubs for each 150 square feet of island unless utilities, traffic sight lines, and heavy anticipated pedestrian traffic make shrubs and trees impractical or as determined through the Development Permit process.
6. All land disturbed by grading shall be re-vegetated with adequate vegetative cover within one year of the disturbance. All finished grades shall emulate natural conditions with all cuts and fill slopes blended back into natural grade with smooth, rounded transitions rather than distinct, angular grade breaks.
7. A change in plant materials over more than five percent (5%) of the total landscaped area shall require submission of an amended landscape plan to the City of Northglenn Planning and Development Department, and such amended landscape plan may be administratively approved by the Director of Planning and Development. Any change over 5% is subject to Planning Commission approval.

(i) Architectural Standards

1. The architectural finish or surface plane, change must be varied along the façade of the building.
2. All buildings shall be designed with the approach of 360 degree architecture. All facades must use the same finish materials, but the percentage of each

type of material used can vary from the front façade. Architectural finishes must be evident on all facades visible from any adjoining property or street.

(j) Site Furnishings

1. Site furnishings shall be designed according to a consistent form, materials, and color. This design shall be coordinated and consistent with the station lighting, and signage.
2. Location of seating shall take into account the needs of transit users and visitors.
3. Benches and bike racks and bike lockers should be provided where appropriate to serve bus stops and other specific needs.

(k) Fences and Retaining Walls not related to transit operations

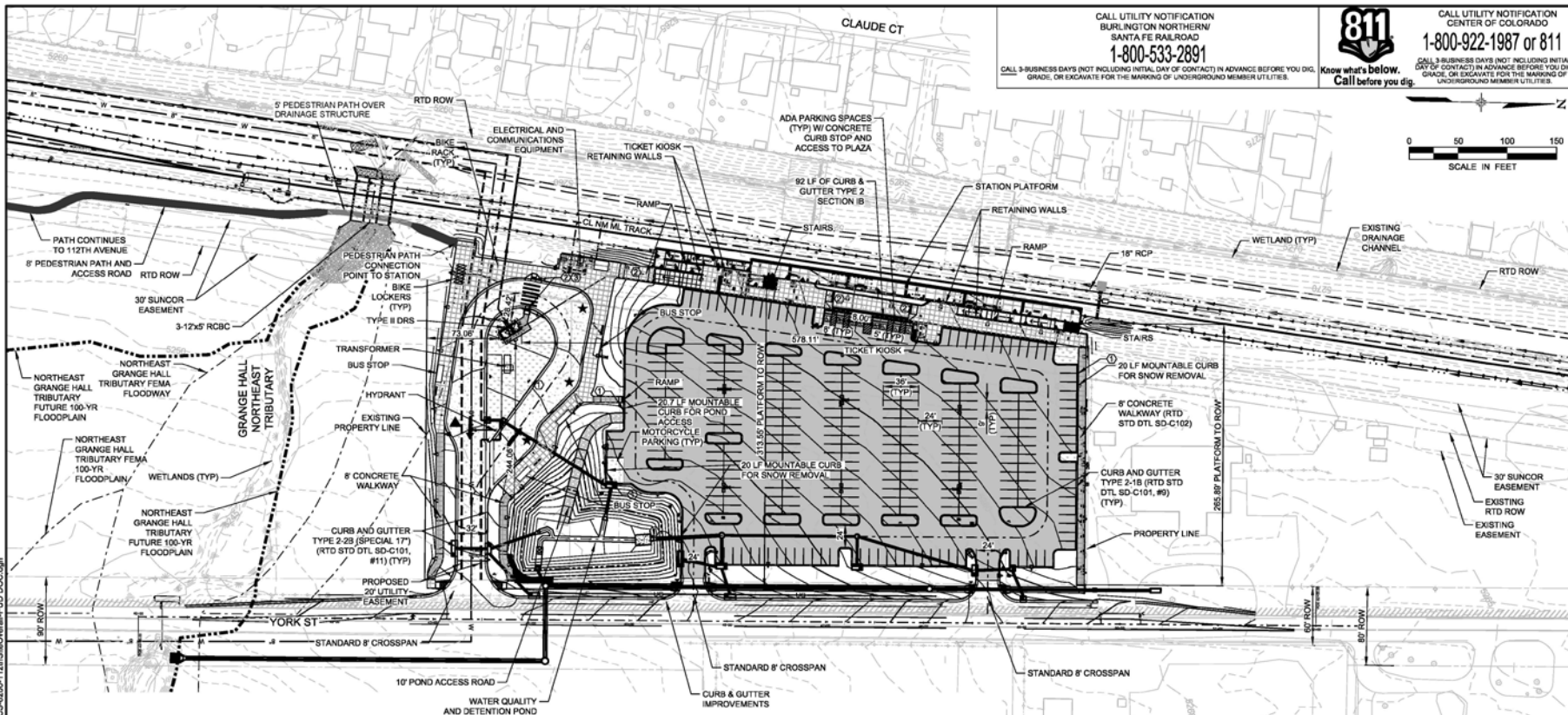
1. Fences and walls should generally contribute to the visual quality of the project or development, while being by design as unobtrusive as possible. A combination of fencing and landscaping should be incorporated wherever practicable.
2. Walls and fencing shall be constructed of materials and finishes that are compatible with, and complementary of, the adjacent building architecture. Fences and walls should be constructed from durable material such as concrete, stone, brick, metal having a dark finish, or any combination approved through the development permit process.

(l) Signs not related to transit operations

1. All other signs are subject to Chapter 21 of the City of Northglenn Municipal Code, the City of Northglenn Sign Code.

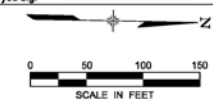
(m) General

1. Other issues, requirements or specifications not covered by these standards will be addressed by the general provisions, regulations and standards of the City of Northglenn Municipal Code in effect at the time of building permit application.



CALL UTILITY NOTIFICATION
BURLINGTON NORTHERN/
SANTA FE RAILROAD
1-800-533-2891
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG,
GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

811
Know what's below.
Call before you dig.
CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL
DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG,
GRADE, OR EXCAVATE FOR THE MARKING OF
UNDERGROUND MEMBER UTILITIES.



NOTES:

- ALL CONSTRUCTION SHALL MEET OR EXCEED THE CITY OF NORTHGLENN AND RTD STANDARDS AND SPECIFICATIONS.
- REFER TO 40 SCALE DRAWINGS FOR ADDITIONAL CONTROL.
- CONTRACTION JOINTS AT BUS LOOP PER CDOT STANDARD JOINT LAYOUT D-412.2.
- ▲ DENOTES ARTICULATED BUS BAY (RTD STD D-C121A).
- ★ DENOTES 45' BUS BAY (RTD STD D-C121B).
- PAVEMENT DEPTHS SHOWN PER DRAFT FINAL GEOTECHNICAL REPORT BY YEH AND ASSOCIATES, 12/8/14 (SEE REPORT FOR SUBGRADE RECOMMENDATIONS).
- THE CITY OF NORTHGLENN RESERVES THE RIGHT TO PERIODICALLY REVIEW THIS PUD FOR CONFORMANCE WITH ITS PROPOSED CONSTRUCTION SCHEDULE AND, WHEN APPROPRIATE, REZONE THIS PROPERTY TO A DIFFERENT ZONING CLASSIFICATION.

STATION SUMMARY

- | | |
|-----|--------------------------------------|
| 310 | PARKING SPACES REQUIRED |
| 301 | REGULAR PARKING SPACES |
| 2 | REGULAR ADA PARKING SPACES |
| 2 | VAN ACCESSIBLE ADA PARKING SPACES |
| 309 | SUBTOTAL REGULAR/ADA SPACES PROVIDED |
| 6 | MOTORCYCLE SPACES |
| 315 | TOTAL SPACES PROVIDED |
| 4 | BUS BAYS PROVIDED |

LEGEND

- | | |
|--|--|
| | CONCRETE PAVEMENT (10") |
| | ASPHALT PAVEMENT (4") |
| | STATION PLAZA PAVING (6")
(SEE ARCH PLANS) |
| | BUS LOOP SURFACING
(LANDSCAPE AND GRAVEL
PER LS-7200 SERIES) |
| | GRAVEL |

SCHEDULE

- 4' MOUNTABLE CURB WITH 1" GUTTER PAN CDOT STD M-609-1
- NEW TRUNCATED DOME/ DETECTABLE WARNING (CITY OF NORTHGLENN STD DWG R11)
- NEW CURB RAMP (CITY OF NORTHGLENN STD DWG R9)

NW-C-6200-12thStOverall-PUD.DOC.dgn

3.43.54 PM

12/13/2018

USER: jstancu

NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL, CDRL 03-036.10.06	REP	05/19/2016
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037.10.06	ALH	04/29/2015
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036.10.06	ALH	10/22/2014



DESIGNED BY: KLR DATE: 07/08/2014
DRAWN BY: KLR DATE: 07/08/2014
CHECKED BY: REP DATE: 05/11/2016
VERIFIED BY: N/A DATE: 05/11/2016
2006 SIXTH COLORADO BOULEVARD
SUITE 2-300
DENVER, CO 80202
(303) 733-6200
stantec.com

RTD FasTracks
North Metro
Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 629-6900

NORTH METRO RAIL LINE

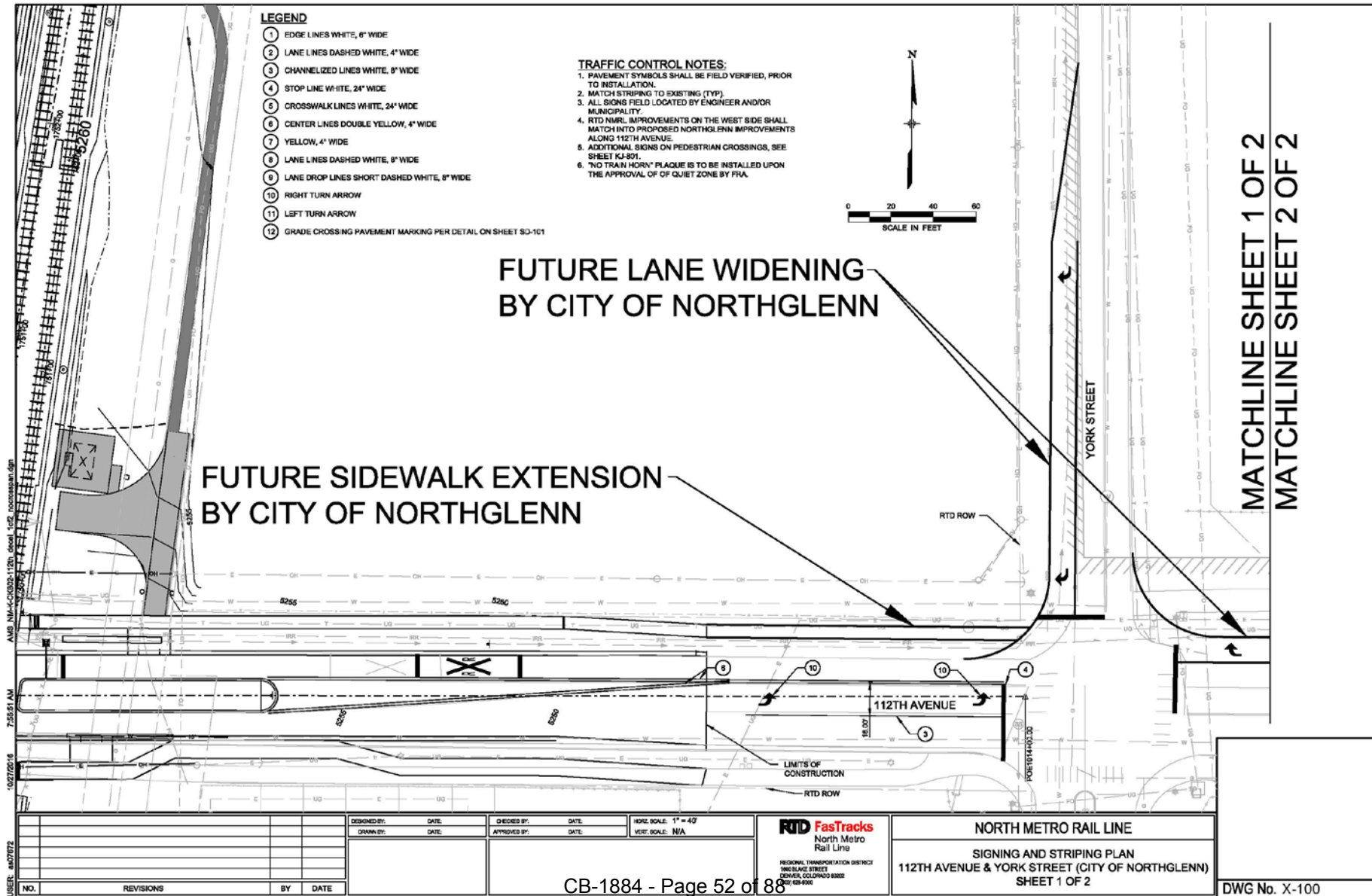
CIVIL
NORTHGLENN AT 112TH STATION
OVERALL SITE PLAN

**PRELIMINARY
NOT FOR
CONSTRUCTION**

DWG No. CS-6200

PARKING & TRAFFIC

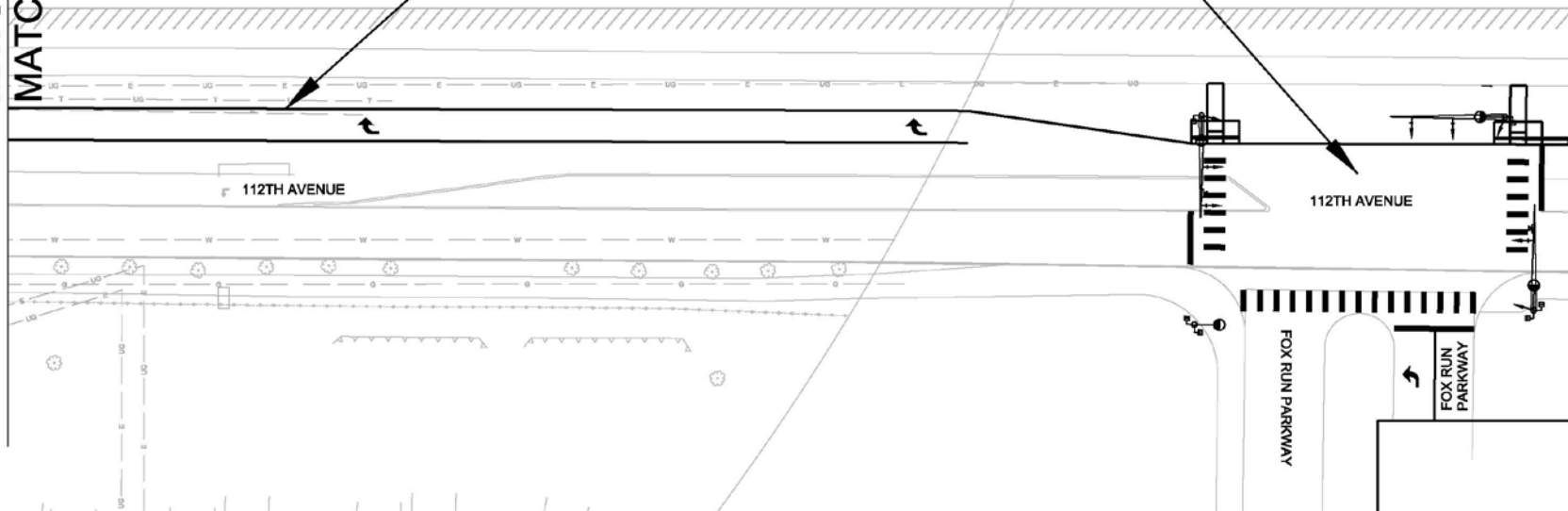
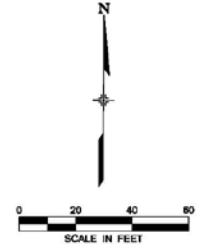
- Environmental Impact Statement (EIS)
 - Future lane widening on York Street to accommodate bus right turn movements onto 112th
 - Sidewalk extended to York on 112th Ave
 - Widening of 112th Avenue on the north side for right turn movements into York Street
 - Intersection improvements at 112th and Fox Run Pkwy
- 315 Parking Spaces provided



MATCHLINE SHEET 1 OF 2
MATCHLINE SHEET 2 OF 2

FUTURE INTERSECTION IMPROVEMENTS
BY CITY OF NORTHGLENN

FUTURE LANE WIDENING
BY CITY OF NORTHGLENN



USER: aa27672 10/26/2018 3:32:28 PM A:\B_NM-X-03020-112N.dwg, 2102.dgn

NO.	REVISIONS	BY	DATE

DESIGNED BY:	DATE:	CHECKED BY:	DATE:	WORK SCALE: 1" = 40'
DRAWN BY:	DATE:	APPROVED BY:	DATE:	VERT. SCALE: N/A

RTD FasTracks
North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1940 BLAKE STREET
DENVER, COLORADO 80202
(303) 455-4500

NORTH METRO RAIL LINE

SIGNING AND STRIPING PLAN
112TH AVENUE & YORK STREET (CITY OF NORTHGLENN)
SHEET 2 OF 2

DWG No. X-101



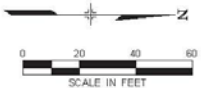
LANDSCAPING

Total
landscape
area
equates to
15%

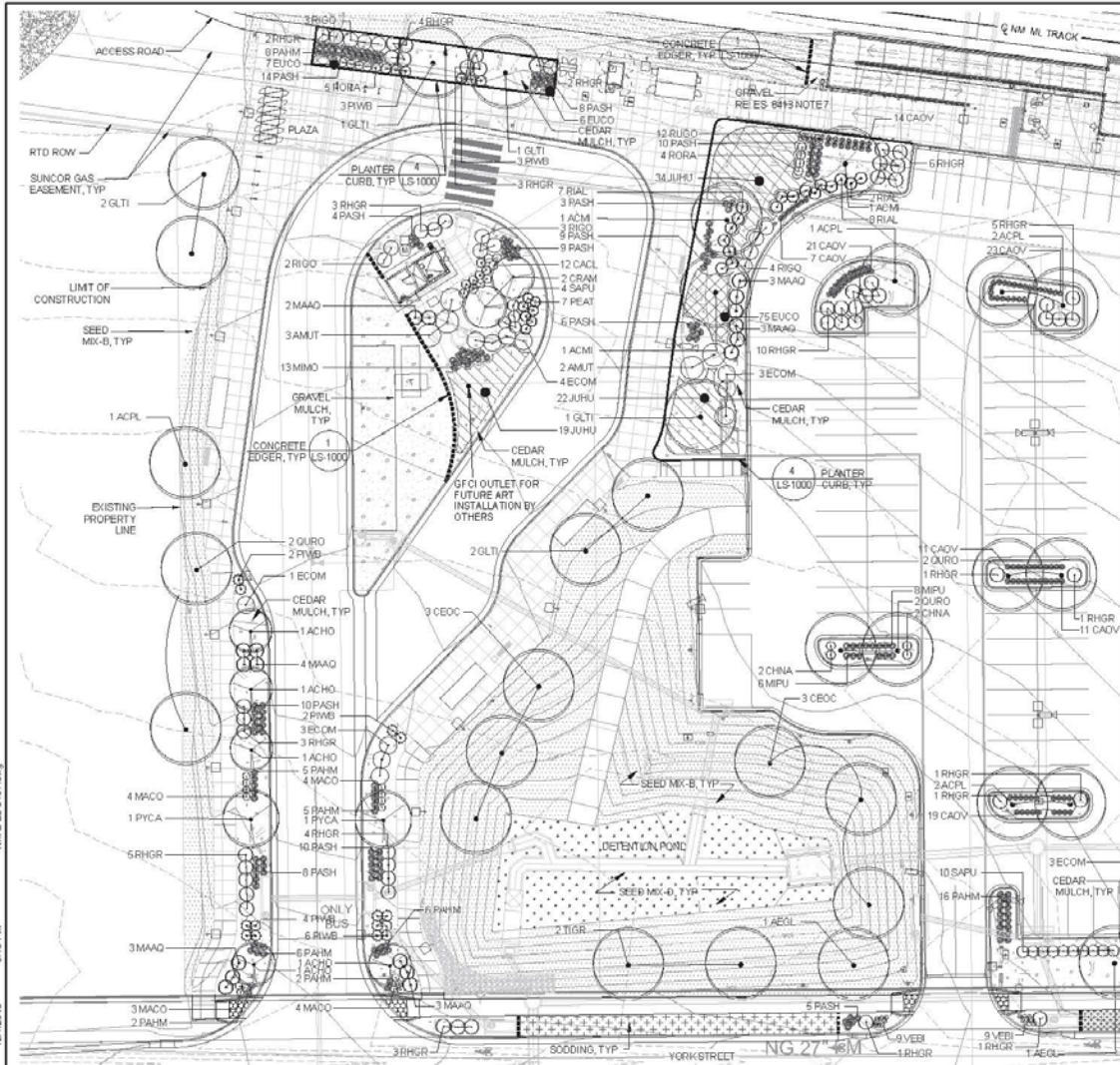


LEGEND

- DECIDUOUS TREE
- EVERGREEN TREE
- ORNAMENTAL TREE
- DECIDUOUS SHRUB
- EVERGREEN SHRUB
- ORNAMENTAL GRASS
- PERENNIALS
- GROUND COVER
- CONCRETE EDGER
- PLANTER CURB
- SOODING
- SEED MX - B
- SEED MX - D
- GRAVEL MULCH
- CEDAR MULCH



PRELIMINARY
NOT FOR
CONSTRUCTION



NNAL-LS-6101.dwg

6:19 PM

12/1/2016

USER: SherryS

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	SLB	12/1/2016

DESIGNED BY: SES
DATE: 09/01/2014
DRAWN BY: BMK
DATE: 09/16/2014

REGIONAL RAIL PARTNERS
RAILWAY TRUSTEES / GRAHAM

CHECKED BY: SES
DATE: 12/1/2016
APPROVED BY: SLB
DATE: 12/1/2016
HORIZ SCALE: 1"=40'
VERT SCALE: NA

VALERIAN
LANDSCAPE ARCHITECTURE & DESIGN
1815 GAIL TRANSPORTATION DISTRICT
100 S FLAKE STREET
DENVER, COLORADO 80202
(303) 733-9000

RTD FasTracks
North Metro
Rail Line
1815 GAIL TRANSPORTATION DISTRICT
100 S FLAKE STREET
DENVER, COLORADO 80202
(303) 733-9000

NORTH METRO RAIL LINE
LANDSCAPE
NORTHGLENN AT 112TH STATION
PLANTING PLAN - SOUTH

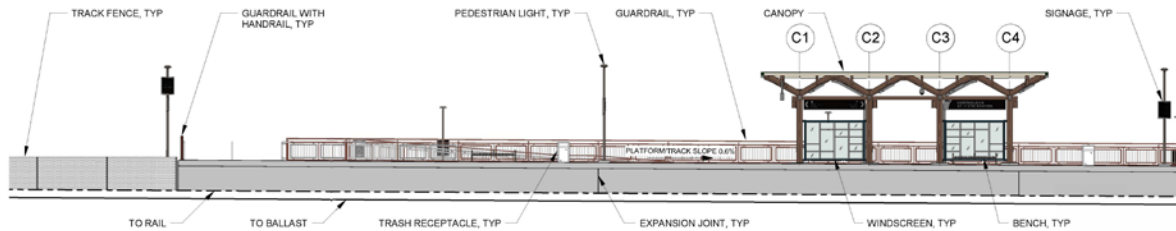
DWG No. LS-6101



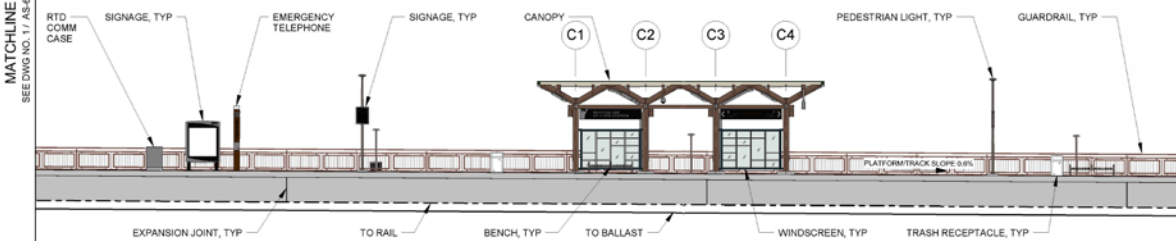
PRELIMINARY
NOT FOR
CONSTRUCTION

ARCHITECTURE

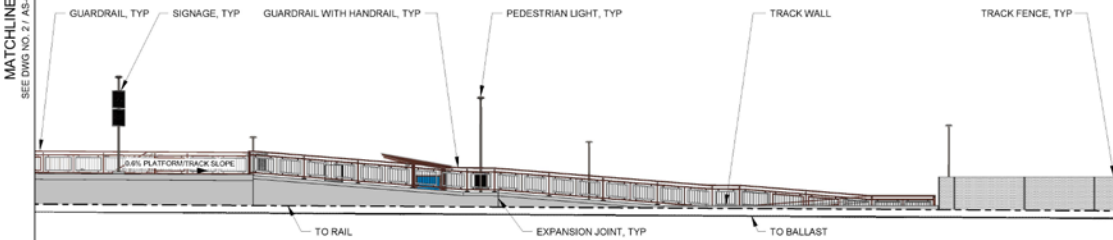
- Station theme is trees:
 - Canopy will have green tint
 - Pillars textured and painted to represent bark
- Art program for station TBD



1 PLATFORM ELEVATION - NORTH



2 PLATFORM ELEVATION - SOUTH



3 PLATFORM ELEVATION - SOUTH RAMP

MATCHLINE
SEE DWG NO. 2 / AS-6200

MATCHLINE
SEE DWG NO. 3 / AS-6200

NOTES:

1. FOR STATION SPECIFIC STRUCTURAL DETAILS, SEE DRAWING SERIES SS-6000
2. FOR STATION SPECIFIC MECHANICAL DETAILS, SEE DRAWING SERIES MS-6000
3. FOR STATION SPECIFIC PLUMBING DETAILS, SEE DRAWING SERIES PS-6000
4. FOR STATION SPECIFIC ELECTRICAL DETAILS, SEE DRAWING SERIES ES-6000
5. FOR STATION SPECIFIC CIVIL DETAILS, SEE DRAWING SERIES CS-6000
6. FOR STATION SPECIFIC GRADING DETAILS, SEE DRAWING SERIES DJ-6000
7. FOR STATION SPECIFIC UTILITY DETAILS, SEE DRAWING SERIES UI-6000
8. FOR STATION SPECIFIC LANDSCAPE DETAILS, SEE DRAWING SERIES LS-6000
9. FOR STATIONS STANDARD ARCHITECTURAL DETAILS SEE DRAWING SERIES AS-1000
10. FOR STATION STANDARD ARCHITECTURAL STRUCTURAL DETAILS SEE DRAWING SERIES SS-1000

AMENITY	MATERIAL	FINISH COLOR	REMARKS
TICKET KIOSK ROOF	GLASS	OLD CASTLE CLEAR WITH ARCTIC SNOW LAMINATE	LAMINATED, TEMPERED
ALL METAL STEEL STRUCTURE, DOWNPOUT/GUTTER FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - TERRA COTTA GRD	COATING SYSTEM B
CANOPY ALL METAL STEEL STRUCTURE, DOWNPOUT/GUTTER FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - SEE STATION SPECIFIC PLANS FOR COLOR	COATING SYSTEM B ALL EXPOSED METAL TO BE COATED, UN
VARIABLE MESSAGE SIGN MOUNTING	METAL	BLACK, RAL 9005	COATING SYSTEM B
LIGHT POLE & FIXTURE	METAL	BLACK, RAL 9005	POWDER COATED
BENCH	METAL	BLACK, RAL 9005	
PAVING PLATFORM RAMPS, STAIRS, LANDINGS, PLAZA, SIDEWALKS	CONCRETE	GRAY	MEDIUM SMOOTH FINISH
TRASH RECEPTACLE	METAL	BLACK, RAL 9005	PREFINISHED
DETECTABLE WARNING STRIP	-	FEDERAL YELLOW #303B	PER MANUFACTURER
BIKE RACK	METAL	GREEN, RAL 6009	GALVANIZED, FINISHED & POLYESTER POWDER COAT
BIKE LOCKER	FIBERGLASS	TAN	PER MANUFACTURER
RAILINGS - DECORATIVE RAILS & GUARDRAILS	METAL	TNEMEC - TERRA COTTA GRD	COATING SYSTEM C
HANDRAILS	METAL	TNEMEC - TERRA COTTA GRD	STAINLESS STEEL
HANDRAIL BRACKETS	METAL	TNEMEC - TERRA COTTA GRD	COATING SYSTEM C
GAGE PLATE	METAL	TNEMEC - TERRA COTTA GRD	COATING SYSTEM C
PEDESTRIAN BUS SHELTER	METAL	BLACK, RAL 9005	PER MANUFACTURER
CAST IN PLACE CONCRETE RETAINING WALLS AT 72ND, 112TH AND 124TH STATIONS	PAINTED CONCRETE	BROWN, FS 3040	FRACTURED FIN FORSLUENER (MIN MAX DEPTH 1-1/2" @ MAX FIN SPACING), EXTERIOR PAINT
DCS POLES AT STATION PLATFORM	STEEL	BLACK, RAL 9005	
YARD HYDRANT CASE	STEEL	STAINLESS STEEL	

CANOPY FINISHES LEGEND

ELEMENT	FINISH	COATING SYSTEM
COLUMN	TNEMEC IRONWOOD 268R	B
SPINE BEAM	TNEMEC IRONWOOD 268R	B
OUTRIGGER	TNEMEC IRONWOOD 268R	B
GLASS PANEL FRAME	TNEMEC BALSAMWOOD 154GN	B
GUTTER	TNEMEC BALSAMWOOD 154GN	B
DOWNPOUT	TNEMEC IRONWOOD 268R	B
WINDSCREEN	TNEMEC BALSAMWOOD 154GN	B
GLASS LITES	OLD CASTLE SOLIDIA WITH ARCTIC SNOW LAMINATE	-
SEALANT	CLEAR	B
ACCESS PANEL COVER	MATCH ADJACENT STEEL FINISH	B
JUNCTION BOX	TNEMEC IRONWOOD 268R	B
STEEL PLATE	TNEMEC IRONWOOD 268R	B



DESIGNED BY: MBB	DATE: 07/14/14	CHECKED BY: TBL	DATE: 09/25/2016	HORIZ SCALE: 1/16" = 1'
DRAWN BY: T.J.G.	DATE: 07/14/14	APPROVED BY: VAM	DATE: 09/25/2016	VERT SCALE: N/A
0 ISSUED FOR CONSTRUCTION	VAM	09/21/2016		
NO.	REVISIONS	BY	DATE	

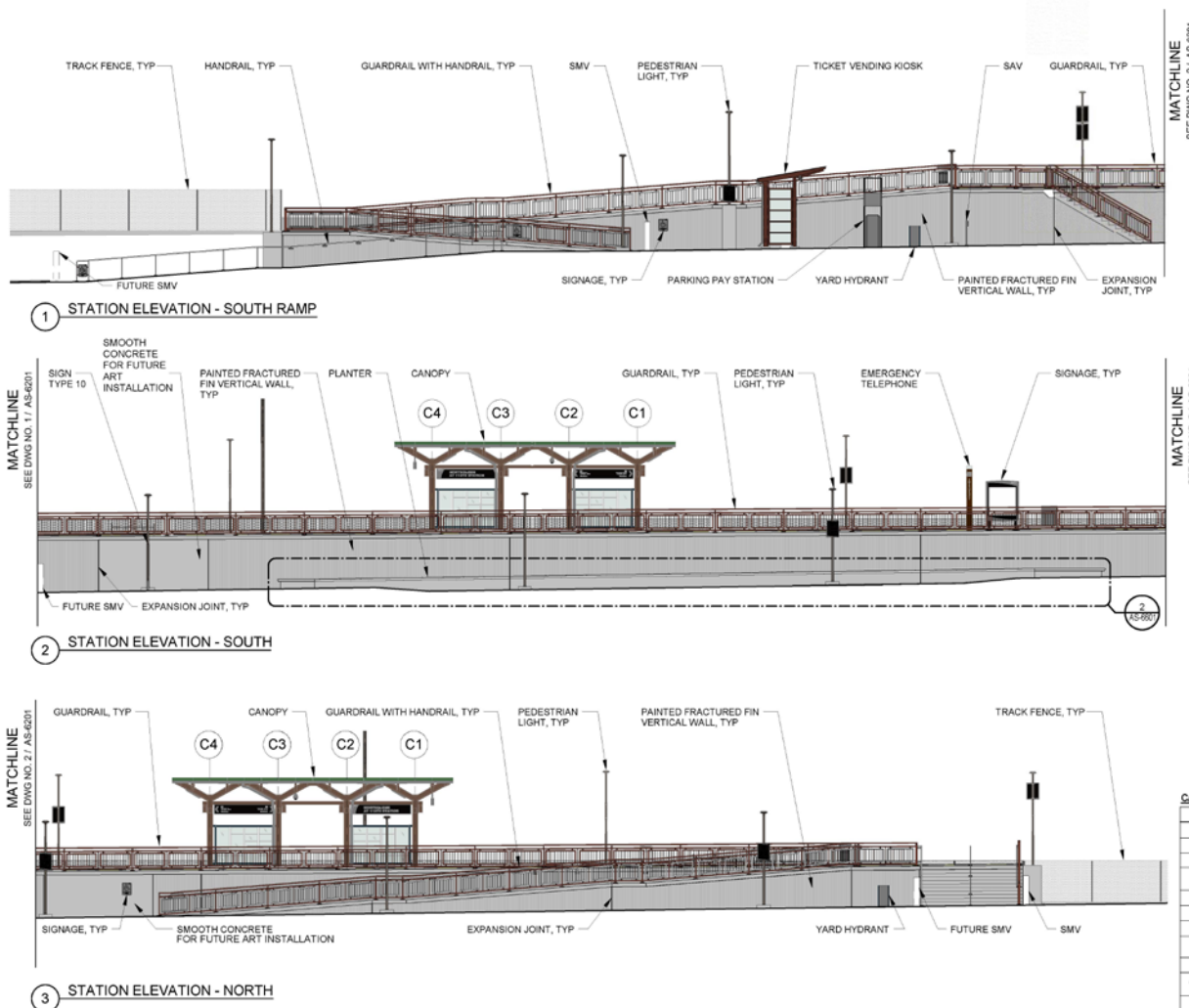


IRON HORSE ARCHITECTS
RESPONSIBLE ARCHITECT
475 17th Street Suite 720
Denver, Colorado 80202
iron-horse-architects.com
303.733.8000

RTD FastTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
100 BLADE STREET
DENVER, COLORADO 80202
303.733.8000

NORTH METRO RAIL LINE
ARCHITECTURAL
NORTHGLENN AT 112TH STATION
STATION ELEVATION - EAST PLATFORM

DWG No. AS-6200



NOTES:

1. FOR STATION SPECIFIC STRUCTURAL DETAILS, SEE DRAWING SERIES SS-6000
2. FOR STATION SPECIFIC MECHANICAL DETAILS, SEE DRAWING SERIES MS-6000
3. FOR STATION SPECIFIC PLUMBING DETAILS, SEE DRAWING SERIES PS-6000
4. FOR STATION SPECIFIC ELECTRICAL DETAILS, SEE DRAWING SERIES ES-6000
5. FOR STATION SPECIFIC CIVIL DETAILS, SEE DRAWING SERIES CS-6000
6. FOR STATION SPECIFIC GRADING DETAILS, SEE DRAWING SERIES GS-6000
7. FOR STATION SPECIFIC UTILITY DETAILS, SEE DRAWING SERIES US-6000
8. FOR STATION SPECIFIC LANDSCAPE DETAILS, SEE DRAWING SERIES LS-6000
9. FOR STATIONS STANDARD ARCHITECTURAL DETAILS SEE DRAWING SERIES AS-1000
10. FOR STATION STANDARD ARCHITECTURAL STRUCTURAL DETAILS SEE DRAWING SERIES SS-1000

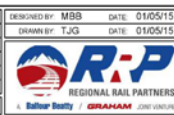
AMENITY	MATERIAL	FINISH COLOR	REMARKS
TICKET KIOSK ROOF	GLASS	OLD CASTLE CLEAR WITH ARCTIC SNOW LAMINATE	LAMINATED, TEMPERED
ALL METAL STEEL STRUCTURE, DOWNPOUT, GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	THEMEC - MONTEREY TILE	COATING SYSTEM B
CANOPY	METAL	THEMEC - SEE CANOPY FINISHES LISTED	COATING SYSTEM B ALL EXPOSED METAL TO BE COATED, UN
MESSAGE BOARD MOUNTING	METAL	BLACK, RAL 9005	COATING SYSTEM B
LIGHT POLE & FIXTURE	METAL	BLACK, RAL 9005	POWDER COATED
BENCH	METAL	BLACK, RAL 9005	
PAVING PLATFORM, RAMPS, STAIRS, LANDINGS, PLAZA, SIDEWALKS	CONCRETE	GRAY	MEDIUM SMOOTH FINISH
TRASH RECEPTACLE	METAL	BLACK, RAL 9005	PREFINISHED
DETECTABLE WARNING STRIP	-	FEDERAL YELLOW #3038	PER MANUFACTURER
BIKE RACK	METAL	GREEN, RAL 6009	GALVANIZED, PANGARD 1 POLYESTER POWDER COAT
BIKE LOCKER	FIBERGLASS	TAN	PER MANUFACTURER
RAILINGS - DECORATIVE RAILS & GUARDRAILS	METAL	THEMEC - MONTEREY TILE	COATING SYSTEM C
HANDRAILS	METAL	-	STAINLESS STEEL
HANDRAIL BRACKETS	METAL	THEMEC - MONTEREY TILE	COATING SYSTEM C
GAGE PLATE	METAL	THEMEC - MONTEREY TILE	COATING SYSTEM C
PEDESTRIAN SHELTER	METAL	BLACK, RAL 9005	PER MANUFACTURER
CAST IN PLACE CONCRETE RETAINING WALLS AT 72ND, 112TH AND 124TH STATIONS	PAINTED CONCRETE	BROWN, KS 3040	FRACTURED FIN FORSKALER (MIN MAX DEPTH 1-1.0" CC MAX FIN SPACING), EXTERIOR PAINT
OCB POLES AT STATION PLATFORM	STEEL	BLACK, RAL 9005	
YARD HYDRANT CASE	STEEL	STAINLESS STEEL	

CANOPY FINISHES LEGEND

ELEMENT	FINISH	COATING SYSTEM
COLUMN	THEMEC IRONWOOD 28BR	B
SPRUE BEAM	THEMEC IRONWOOD 28BR	B
OUTRIGGER	THEMEC IRONWOOD 28BR	B
GLASS PANEL FRAME	THEMEC BALSAMWOOD 134SN	B
GUTTER	THEMEC BALSAMWOOD 134SN	B
DOWNPOUT	THEMEC IRONWOOD 28BR	B
WINDSCREEN	THEMEC BALSAMWOOD 134SN	B
GLASS LITES	OLDCASTLE SOLENA WITH ARCTIC SNOW LAMINATE	-
REALANT	CLEAR	B
ACCESS PANEL COVER	MATCH ADJACENT STEEL FINISH	B
JUNCTION BOX	THEMEC IRONWOOD 28BR	B
STEEL PLATE	THEMEC IRONWOOD 28BR	B



DESIGNED BY: MBB	DATE: 01/05/15	CHECKED BY: TBL	DATE: 09/25/2018	HOSE SCALE: 1/16" = 1'
DRAWN BY: T.J.G.	DATE: 01/05/15	APPROVED BY: VAM	DATE: 09/25/2018	UNIT SCALE: N/A
0	ISSUED FOR CONSTRUCTION	VAM	09/21/2018	
NO.	REVISIONS	BY	DATE	



475 17th Street Suite 720
Denver, Colorado 80202
iron-horse-architects.com
773-800-8000

RTD FastTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
100 BLADE STREET
DENVER, COLORADO 80202
773-800-8000

NORTH METRO RAIL LINE
ARCHITECTURAL
NORTHGLENN AT 112TH STATION
STATION ELEVATION - EAST PLATFORM WALL

DWG No. AS-6201



UTILITIES & DRAINAGE

- Looped water line
 - City of Northglenn water
 - City main water line in City of Thornton's right-of-way
 - Required to obtain necessary permits to construct the waterline in their right-of-way
- Drainage
 - Onsite detention to mitigate impacts
 - SWFMA required



COMMISSION OPTIONS

Options:

1. Approve the requests as submitted;
2. Approve the requests with conditions/stipulations;
3. Deny this request for reasons stated;
4. Table the request for further consideration.

RECOMMENDATION

Staff recommends approval of Preliminary PUD with the following conditions and based on these Findings of Fact (Section 11-37-2).

CONDITIONS OF APPROVAL

- RTD and Northglenn will enter into an Agreement for access and on-going maintenance for the Pedestrian Trail. Said agreement shall be considered by the City Council at the time of the Public Hearing for Preliminary PUD.
- RTD will be required to enter into a Stormwater Facilities Maintenance Agreement.
- RTD/RRP will be required to pull all necessary permits prior to construction.

SPONSORED BY: MAYOR DOWNING

COUNCILMAN'S BILL

ORDINANCE NO.

No. CB-1884
Series of 2017

Series of 2017

A BILL FOR A SPECIAL ORDINANCE REZONING FROM AGRICULTURE AND C-4 COMMERCIAL, TO PLANNED UNIT DEVELOPMENT, CERTAIN REAL PROPERTY IN THE CITY OF NORTHGLENN COMMONLY KNOWN AS THE “NORTHGLENN STATION AT 112TH” IN THE COUNTY OF ADAMS, STATE OF COLORADO, AND AMENDING THE ZONING MAP OF THE CITY OF NORTHGLENN

WHEREAS, all of the property described in **Exhibit A** is currently zoned Agriculture and C-4 Commercial with a legal description as follows:

Southeast Quarter Section 2, Township 2 South, Range 68 West of the 6th P.M. City of Northglenn, County of Adams, State of Colorado

WHEREAS, the City Council desires to rezone the property described in **Exhibit A** to PUD, pursuant to the “North Metro Rail Line Preliminary and Final Planned Unit Development (PUD)” attached hereto as **Exhibit B**.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF NORTHGLENN, COLORADO, THAT:

Section 1. Findings of Fact. The City Council hereby makes the following findings of fact:

- (a) The proposed development is compatible with the surrounding area;
- (b) The proposed development is not inconsistent with the City’s Master Plan;
- (c) The proposed development does not adversely affect the health, safety, and welfare of the inhabitants of the area and the City of Northglenn;
- (d) Adequate circulation exists and traffic movement will not be impeded by the proposed development; and
- (e) Additional municipal service costs will not be incurred.

Section 2. Rezoning. Pursuant to the provisions of the Northglenn Zoning Ordinance, as amended, and upon the recommendation of the Northglenn Planning Commission (Case No. Z-2-17), a zoning change to Planned Unit Development (PUD) is hereby allowed and granted for the real property in the City of Northglenn, Colorado described in the attached **Exhibit A**, pursuant to the “Preliminary & Final PUD” attached as **Exhibit B**.

Section 3. Change of Zone Maps. The official zoning map of the City of Northglenn, pursuant to the provisions of Section 11-3-1 of the Municipal Code of the City of Northglenn, as amended, and the Comprehensive Master Plan of the City of Northglenn shall be and hereby are ordered amended to conform with the provisions of this Ordinance.

Section 4. Violations-Penalty. Violations of the provisions of this Ordinance shall be punishable under the provisions provided in Section 1-1-10 of the Northglenn Municipal Code.

INTRODUCED, READ AND ORDERED POSTED this ____ day of _____, 2017.

JOYCE DOWNING
Mayor

ATTEST:

JOHANNA SMALL, CMC
City Clerk

PASSED ON SECOND AND FINAL READING this ____ day of _____, 2017.

JOYCE DOWNING
Mayor

ATTEST:

JOHANNA SMALL, CMC
City Clerk

APPROVED AS TO FORM:

COREY Y. HOFFMANN
City Attorney

EXHIBIT A

LEGAL DESCRIPTION

Southeast Quarter Section 2, Township 2 South, Range 68 West of the 6th P.M. City of Northglenn, County of Adams, State of Colorado.

EXHIBIT B

[ATTACH PRELIMINARY PUD]

LEGAL DESCRIPTION OF PROPERTY

NORTHGLENN AT 112TH OF THE RTD NORTH METRO CORRIDOR COMMUTER RAIL PROJECT, BEING ALL OF THE TRACTS OF LAND DESCRIBED AT RECEPTION NO. 2015000064551, RECORDED ON AUGUST 6, 2015, AND RECEPTION NO. 2015000053437, RECORDED ON JULY 7, 2015, BOTH IN THE ADAMS COUNTY CLERK AND RECORDER'S OFFICE, LOCATED IN THE SOUTHEAST QUARTER OF SECTION 2, TOWNSHIP 2 SOUTH, RANGE 68 WEST OF THE 6TH PRINCIPAL MERIDIAN, CITY OF NORTHGLENN, COUNTY OF ADAMS, STATE OF COLORADO, ADDITIONALLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE EAST QUARTER CORNER OF SAID SECTION 2 (A FOUND 3-1/4" ALUMINUM CAP IN RANGE BOX STAMPED "JR ENG T2S R68W 1/4 S2 S1 1998 PLS 24308"; WHENCE THE SOUTHEAST CORNER OF SAID SECTION 2 (A FOUND 2-1/2" ALUMINUM CAP SET ON AN AXLE IN RANGE BOX STAMPED "T2S R68W S2 S1 S11 S12 2000 PLS 28286") BEARS S00°34'18"E A DISTANCE OF 2,627.20 FEET (BASIS OF BEARING - ASSUMED); THENCE N89°09'58"W, COINCIDENT WITH THE NORTH LINE OF SAID SOUTHEAST QUARTER, A DISTANCE OF 30.01 FEET TO THE WESTERLY RIGHT OF WAY LINE OF YORK STREET; THENCE S00°34'18"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 1,252.97 FEET TO THE NORTHEASTERLY CORNER OF SAID TRACT OF LAND DESCRIBED AT RECEPTION NO. 2015000064551, ALSO BEING THE POINT OF BEGINNING;

THENCE S00°34'18"E, COINCIDENT WITH SAID WESTERLY RIGHT OF WAY LINE, A DISTANCE OF 674.65 FEET TO THE SOUTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000053437; THENCE S89°26'30"W, COINCIDENT WITH SAID SOUTHERLY LINE, A DISTANCE OF 305.39 FEET TO THE EASTERLY RIGHT OF WAY LINE OF THE RTD NORTH METRO CORRIDOR AS DESCRIBED AT RECEPTION NO. 2009000046216, RECORDED JUNE 26, 2009 IN SAID CLERK AND RECORDER'S OFFICE; THENCE N07°30'24"E, COINCIDENT WITH SAID EASTERLY RIGHT OF WAY LINE, A DISTANCE OF 681.34 FEET TO THE NORTHERLY LINE OF SAID PARCEL DESCRIBED AT RECEPTION NO. 2015000064551; THENCE N89°25'42"E, COINCIDENT WITH SAID NORTHERLY LINE, A DISTANCE OF 209.65 FEET TO THE POINT OF BEGINNING.

CONTAINING 173,727 SQUARE FEET, (3.988 ACRES), MORE OR LESS.

Prepared by: *Kenneth W. Jacobs*
Kenneth W. Jacobs, P.E.
For and on behalf of Jacobs Engineering Group Inc.
707 17th Street, Suite #3400
Denver, CO 80202
303.820.5240

GENERAL NOTES

RTD IS RESPONSIBLE FOR MAINTENANCE OF COMMON AND RIGHT-OF-WAY LANDSCAPE.

ALL WATER AND SANITARY SEWER CONSTRUCTION SHALL CONFORM TO CITY OF NORTHGLENN UTILITY STANDARDS AND SPECIFICATIONS.

INSTALLATION OF ALL UTILITY TAPS SHALL BE OBSERVED BY CITY OF NORTHGLENN PERSONNEL.

CONSTRUCTION OF THE YORK STREET PUBLIC RIGHT-OF-WAY SHALL BE IN CONFORMANCE WITH CITY OF THORNTON STREET STANDARDS AND SPECIFICATIONS. ALL OTHER CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY SHALL BE IN CONFORMANCE WITH CITY OF NORTHGLENN STREET AND DRAINAGE STANDARDS AND SPECIFICATIONS.

ALL CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY SHALL MEET THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT OF 1990.

PRIOR TO ISSUANCE OF A BUILDING PERMIT, THE CITY MUST REVIEW AND APPROVE THE FOLLOWING:

1. GRADING AND DRAINAGE PLAN REPORT
2. EROSION CONTROL PLAN
3. WATER AND SANITARY SEWER UTILITY CONSTRUCTION PLAN
4. LANDSCAPE PLAN
5. IRRIGATION PLAN
6. COMPLETE CONSTRUCTION PLANS FOR ALL REQUIRED PUBLIC IMPROVEMENTS

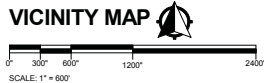
ALL EXISTING METER VAULTS, UTILITY SERVICE LINES APPURTENANCES, STREET CURB, GUTTER, SIDEWALK AND TRAIL ON AND ADJACENT TO THIS SITE SHALL BE REPAIRED AS NECESSARY AND BROUGHT TO CURRENT CITY STANDARDS.

STREET LIGHTS ON PUBLIC STREETS ADJACENT TO THIS SITE, IF NECESSARY, SHALL BE DESIGNED AND INSTALLED BY XCEL ENERGY ACCORDING TO CITY APPROVED STANDARDS AND PAID FOR BY THE DEVELOPER OR PROPERTY OWNER PRIOR TO ISSUANCE OF ANY CERTIFICATE OF OCCUPANCY.

DESCRIPTION OF PROPOSED ARCHITECTURAL USE, STYLE, AND MATERIALS

112TH STATION IS A SINGLE SIDE PLATFORM CONFIGURATION WITH A TRANSITION PLAZA AND PARK-N-RIDE INCLUDING 315 PARKING SPACES. THE ARCHITECTURE AND LANDSCAPE APPROACH INCLUDES A TREE LIKE CANOPY STRUCTURE REMINISCENT OF THE MANICURED PARKS OF THE 1960S INGRAINED IN THE SUBURBAN LIFESTYLE AND MODERN AMERICAN LIVING. THE PLAZA INCLUDES TWO LARGE PLANTERS AND A MANICURED TREE LAWN ALONG YORK STREET. THE PLANNING CONCEPT INCLUDES A PALETTE OF DIVERSE, LOW WATER SPECIES TO SUPPORT NORTHGLENN'S WATER CONSERVING MEASURES.

PRELIMINARY AND FINAL P.U.D. NORTH METRO RAIL LINE P.U.D. 112TH AVENUE STATION, NORTHGLENN, COLORADO COVER SHEET



SITE DATA TABLE

LOT SIZE	173,727 SQ FT	3.98 ACRES
BUILDING FLOOR AREA	9,500 SQ FT ALLOWABLE PER IBC	
BUILDING CONSTRUCTION TYPE	EXISTING	PROPOSED
ZONING DISTRICT	C-4 & A-1	P.U.D.
LAND USE	ALLOWED	PROPOSED
BUILDING HEIGHT	35	DRS: 1 STORY, 12' - 7"
FLOOR AREA RATIO	REQUIRED	PROPOSED
SETBACKS	NORTH 25'	578.11'
	EAST 10'	244.06'
	SOUTH 10'	73.06'
	WEST 10'	28.42'
PARKING SPACES	310	315 TOTAL SPACES
OFF-STREET PARKING	LOADING SPACES	NONE
	HANDICAP SPACES	7
LANDSCAPE AREA	15%	66,332 SQ FT 32%

LAND USE TABLE

SURFACE	AREA (SQ. FT.)
STATION PLAZA AND SIDEWALK	33,905
STATION PARKING LOT	106,957
BUS LOOP	19,274
RETENTION BASIN ACCESS ROAD	1,348
LANDSCAPE AREA	45,533
PLATFORM	5,440
DRIVER RELIEF STATION	213

PUBLIC IMPROVEMENTS

PUBLIC IMPROVEMENTS IDENTIFIED BY THE DESIGN OF THIS PROJECT INCLUDE:	
YORK ST. IMPROVEMENTS	\$410,000
8" WATER MAIN IMPROVEMENTS	\$245,200
STATION FRONTAGE IMPROVEMENTS (SIDEWALK, LANDSCAPING)	\$37,500

SHEET INDEX

COVER PAGE
PUD STANDARDS PAGE
CIVIL
CS-6200 - OVERALL SITE PLAN
UB-6100 - OVERALL UTILITY PLAN
DJ-6204 - DRAINAGE BASIN MAP - PROPOSED

LANDSCAPE
LS-6101 - PLANTING PLAN - SOUTH
LS-6102 - PLANTING PLAN - NORTH
LS-6200 - PLANTING NOTES

ARCHITECTURAL
AS-6200 - PLATFORM ELEVATIONS
AS-6201 - STATION ELEVATIONS

ELECTRICAL
ES-6103 - SITE PHOTOMETRIC PLAN - SOUTH
ES-6104 - SITE PHOTOMETRIC PLAN - NORTH
ES-6213 - PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH
ES-6214 - PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER
ES-6215 - PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH
ES-6903 - LUMINAIRE SCHEDULE
ES-1800 - PEDESTRIAN LIGHT POLE DETAILS
ES-1808 - PARKING LOT LIGHT POLE DETAILS

SUPPLEMENTAL
X-100 - 112TH AVE & YORK STREET SIGNING AND STRIPING PLAN
X-101 - 112TH AVE & FOX RUN SIGNING AND STRIPING PLAN

OWNER

RTD
1600 BLAKE STREET
DENVER, CO 80202

ARCHITECT OF RECORD

IRON HORSE ARCHITECTS, INC.
475 17TH ST, #720
DENVER, CO 80202
720-855-7572

CONSTRUCTION SCHEDULE

THIS SITE WILL DEVELOP IN A LOGICAL SEQUENCE BEGINNING WITH STATION PLATFORM WORK, UNDERGROUND UTILITIES, SITE GRADING, HARDSCAPES, THEN LANDSCAPING AND FINISHES.

THE FOLLOWING MILESTONES ARE ANTICIPATED TO OVERLAP WITH A FINAL COMPLETION BY JANUARY 2018:

- A) PLATFORM WORK (3-4 MONTHS): INCLUDES CONCRETE FOUNDATIONS AND UNDERGROUND ELECTRICAL SYSTEMS.
- B) UNDERGROUND UTILITIES (3 MONTHS): INCLUDES PARKING LOT DRAIN PIPING, WATER MAIN EXTENSIONS, AND SEWER SERVICE CONNECTING TO SERVICE IN 112TH AVE.
- C) SITE GRADING (2 MONTHS): INCLUDES BASE COURSE FOR HARDSCAPES AND GRADING OF THE WATER QUALITY POND.
- D) HARDSCAPES (3 MONTHS): INCLUDES CURB AND GUTTER, ASPHALT PAVING, CONCRETE PAVING (BUS LOOP), PLAZA PAVING, AND SIDEWALKS.
- E) LANDSCAPING AND FINISHES (4 MONTHS): INCLUDES IRRIGATION SYSTEMS, LANDSCAPING, SIGNS, LIGHT POLES/FIXTURES, PLATFORM CANOPIES, PLATFORM GUARDRAIL, AND BUS SHELTERS.

ARCHITECT'S CERTIFICATE:

I, VIRGINIA MCALLISTER, A REGISTERED PROFESSIONAL ARCHITECT, DO HEREBY CERTIFY THAT THE PLANS FOR THE P.U.D. NORTH METRO RAIL LINE 112TH AVENUE STATION WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

VIRGINIA MCALLISTER DATE

ENGINEER'S CERTIFICATE:

I, ROB PRATT, A REGISTERED PROFESSIONAL ENGINEER, DO HEREBY CERTIFY THAT THE PLANS FOR THE P.U.D. NORTH METRO RAIL LINE 112TH AVENUE STATION WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

ROB PRATT DATE

OWNER'S CERTIFICATE:

I, ASHLAND VAUGHN, AS PROJECT MANAGER OF THE NORTH METRO RAIL LINE FOR RTD, OWNER OF THE PROPERTY DESCRIBED HEREIN, KNOWN AS THE NORTHGLENN 112TH SUBDIVISION.

ASHLAND VAUGHN DATE

MAYOR ATTEST: CITY CLERK

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE MAYOR OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

MAYOR

ATTEST: CITY CLERK

PLANNING COMMISSION CHAIRPERSON

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE PLANNING COMMISSION CHAIRPERSON OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

PLANNING COMMISSION, CHAIRPERSON

DIRECTOR OF PLANNING AND DEVELOPMENT

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE DIRECTOR OF THE DEPARTMENT OF PLANNING AND DEVELOPMENT OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

PLANNING AND DEVELOPMENT, DIRECTOR

DIRECTOR PUBLIC WORKS AND UTILITIES

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BY THE DIRECTOR OF PUBLIC WORKS AND UTILITIES OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

PUBLIC WORKS AND UTILITIES, DIRECTOR

ADAMS COUNTY CLERK AND RECORDER

THIS PLANNED UNIT DEVELOPMENT (P.U.D.) HAS BEEN APPROVED BYCOUNTY CLERK OF THE CITY OF ADAMS COUNTY OF THE CITY OF NORTHGLENN, COLORADO, ON THE _____ DAY OF _____, 2016.

CLERK AND RECORDER

NOTARY:

STATE OF COLORADO
COUNTY OF ADAMS
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS _____

DAY OF _____, 2016 BY

MY COMMISSION EXPIRES _____ DATE

PRELIMINARY AND FINAL DEVELOPMENT PERMIT
NORTH METRO RAIL LINE
P.U.D. 112TH AVENUE STATION, NORTHGLENN, COLORADO
PLANNED UNIT DEVELOPMENT STANDARDS

(a) Purpose. The RTD Planned Unit Development serves to provide high-quality, environmentally friendly public transit service and facilities on established corridors. The P.U.D Standards provide accessible, and convenient services capable of attracting and promoting development and investment around stations and along corridors.

(b) All regulations not specified in these Planned Unit Development (P.U.D) standards for the development shall conform to the requirements for each use or most compatible zoning districts as outlined within the City Code in effect at the time of development as determined by the Planning and Development Director. The Planning and Development Director may administratively approve a change to these P.U.D Standards if the requested change is determined by the Planning and Development Director to be a minor amendment to the P.U.D Standards.

(c) The Director of Planning and Development or designee may authorize minor changes or adjustments through an administrative amendment to the approved P.U.D in response to alterations needed when specific applications are submitted. These changes can include (but are not limited to) the reconfiguration, relocation, or reorientation of building pad sites, parking areas and common landscape areas. Minor amendments to the P.U.D Standards must meet the intent of the project.

(d) Main uses permitted.

- (1) Temporary Uses permitted only through the approval of a Temporary Use Permit (TUP).
- Seasonal sales stand
 - Food truck
- Temporary construction trailers and offices are permitted, provided that construction offices and trailers are removed from the site within 30 days of the issuance of a Certificate of Occupancy for the building to which the office is related.

(2) Transportation uses.

(3) All uses necessary for Transit operations including but not limited to:

- Transit passenger shelters and canopies
- Ticket kiosks
- Passenger platforms
- Bus stops and shelters
- Bike lockers
- Parking structures
- Pedestrian bridges or walkways
- Operations support facilities
- Driver's relief facilities
- Surface parking
- Parking Garage

(4) Utility and public service uses.

- Local utilities

(5) Commercial Mobile Radio Service facilities as allowed in the Zoning Ordinance (CMRS)

(6) Accessory uses.

- Accessory outside display
- Accessory outside sales
- Accessory outside storage
- Artwork
- Attached telecommunication antenna to existing structure
- Photovoltaic Systems (PV Power systems)

(e) Maximum height of any structure is 35' feet.

b. Building Structure Setbacks:

- Front: 25 feet
- Side: 10 feet
- Rear: 10 feet

(f) Parking

- a. Setbacks are to be a minimum 15' from collector and arterial streets.

(g) Lighting

1. An overall lighting plan shall be developed in order to coordinate with public street lighting and to establish a hierarchy of illuminated areas.
2. Site lighting shall be even across public spaces, avoiding dark or overly bright areas.
3. Lighting shall be located, oriented, and shielded so as to reduce glare for surrounding buildings, especially residential buildings.
4. Maximum height of pole lights shall not exceed 30' feet in height.
5. All fixtures require full shielding where light emissions are directed downward.

(h) Landscape Requirements

1. Minimum amount of landscaped area per lot is 15%. Plaza areas with landscape elements such as trees in grates, benches, and bike racks qualify towards the 15% landscape area requirement).
2. Along all collector and arterial streets adjacent to or within the P.U.D, there shall be a minimum 15' foot wide landscaped area measured from the curb of the street to any building or parking area. Landscaped width may include paved sidewalks or plaza areas. Along all collector and arterial street right-of-ways.
3. No more than 25 percent of the ground surface area can be exposed gravel or mulch.
4. Within all parking areas, there shall be landscaped end islands and interior islands provided to break up the large expanse of parking area.
5. Landscaping of parking islands shall be at least one tree and five shrubs for each 150 square feet of island unless utilities, traffic sight lines, and heavy anticipated pedestrian traffic make shrubs and trees impractical or as determined through the Development Permit process.
6. All land disturbed by grading shall be re-vegetated with adequate vegetative cover within one year of the disturbance. All finished grades shall emulate natural conditions with all cuts and fill slopes blended back into natural grade with smooth, rounded transitions rather than distinct, angular grade breaks.
7. A change in plant materials over more than five percent (5%) of the total landscaped area shall require submission of an amended landscape plan to the City of Northglenn Planning and Development Department, and such amended landscape plan may be administratively approved by the Director of Planning and Development. Any change over 5% is subject to Planning Commission approval.

(i) Architectural Standards

1. The architectural finish or surface plane, change must be varied along the façade of the building.
2. All buildings shall be designed with the approach of 360 degree architecture. All facades must use the same finish materials, but the percentage of each

type of material used can vary from the front façade. Architectural finishes must be evident on all facades visible from any adjoining property or street.

(j) Site Furnishings

1. Site furnishings shall be designed according to a consistent form, materials, and color. This design shall be coordinated and consistent with the station lighting, and signage.
2. Location of seating shall take into account the needs of transit users and visitors.
3. Benches and bike racks and bike lockers should be provided where appropriate to serve bus stops and other specific needs.

(k) Fences and Retaining Walls not related to transit operations

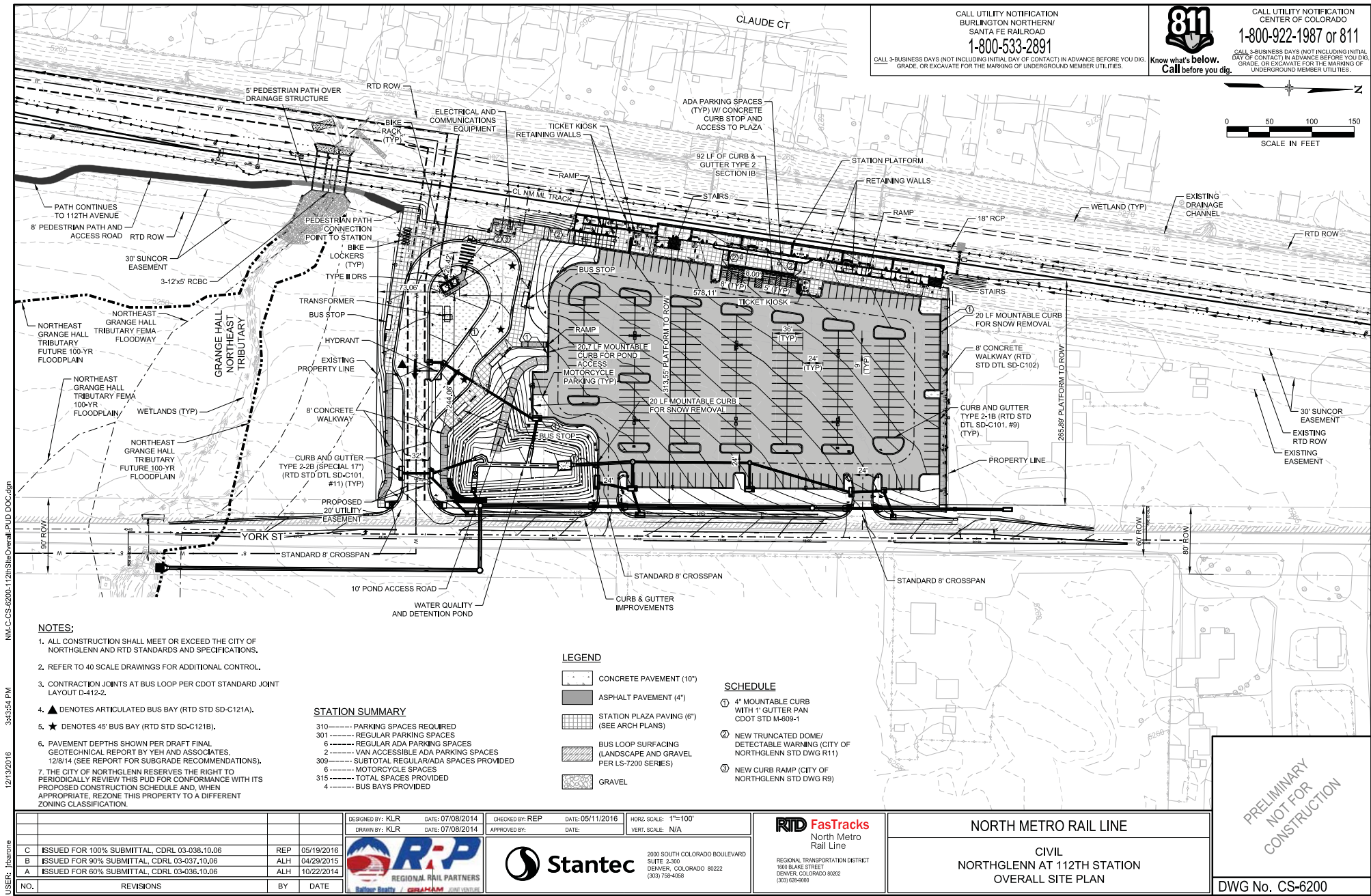
1. Fences and walls should generally contribute to the visual quality of the project or development, while being by design as unobtrusive as possible. A combination of fencing and landscaping should be incorporated wherever practicable.
2. Walls and fencing shall be constructed of materials and finishes that are compatible with, and complementary of, the adjacent building architecture. Fences and walls should be constructed from durable material such as concrete, stone, brick, metal having a dark finish, or any combination approved through the development permit process.

(l) Signs not related to transit operations

1. All other signs are subject to Chapter 21 of the City of Northglenn Municipal Code, the City of Northglenn Sign Code.

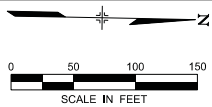
(m) General

1. Other issues, requirements or specifications not covered by these standards will be addressed by the general provisions, regulations and standards of the City of Northglenn Municipal Code in effect at the time of building permit application.



CALL UTILITY NOTIFICATION
BURLINGTON NORTHERN/
SANTA FE RAILROAD
1-800-533-2891
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG.
GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

811
Know what's below.
Call before you dig.
CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG.
GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.



- NOTES:**
1. ALL CONSTRUCTION SHALL MEET OR EXCEED THE CITY OF NORTHGLENN AND RTD STANDARDS AND SPECIFICATIONS.
 2. REFER TO 40 SCALE DRAWINGS FOR ADDITIONAL CONTROL.
 3. CONTRACTION JOINTS AT BUS LOOP PER CDOT STANDARD JOINT LAYOUT D-412-2.
 4. ▲ DENOTES ARTICULATED BUS BAY (RTD STD SD-C121A).
 5. ★ DENOTES 45' BUS BAY (RTD STD SD-C121B).
 6. PAVEMENT DEPTHS SHOWN PER DRAFT FINAL GEOTECHNICAL REPORT BY YEH AND ASSOCIATES, 12/8/14 (SEE REPORT FOR SUBGRADE RECOMMENDATIONS).
 7. THE CITY OF NORTHGLENN RESERVES THE RIGHT TO PERIODICALLY REVIEW THIS PUD FOR CONFORMANCE WITH ITS PROPOSED CONSTRUCTION SCHEDULE AND, WHEN APPROPRIATE, REZONE THIS PROPERTY TO A DIFFERENT ZONING CLASSIFICATION.

STATION SUMMARY

- 310 ----- PARKING SPACES REQUIRED
- 301 ----- REGULAR PARKING SPACES
- 6 ----- REGULAR ADA PARKING SPACES
- 2 ----- VAN ACCESSIBLE ADA PARKING SPACES
- 309 ----- SUBTOTAL REGULAR/ADA SPACES PROVIDED
- 6 ----- MOTORCYCLE SPACES
- 315 ----- TOTAL SPACES PROVIDED
- 4 ----- BUS BAYS PROVIDED

LEGEND

- CONCRETE PAVEMENT (10")
- ASPHALT PAVEMENT (4")
- STATION PLAZA PAVING (6") (SEE ARCH PLANS)
- BUS LOOP SURFACING (LANDSCAPE AND GRAVEL PER LS-7200 SERIES)
- GRAVEL

SCHEDULE

- ① 4" MOUNTABLE CURB WITH 1" GUTTER PAN CDOT STD M-609-1
- ② NEW TRUNCATED DOME/ DETECTABLE WARNING (CITY OF NORTHGLENN STD DWG R11)
- ③ NEW CURB RAMP (CITY OF NORTHGLENN STD DWG R9)

NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038.10.06	REP	05/19/2016
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037.10.06	ALH	04/29/2015
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036.10.06	ALH	10/22/2014

DESIGNED BY: KLR DATE: 07/08/2014
DRAWN BY: KLR DATE: 07/08/2014

CHECKED BY: REP DATE: 05/11/2016
APPROVED BY: DATE:

2000 SOUTH COLORADO BOULEVARD
SUITE 2-300
DENVER, COLORADO 80222
(303) 758-0055

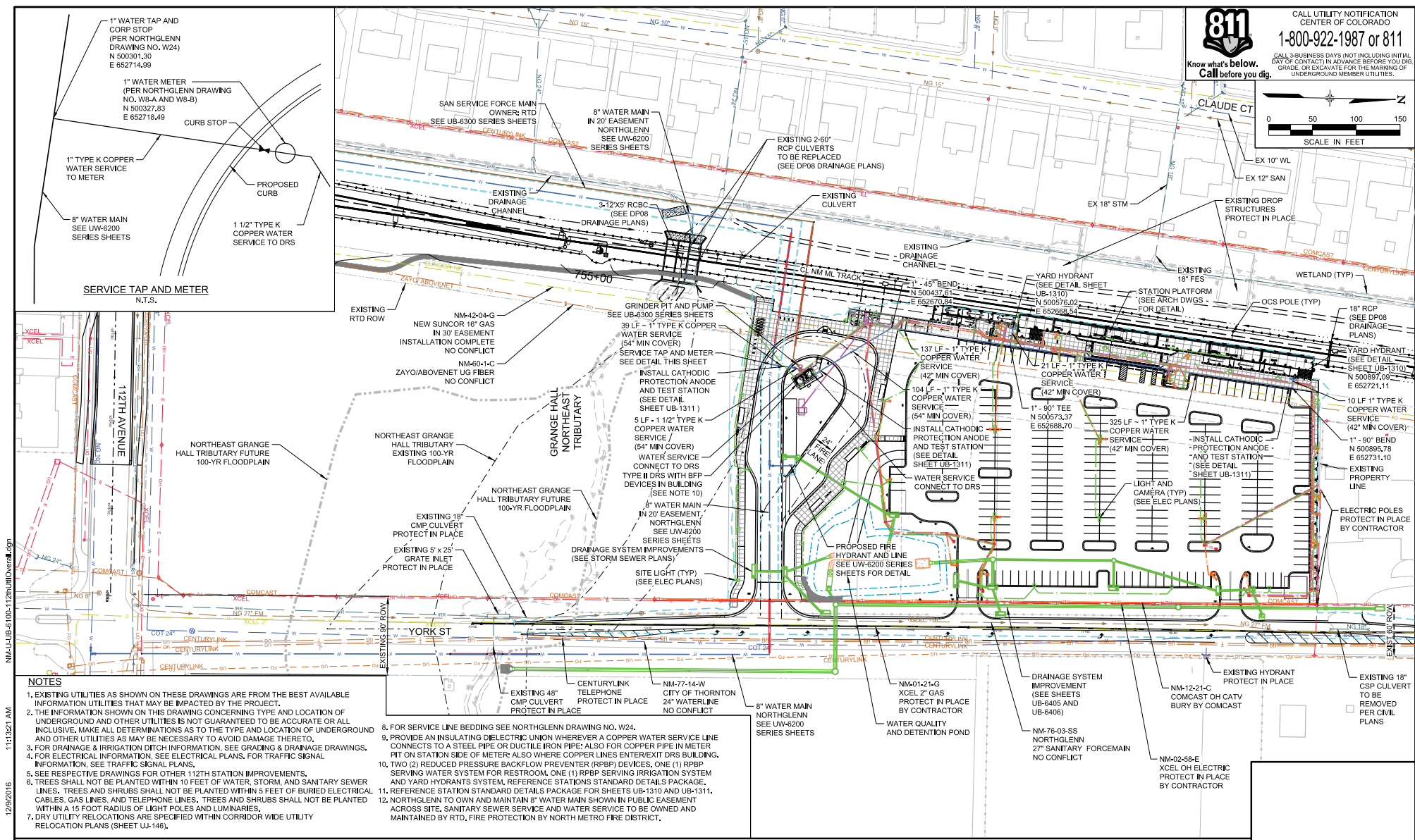
RTD FastTracks
North Metro
Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLAINE STREET
DENVER, COLORADO 80202
(303) 628-6000


NORTH METRO RAIL LINE

CIVIL
NORTHGLENN AT 112TH STATION
OVERALL SITE PLAN

PRELIMINARY
NOT FOR
CONSTRUCTION

DWG No. CS-6200





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UNDERGROUND MEMBER UTILITIES.

Know what's below.
Call before you dig.

CLAUDE CT

EX 10" WL

EX 12" SAN

EX 18" STM

EX 18" FES

STATION PLATFORM
(SEE ARCH DWGS -
FOR DETAIL)

OCS POLE (TYP)

18" RCP
(SEE DP08
DRAINAGE
PLANS)

YARD HYDRANT
(SEE DETAIL
SHEET UB-1310)
N 500897.09
E 652721.11

10 LF 1" TYPE K
COPPER WATER
SERVICE
(42" MIN COVER)

1" - 90° BEND
N 500895.78
E 652731.10

EXISTING
PROPERTY
LINE

ELECTRIC POLES
PROTECT IN PLACE
BY CONTRACTOR

SCALE IN FEET


0 50 100 150

- NOTES**
1. EXISTING UTILITIES AS SHOWN ON THESE DRAWINGS ARE FROM THE BEST AVAILABLE INFORMATION UTILITIES THAT MAY BE IMPACTED BY THE PROJECT.
 2. THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. MAKE ALL DETERMINATIONS AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.
 3. FOR DRAINAGE & IRRIGATION DITCH INFORMATION, SEE GRADING & DRAINAGE DRAWINGS.
 4. FOR ELECTRICAL INFORMATION, SEE ELECTRICAL PLANS, FOR TRAFFIC SIGNAL INFORMATION, SEE TRAFFIC SIGNAL PLANS.
 5. SEE RESPECTIVE DRAWINGS FOR OTHER 112TH STATION IMPROVEMENTS.
 6. TREES SHALL NOT BE PLANTED WITHIN 10 FEET OF WATER, STORM, AND SANITARY SEWER LINES. TREES AND SHRUBS SHALL NOT BE PLANTED WITHIN 5 FEET OF BURIED ELECTRICAL CABLES, GAS LINES, AND TELEPHONE LINES. TREES AND SHRUBS SHALL NOT BE PLANTED WITHIN A 15 FOOT RADIUS OF LIGHT POLES AND LUMINAIRES.
 7. DRY UTILITY RELOCATIONS ARE SPECIFIED WITHIN CORRIDOR WIDE UTILITY RELOCATION PLANS (SHEET UJ-146).
 8. FOR SERVICE LINE BEDDING SEE NORTHGLENN DRAWING NO. W24.
 9. PROVIDE AN INSULATING DIELECTRIC UNION WHEREVER A COPPER WATER SERVICE LINE CONNECTS TO A STEEL PIPE OR DUCTILE IRON PIPE; ALSO FOR COPPER PIPE IN METER PIT ON STATION SIDE OF METER; ALSO WHERE COPPER LINES ENTER/EXIT DRS BUILDING.
 10. TWO (2) REDUCED PRESSURE BACKFLOW PREVENTER (RPBP) DEVICES, ONE (1) RPBP SERVING WATER SYSTEM FOR RESTROOM, ONE (1) RPBP SERVING IRRIGATION SYSTEM AND YARD HYDRANTS SYSTEM, REFERENCE STATIONS STANDARD DETAILS PACKAGE.
 11. REFERENCE STATION STANDARD DETAILS PACKAGE FOR SHEETS UB-1310 AND UB-1311.
 12. NORTHGLENN TO OWN AND MAINTAIN 8" WATER MAIN SHOWN IN PUBLIC EASEMENT ACROSS SITE. SANITARY SEWER SERVICE AND WATER SERVICE TO BE OWNED AND MAINTAINED BY RTD, FIRE PROTECTION BY NORTH METRO FIRE DISTRICT.


DESIGNED BY: BEP DATE: 11/16/2015
DRAWN BY: VRE DATE: 11/16/2015

CHECKED BY: CRH DATE: 11/30/2016
APPROVED BY: BEP DATE: 12/01/2016


HORIZ. SCALE: 1"=100'
VERT. SCALE: N/A



REGIONAL RAIL PARTNERS



2000 SOUTH COLORADO BOULEVARD
SUITE 2-300
DENVER, COLORADO 80222
(303) 758-4055



North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BLANK STREET
DENVER, COLORADO 80202
(303) 628-0000

NORTH METRO RAIL LINE

CIVIL

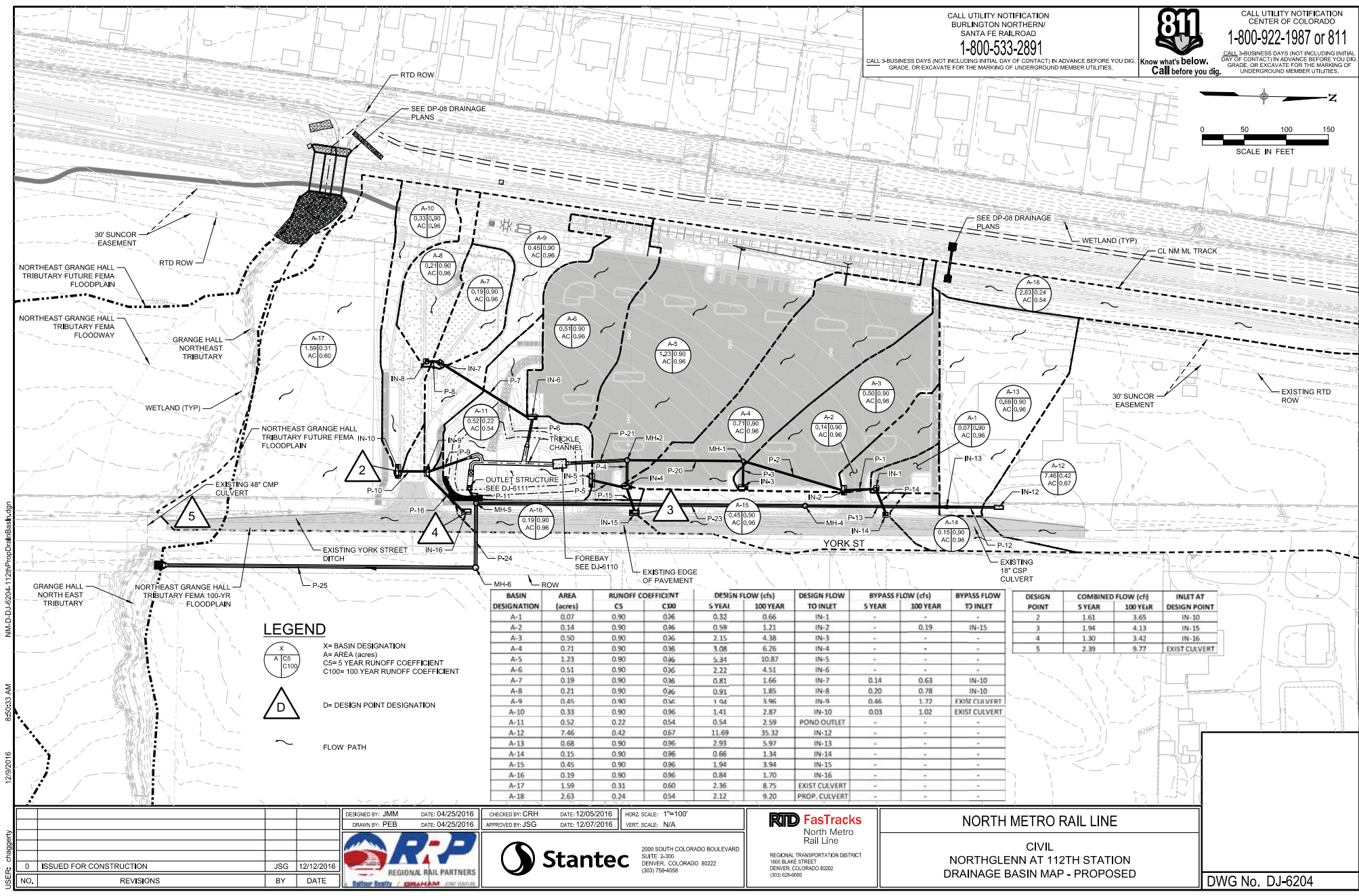
NORTHGLENN AT 112TH STATION

OVERALL UTILITY PLAN

DWG No. UB-6100

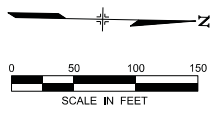
NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	BEP	12/12/2016

USER: JohnBarone
11/15/2016 12:02:06 PM
NML-UB-6100-112thUtilOverall.dgn



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BURLINGTON NORTHERN/
SANTA FE RAILROAD
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CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG,
GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.
Know what's below. Call before you dig.



LEGEND

- X = BASIN DESIGNATION
- A = AREA (acres)
- C5 = 5 YEAR RUNOFF COEFFICIENT
- C100 = 100 YEAR RUNOFF COEFFICIENT
- D = DESIGN POINT DESIGNATION
- ~ FLOW PATH

BASIN DESIGNATION	AREA (acres)	RUNOFF COEFFICIENT C5	RUNOFF COEFFICIENT C100	DESIGN FLOW (cfs) 5 YEAR	DESIGN FLOW (cfs) 100 YEAR	DESIGN FLOW TO INLET	BYPASS FLOW (cfs) 5 YEAR	BYPASS FLOW (cfs) 100 YEAR	BYPASS FLOW TO INLET
A-1	0.07	0.90	0.96	0.32	0.66	IN-1	-	-	-
A-2	0.14	0.90	0.96	0.59	1.21	IN-2	-	0.19	IN-15
A-3	0.50	0.90	0.96	2.15	4.38	IN-3	-	-	-
A-4	0.71	0.90	0.96	3.08	6.26	IN-4	-	-	-
A-5	1.23	0.90	0.96	5.34	10.87	IN-5	-	-	-
A-6	0.51	0.90	0.96	2.22	4.51	IN-6	-	-	-
A-7	0.19	0.90	0.96	0.81	1.66	IN-7	0.14	0.63	IN-10
A-8	0.21	0.90	0.96	0.91	1.85	IN-8	0.20	0.78	IN-10
A-9	0.45	0.90	0.96	1.04	3.96	IN-9	0.46	1.72	EXIST CULVERT
A-10	0.33	0.90	0.96	1.41	2.87	IN-10	0.03	1.02	EXIST CULVERT
A-11	0.52	0.22	0.54	0.54	2.59	POND OUTLET	-	-	-
A-12	7.46	0.42	0.67	11.69	35.32	IN-12	-	-	-
A-13	0.68	0.90	0.96	2.93	5.97	IN-13	-	-	-
A-14	0.15	0.90	0.96	0.66	1.34	IN-14	-	-	-
A-15	0.45	0.90	0.96	1.94	3.94	IN-15	-	-	-
A-16	0.19	0.90	0.96	0.84	1.70	IN-16	-	-	-
A-17	1.59	0.31	0.50	2.36	8.75	EXIST CULVERT	-	-	-
A-18	2.63	0.24	0.54	2.12	9.20	PROP. CULVERT	-	-	-

DESIGN POINT	COMBINED FLOW (cfs) 5 YEAR	COMBINED FLOW (cfs) 100 YEAR	INLET AT DESIGN POINT
2	1.61	3.65	IN-10
3	1.94	4.13	IN-15
4	1.30	3.42	IN-16
5	2.39	9.77	EXIST CULVERT

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	JSG	12/12/2016

DESIGNED BY: JMM DATE: 04/25/2016
DRAWN BY: PEB DATE: 04/25/2016
R-P
REGIONAL RAIL PARTNERS
Burlington Northern Santa Fe

CHECKED BY: CRH DATE: 12/05/2016
APPROVED BY: JSG DATE: 12/07/2016
Stantec
2000 SOUTH COLORADO BOULEVARD
SUITE 2-300
DENVER, COLORADO 80222
(303) 758-4055

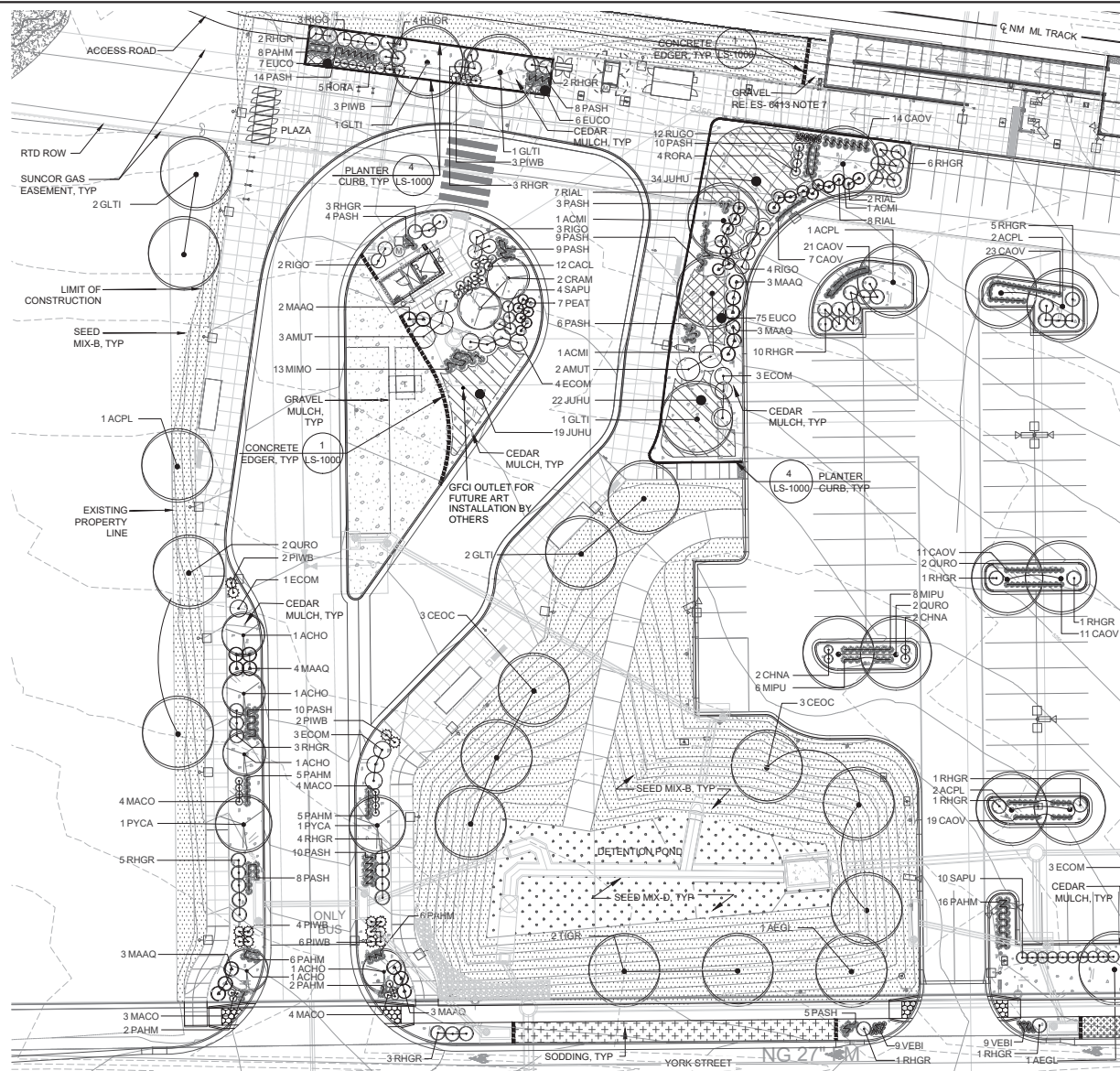
RTD FastTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLAINE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE
CIVIL
NORTHGLENN AT 112TH STATION
DRAINAGE BASIN MAP - PROPOSED

DWG No. DJ-6204

USER: chappery 12/29/2016 8:50:33 AM N:\CD\DJ-6204-112thPropDrainBasin.dgn

USER: StaceyS 12/12/2016 6:18 PM NML-LS-6101.dwg



MATCHLINE
SEE DWG NO. LS-6102

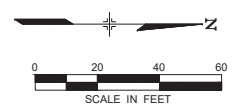


CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987 or 811
CALL 3-BUSINESS DAYS (NOT INCLUDING INITIAL DAY OF CONTACT) IN ADVANCE BEFORE YOU DIG.
Know what's Below. Call before you dig.

GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

LEGEND

- DECIDUOUS TREE
- EVERGREEN TREE
- ORNAMENTAL TREE
- DECIDUOUS SHRUB
- EVERGREEN SHRUB
- ORNAMENTAL GRASS
- PERENNIALS
- GROUNDCOVER
- CONCRETE EDGER
- PLANTER CURB
- SODDING
- SEED MIX - B
- SEED MIX - D
- GRAVEL MULCH
- CEDAR MULCH



PRELIMINARY
NOT FOR
CONSTRUCTION

DWG No. LS-6101

REVISIONS				BY	DATE	DESIGNED BY: SES	DATE: 09/01/2014	CHECKED BY: SES	DATE: 12/12/2016	HORIZ. SCALE: 1"=40'
						DRAWN BY: BMK	DATE: 09/16/2014	APPROVED BY: SLB	DATE: 12/12/2016	VERT. SCALE: NA
						 REGIONAL RAIL PARTNERS				
0	ISSUED FOR CONSTRUCTION			SLB	12/12/2016	 VALERIAN Landscape Architecture Urban Design Regain Design Planning				


















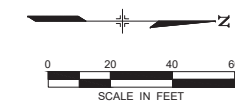
FastTracks
North Metro
Rail Line

REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE

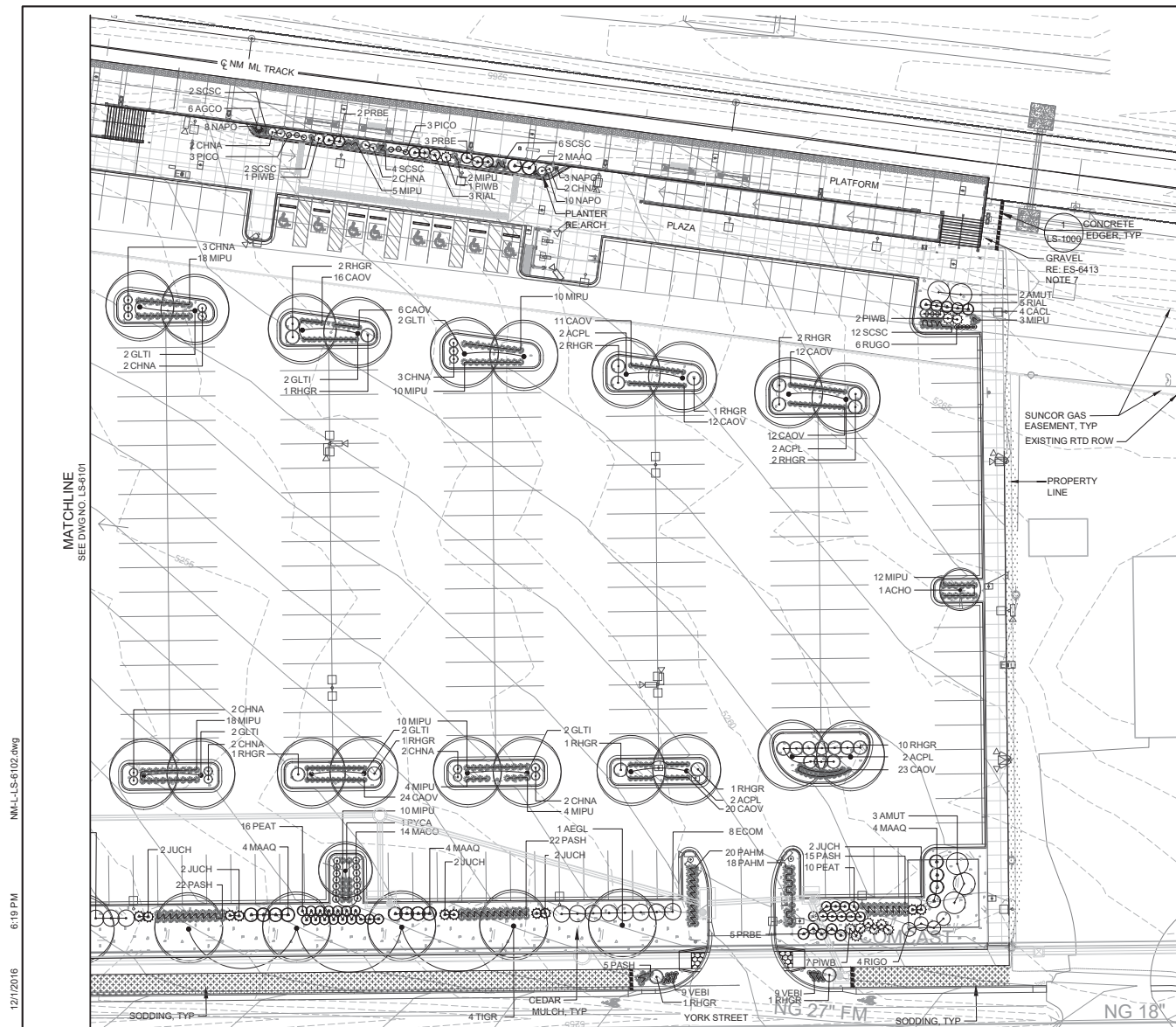
LANDSCAPE
NORTHGLENN AT 112TH STATION
PLANTING PLAN - SOUTH



- | | |
|---|------------------|
|  | DECIDUOUS TREE |
|  | EVERGREEN TREE |
|  | ORNAMENTAL TREE |
|  | DECIDUOUS SHRUB |
|  | EVERGREEN SHRUB |
|  | ORNAMENTAL GRASS |
|  | PERENNIALS |
|  | GROUNDCOVER |
|  | CONCRETE EDGER |
|  | PLANTER CURB |
|  | SODDING |
|  | SEED MIX - B |
|  | SEED MIX - D |
|  | GRAVEL MULCH |
|  | CEDAR MULCH |



PRELIMINARY
NOT FOR
CONSTRUCTION

DWG No. LS-6102



NO.		REVISIONS		BY	DATE	DESIGNED BY: SES DATE: 09/01/2014	CHECKED BY: SES DATE: 12/12/2016	HORIZ. SCALE: 1"=40'
						DRAWN BY: BMK DATE: 09/16/2014	APPROVED BY: SLB DATE: 12/12/2016	VERT. SCALE: NA
						  VALERIAN Landscape Architecture Urban Design Intelligent Design Planning		
0	ISSUED FOR CONSTRUCTION				SLB	12/12/2016		

REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE

LANDSCAPE

NORTHGLENN AT 112TH STATION

PLANTING PLAN - NORTH

PLANT SCHEDULE

DECIDUOUS TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
ACMI	3	ACER MIYABEI 'STATE STREET'	STATE STREET MAPLE	B & B	2" CAL
ACPL	14	ACER PLATANOIDES 'EMERALD QUEEN'	EMERALD QUEEN MAPLE	B & B	2" CAL
AEGL	3	AESCULUS GLABRA	OHIO BUCKEYE	B & B	2" CAL
CEOC	6	CELTIS OCCIDENTALIS	COMMON HACKBERRY	B & B	2" CAL
GLTI	19	GLEDITSIA TRIACANTHOS INERMIS 'SKYLINE'	SKYLINE COMMON HONEYLOCUST	B & B	2" CAL
QURO	6	QUERCUS ROBUR	ENGLISH OAK	B & B	2" CAL
TIGR	6	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LITTLELEAF LINDEN	B & B	2" CAL

EVERGREEN TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
PICO	6	PICEA GLAUCA 'CONICA'	DWARF ALBERTA SPRUCE	15 GAL	
PSST	1	PINUS STROBIFORMIS	SOUTHWESTERN WHITE PINE	B & B	6' HT
ORNAMENTAL TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
ACHO	6	ACER TATARICUM 'HOT WINGS'	HOT WINGS TATARIAN MAPLE	B & B	6' CLUMP
CRAM	2	CRATAEGUS AMBIGUA	RUSSIAN HAWTHORN	B & B	6' CLUMP
PYCA	3	PYRUS CALLERYANA 'ARISTOCRAT'	ARISTOCRAT FLOWERING PEAR	B & B	2" CAL

DECIDUOUS SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
AMUT	10	AMELANCHIER UTAHENSIS	UTAH SERVICEBERRY	5 GAL	
CACL	16	CARYOPTERIS X CLANDONENSIS 'BLUE MIST'	BLUE MIST SHRUB	5 GAL	
CHNA	26	CHRYSOETHAMNUS NAUSEOSUS NAUSEOSUS	DWARF BLUE RABBITBRUSH	5 GAL	
ECOM	22	EUONYMUS ALATUS 'COMPACTUS'	COMPACT BURNING BUSH	5 GAL	
MAAQ	32	MAHONIA AQUIFOLIUM	OREGON GRAPE	5 GAL	
MACO	29	MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE	5 GAL	
PEAT	33	PEROVSKIA ATRIPLICIFOLIA	RUSSIAN SAGE	5 GAL	
PRBE	10	PRUNUS BESSEYI 'PAWNEE BUTTES'	SAND CHERRY	5 GAL	
RHOR	82	RHUS AROMATICA 'GRO-LOW'	GRO-LOW FRAGRANT SUMAC	5 GAL	
RIAL	25	RIBES ALPINUM	ALPINE CURRANT	5 GAL	
RIGO	16	RIBES AUREUM	GOLDEN CURRANT	5 GAL	
RORA	9	ROSA X 'RADCON'	RADCON KNOCKOUT ROSE	5 GAL	
SAFU	14	SALIX PURPUREA 'NANA'	DWARF ARCTIC WILLOW	5 GAL	


EVERGREEN SHRUB	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
JUCH	10	JUNIPERUS CHINENSIS 'ARMSTRONGII'	ARMSTRONG JUNIPER	5 GAL	
JUCO	1	JUNIPERUS SCOPULORUM 'COLOGREEN'	COLOGREEN JUNIPER	15 GAL	
PIMU	1	PINUS MUGO 'MOPS'	MOPS MUGO PINE	5 GAL	
PWIB	31	PINUS MUGO 'WHITEBUD'	MUGO PINE	5 GAL	

ORNAMENTAL GRASSES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
CAOV	242	CALAMAGROSTIS X ACUTIFLORA 'OVERDAM'	OVERDAM FEATHER REED GRASS	1 GAL	
MIMO	13	MISCANTHUS SINENSIS 'MORNING LIGHT'	EULALIA GRASS	1 GAL	
MIPU	120	MISCANTHUS SINENSIS 'PURPURESCENS'	FLAME GRASS	1 GAL	
NAPO	21	NASSELLA TENUISSIMA 'PONY TAILS'	MEXICAN FEATHERGRASS	1 GAL	
PAHM	88	PANICUM VIRGATUM 'HEAVY METAL'	BLUE SWITCH GRASS	1 GAL	
PASH	160	PANICUM VIRGATUM 'SHENENDOAH'	BURGUNDY SWITCH GRASS	1 GAL	
SCSC	26	SCHIZACHYRIUM SCOPARIUM 'THE BLUES'	THE BLUES LITTLE BLUESTEM	1 GAL	

PERENNIALS	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL
AGCO	6	AGASTACHE X 'CORANADO RED'	ANISE HYSSOP	1 GAL	
RUGO	18	RUDEBECKIA FULGIDA 'GOLDSTURM'	GOLDSTURM BLACK EYED SUSAN	1 GAL	
VEBI	36	VERBENA BIPINNATIFIDA	NATIVE VERBENA	1 GAL	

GROUNDCOVERS	QTY	BOTANICAL NAME	COMMON NAME	CONT @	CAL
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
 - EUCO	88	EUONYMUS COLORATUS	PURPLE WINTERCREEPER	1 GAL	
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 - JUHU	75	JUNIPERUS HORIZONTALIS 'HUGHES'	HUGHES JUNIPER	5 GAL	
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SODDING/SEEDING	QTY	BOTANICAL NAME	COMMON NAME	CONT @	CAL
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	22,989 SF	SEED MIX - B	NATIVE SEED MIX	SEED	
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	3,013 SF	SEED MIX - D	WET/DRY SEED MIX	SEED	
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	2,027 SF	SODDING	TEXAS HYBRID BLUEGRASS SOD	SOD	
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LANDSCAPE NOTES

- SOIL:**
- SOIL PREPARATION FOR LANDSCAPE AREAS TO RECEIVE SOD, TREES, SHRUBS AND ALL PLANTING BEDS SHALL INCLUDE COMPOST ADDED AT A RATE OF FOUR (4) CUBIC YARDS PER ONE THOUSAND SQUARE FEET AND TILLED SIX INCHES INTO THE SOIL.
 - SOIL PREPARATION FOR LANDSCAPE AREAS TO RECEIVE NATIVE SEED SHALL BE AMENDED WITH 1,000 LBS /ACRE BIOSOL. ALL LANDSCAPE AREAS SHALL RECEIVE 4" TOPSOIL AND SOIL PREPARATION AS SPECIFIED.

- MULCH:**
- ALL PLANTING BEDS SHALL HAVE A MINIMUM 3" DEPTH OF CEDAR MULCH.
 - ALL PARKING LOT ISLAND PLANTING BEDS SHALL HAVE A MINIMUM 3" DEPTH OF CEDAR MULCH.
 - TREES AND SHRUBS IN SOD OR NATIVE AREAS - 3" DEPTH CEDAR MULCH AS SPECIFIED ON PLANTING DETAILS.
 - ALL GRAVEL AREAS SHALL HAVE A MINIMUM 3" DEPTH OF 3/4" BUTTER ROCK GRAVEL MULCH WITH GEOTEXTILE FABRIC AS WEED BARRIER.

- IRRIGATION:**
- ALL LANDSCAPE AREAS SHALL BE IRRIGATED BY A PERMANENT, AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
 - TURF AREAS SHALL BE ZONED SEPARATELY FROM BED AREAS.
 - THE CONTROLLER SHALL INCLUDE A RAIN SHUT-OFF.
 - THE SYSTEM SHALL BE PROPERLY ZONED TO SEPARATE PLANT MATERIAL BY WATER REQUIREMENT.
 - IRRIGATION CONSTRUCTION DRAWINGS SHALL BE SUBMITTED PRIOR TO APPROVAL OF THE CIVIL CONSTRUCTION DRAWINGS.

- MAINTENANCE:**
- PROPERTY OWNER SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPING.
 - PROPERTY OWNER SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL ARTERIAL AND COLLECTOR RIGHT-OF-WAY LANDSCAPING.

- PRE-CONSTRUCTION MEETING:**
- A PRE-CONSTRUCTION MEETING SHALL BE REQUIRED BETWEEN THE LANDSCAPE CONTRACTOR, PROPERTY OWNER AND CITY OF NORTHGLENN PERSONNEL BEFORE START OF CONSTRUCTION.

- SOD:**
- SOD SHALL BE TEXAS HYBRID BLUEGRASS VARIETY THAT IS LOCALLY GROWN AND SHALL CONFORM TO NORTHGLENN REQUIREMENTS.

- SEED:**
- ALL NATIVE GRASS SEED AREAS SHALL BE SEEDED WITH SEED MIXES AND RATES PER THE CITY OF NORTHGLENN STANDARDS AND SPECIFICATIONS.

- PLANTING NOTES:**
- PLANT DECIDUOUS TREES MINIMUM 4' AND SHRUBS 2' OR 1/2 MATURE WIDTH FROM SIDEWALKS, BED EDGES, FENCES AND BUILDINGS.
 - ALL DECIDUOUS TREES SHALL BE BALLED AND BURLAPPED WITH FULL HEADS, STRAIGHT TRUNKS, AND SINGLE LEADERS UNLESS OTHERWISE NOTED.
 - CONTRACTOR TO SELECT TREES THAT HAVE THEIR FIRST BRANCH 6'-0" ABOVE THE TOP OF THE ROOTBALL. IF NEEDED, THE CONTRACTOR SHALL REQUEST THE SUPPLIER TO PRUNE ACCORDINGLY.
 - NO TREE OR SHRUB MORE THAN 30 INCHES HIGHER THAN TOP OF CURB SHALL BE PLANTED NEITHER IN ANY 50 FOOT VISION TRIANGLE AT ANY STREET INTERSECTION NOR IN ANY 25 FOOT VISION TRIANGLE.
 - NO TREE SHALL BE PLANTED WITHIN 10 FEET OF ANY UNDERGROUND UTILITY.

- DETENTION POND NOTES:**
- THE DETENTION POND SHALL BE ON A PERMANENT IRRIGATION SYSTEM ZONED SEPARATE FROM THE SURROUNDING AREA.
 - THE DETENTION POND SHALL BE SEEDED WITH A NATIVE SEED MIX.

SEED MIX SCHEDULE

SEED MIX - B - NATIVE SEED MIX	
40% - BUFFALOGRASS (BUCHLOE DACTYLOIDES)	25.0 LBS/ACRE
40% - BLUE GRAMA (BOUTELLOUA GRACILIS)	5.0 LBS/ACRE
20% - SIDEOTS GRAMA (BOUTELLOUA CURTIPENDULA)	10.0 LBS/ACRE
	40.0 LBS/ACRE

SEED MIX - B - WILDFLOWERS	
20% PURPLE PRAIRIE CLOVER (PETALOSTEMON PURPUREA)	0.6 LBS/ACRE
20% AMERICAN VETCH (VIGIA AMERICANA)	2.3 LBS/ACRE
15% PRAIRIE CONEFLOWER (RATIBIDA COLUMNIFERA)	0.15 LBS/ACRE
10% PERENNIAL GALLARDIA (GALLARDIA ARISTATA)	0.55 LBS/ACRE
10% GOLDEN CROWNBEARD (VERBESINE ENCELLIOIDES)	0.65 LBS/ACRE
10% ROCKY MOUNTAIN BEEPLANT (CLEOME SERRULATA)	1.4 LBS/ACRE
5% FRINGED SAGE (ARTEMESIA FRIGIDA)	0.01 LBS/ACRE
5% NARROWLEAF PENSTEMON (PENSTEMON ANGUSTIFOLIUS)	0.15 LBS/ACRE
5% WESTERN YARROW (ACHILLEA LANULOSA)	0.01 LBS/ACRE
	5.83 LBS/ACRE

SEED MIX - D - WET/DRY SEED MIX	
40% - ALKALIGRASS (PUCCINELLIA DISTANS)	2.00 LBS/ACRE
30% - WESTERN WHEATGRASS (PASCOPYRUM SMITHII)	22.0 LBS/ACRE
30% - SMOOTH BROME (BROMUS INERMIS)	16.0 LBS/ACRE
	40.0 LBS/ACRE

LANDSCAPE TABLE

DESCRIPTION	AREA	PERCENTAGE
SITE AREA (INCLUDING TRACKWAY)	276,276 SF	--
AREA OF ON-SITE LIVING MATERIAL	48,672 SF	17.7%
AREA OF ON-SITE NON-LIVING MATERIAL	45,732 SF	16.7%
AREA OF PAVING AND STRUCTURES	180,108 SF	65.6%
AREA OF LIVING MATERIAL IN ROW	3,067 SF	
AREA OF NON-LIVING MATERIAL IN ROW	6,524 SF	
TOTAL SF OF SPRAY IRRIGATED AREA	26,210 SF	
TOTAL SF OF DRIP IRRIGATED AREA	22,462 SF	



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6:19 PM

12/1/2016

USER: StaceyS

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	SLB	12/1/2016

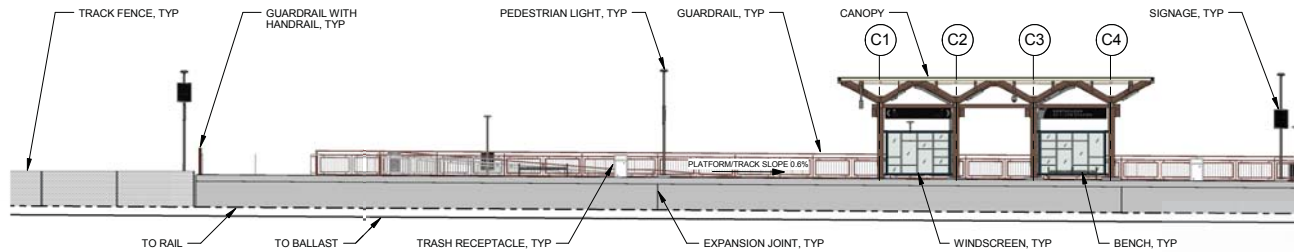
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DRAWN BY: BMK	DATE: 09/16/2014	APPROVED BY: SLB	DATE: 12/1/2016	VERT SCALE: NA
 				


REGIONAL TRANSPORTATION DISTRICT 1600 BLAKE STREET DENVER, COLORADO 80202 (303) 628-9000

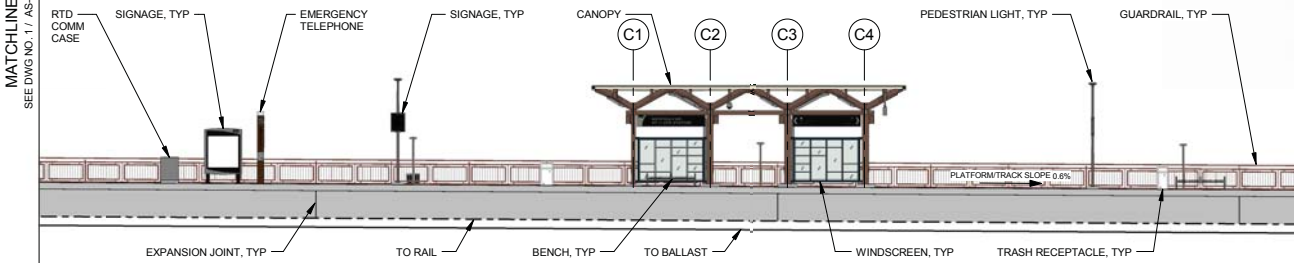
NORTH METRO RAIL LINE LANDSCAPE NORTHGLENN AT 112TH STATION PLANTING NOTES

PRELIMINARY
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CONSTRUCTION

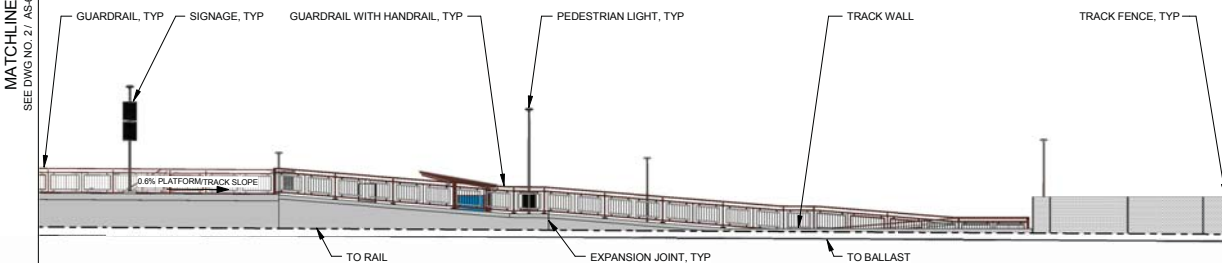
DWG No. LS-6200



1 PLATFORM ELEVATION - NORTH



2 PLATFORM ELEVATION - SOUTH



3 PLATFORM ELEVATION - SOUTH RAMP

MATCHLINE
SEE DWG NO. 2 / AS-6200

MATCHLINE
SEE DWG NO. 3 / AS-6200

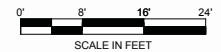
NOTES:

1. FOR STATION SPECIFIC STRUCTURAL DETAILS, SEE DRAWING SERIES SS-6000
2. FOR STATION SPECIFIC MECHANICAL DETAILS, SEE DRAWING SERIES MS-6000
3. FOR STATION SPECIFIC PLUMBING DETAILS, SEE DRAWING SERIES PS-6000
4. FOR STATION SPECIFIC ELECTRICAL DETAILS, SEE DRAWING SERIES ES-6000
5. FOR STATION SPECIFIC CIVIL DETAILS, SEE DRAWING SERIES CS-6000
6. FOR STATION SPECIFIC GRADING DETAILS, SEE DRAWING SERIES DJ-6000
7. FOR STATION SPECIFIC UTILITY DETAILS, SEE DRAWING SERIES UB-6000
8. FOR STATION SPECIFIC LANDSCAPE DETAILS, SEE DRAWING SERIES LS-6000
9. FOR STATIONS STANDARD ARCHITECTURAL DETAILS SEE DRAWING SERIES AS-1000
10. FOR STATIONS STANDARD ARCHITECTURAL STRUCTURAL DETAILS SEE DRAWING SERIES SS-1000

AMENITY	MATERIAL	FINISH COLOR	REMARKS
TICKET KIOSK ROOF	GLASS	OLD CASTLE CLEAR WITH ARCTIC SNOW LAMINATE	LAMINATED, TEMPERED
ALL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM B
CANOPY	METAL	TNEMEC - SEE STATION SPECIFIC PLANS FOR COLOR	COATING SYSTEM B, ALL EXPOSED METAL TO BE COATED, UON
ALL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - SEE STATION SPECIFIC PLANS FOR COLOR	COATING SYSTEM B, ALL EXPOSED METAL TO BE COATED, UON
VARIABLE MESSAGE SIGN MOUNTING	METAL	BLACK, RAL 9005	COATING SYSTEM B
LIGHT POLE & FIXTURE	METAL	BLACK, RAL 9005	POWDER COATED
BENCH	METAL	BLACK, RAL 9005	POWDER COATED
PAVING: PLATFORM, RAMPS, STAIRS, LANDINGS, PLAZA, SIDEWALKS	CONCRETE	GRAY	MEDIUM BROOM FINISH
TRASH RECEPTACLE	METAL	BLACK, RAL 9005	PREFINISHED
DETECTABLE WARNING STRIP	-	FEDERAL YELLOW #5353	PER MANUFACTURER
BIKE RACK	METAL	GREEN, RAL 6009	GALVANIZED, PANGARD, POLYESTER POWDER COAT
BIKE LOCKER	FIBERGLASS	TAN	PER MANUFACTURER
RAILINGS: DECORATIVE RAILS & GUARDRAILS	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM C
HANDRAILS	METAL	-	STAINLESS STEEL
HANDRAIL BRACKETS	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM C
BASE PLATE	METAL	TNEMEC - TERRA COTTA 07RD	COATING SYSTEM C
PEDESTRIAN BUS SHELTER	METAL	BLACK, RAL 9005	PER MANUFACTURER
CAST IN PLACE CONCRETE RETAINING WALLS AT 12ND, 112TH AND 104TH STATIONS	PAINTED CONCRETE	BROWN, FS 30450	FRACTURED FIN FORMLINER, 1/4" MAX DEPTH, 1-1/2" OC MAX FIN SPACING, EXTERIOR PAINT
OCS POLES AT STATION PLATFORM	STEEL	BLACK, RAL 9005	PER MANUFACTURER
HYD YDRANT CAGE	STEEL	STAINLESS STEEL	PER MANUFACTURER

CANOPY FINISHES LEGEND

ELEMENT	FINISH	COATING SYSTEM
COLUMN	TNEMEC IRONWOOD 28BR	B
SPINE BEAM	TNEMEC IRONWOOD 28BR	B
OUTRIGGER	TNEMEC IRONWOOD 28BR	B
GLASS PANEL FRAME	TNEMEC BALSAMWOOD 134DN	B
GUTTER	TNEMEC BALSAMWOOD 134DN	B
DOWNSPOUT	TNEMEC IRONWOOD 28BR	B
WINDSCREEN	TNEMEC BALSAMWOOD 134DN	B
GLASS LITES	OLDCASTLE SOLIDEX WITH ARCTIC SNOW LAMINATE	-
SEALANT	CLEAR	B
ACCESS PANEL COVER	MATCH ADJACENT STEEL FINISH	B
JUNCTION BOX	TNEMEC IRONWOOD 28BR	B
STEEL PLATE	TNEMEC IRONWOOD 28BR	B



NO.	ISSUED FOR CONSTRUCTION	VAM	09/21/2016
REVISIONS	BY	DATE	

DESIGNED BY: MBB DATE: 07/14/14
DRAWN BY: T.J.G DATE: 07/14/14
CHECKED BY: TLB DATE: 09/26/2016
APPROVED BY: VAM DATE: 09/26/2016

REGIONAL RAIL PARTNERS
A. Balfour Beatty / GRAHAM JOINT VENTURE

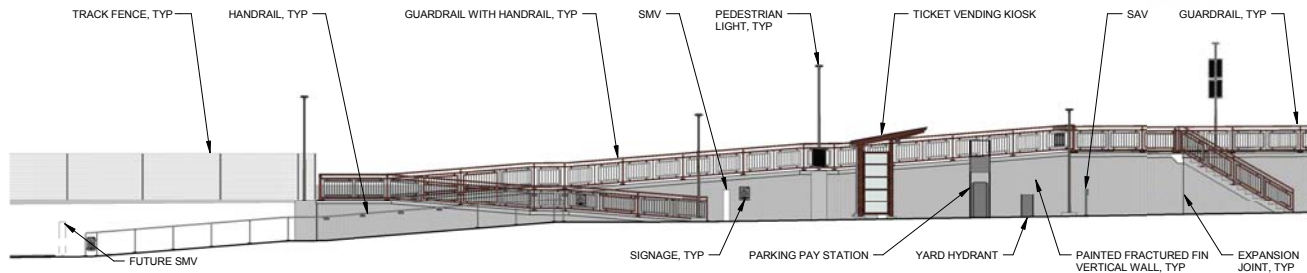
IRON HORSE ARCHITECTS
RESPONSIBLE DESIGN

475 17th Street Suite 720
Denver, Colorado 80202
iron-horse-architects.com

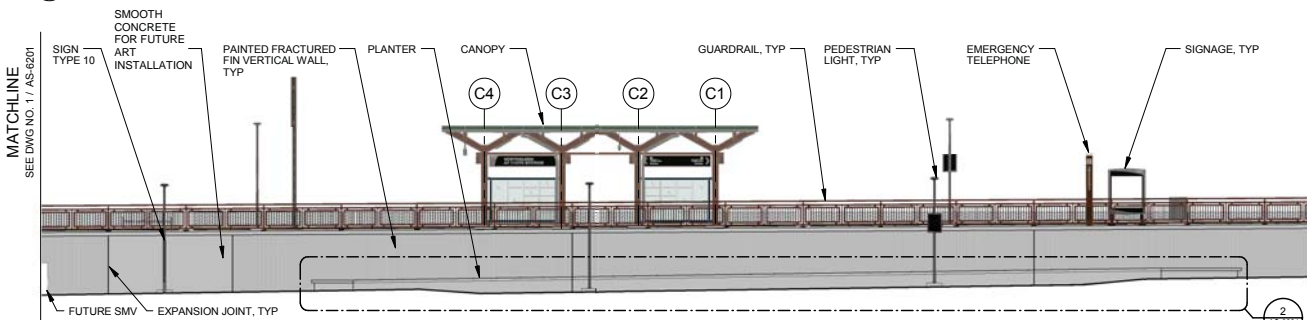
RTD FasTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BLAKE STREET
DENVER, COLORADO 80202
(303) 628-9000

NORTH METRO RAIL LINE
ARCHITECTURAL
NORTHGLENN AT 112TH STATION
STATION ELEVATION - EAST PLATFORM

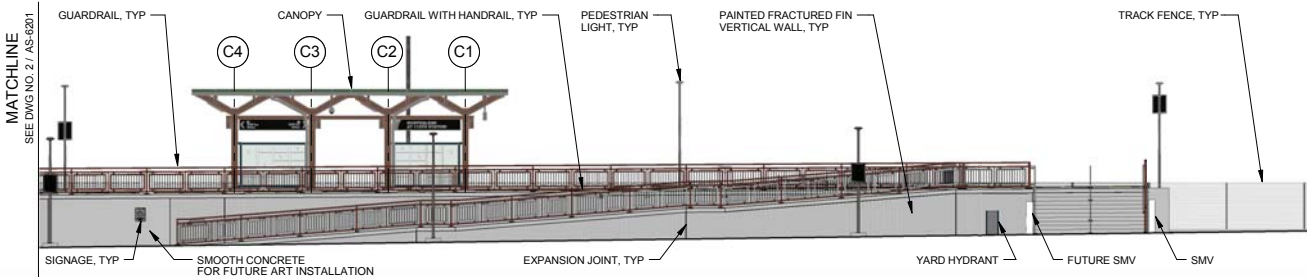
DWG No. AS-6200



1 STATION ELEVATION - SOUTH RAMP



2 STATION ELEVATION - SOUTH



3 STATION ELEVATION - NORTH

NOTES:

1. FOR STATION SPECIFIC STRUCTURAL DETAILS, SEE DRAWING SERIES SS-6000
2. FOR STATION SPECIFIC MECHANICAL DETAILS, SEE DRAWING SERIES MS-6000
3. FOR STATION SPECIFIC PLUMBING DETAILS, SEE DRAWING SERIES PS-6000
4. FOR STATION SPECIFIC ELECTRICAL DETAILS, SEE DRAWING SERIES ES-6000
5. FOR STATION SPECIFIC CIVIL DETAILS, SEE DRAWING SERIES CS-6000
6. FOR STATION SPECIFIC GRADING DETAILS, SEE DRAWING SERIES DJ-6000
7. FOR STATION SPECIFIC UTILITY DETAILS, SEE DRAWING SERIES UB-6000
8. FOR STATION SPECIFIC LANDSCAPE DETAILS, SEE DRAWING SERIES LS-6000
9. FOR STATIONS STANDARD ARCHITECTURAL DETAILS SEE DRAWING SERIES AS-1000
10. FOR STATION STANDARD ARCHITECTURAL STRUCTURAL DETAILS SEE DRAWING SERIES SS-1000

AMENITY	MATERIAL	FINISH COLOR	REMARKS
TICKET KIOSK ROOF	GLASS	OLD CASTLE CLEAR WITH ARCTIC SNOW LAMINATE	LAMINATED, TEMPERED
JAIL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM B
CANOPY	JAIL METAL: STEEL STRUCTURE, DOWNSPOUT GUTTER, FLASHING, JUNCTION BOX, HAND HOLE COVER PLATES	METAL	TNEMEC - SEE CANOPY FINISHES LEGEND
VARIABLE MESSAGE SIGN MOUNTING	METAL	BLACK, RAL 9005	COATING SYSTEM B
LIGHT POLE & FIXTURE	METAL	BLACK, RAL 9005	POWDER COATED
BENCH	METAL	BLACK, RAL 9005	
PAVING: PLATFORM, RAMPS, STAIRS, LANDINGS, PLAZA, SIDEWALKS	CONCRETE	GRAY	MEDIUM BROOM FINISH
TRASH RECEPTACLE	METAL	BLACK, RAL 9005	PREFINISHED
DETECTABLE WARNING STRIP	-	FEDERAL YELLOW #5353	PER MANUFACTURER
BIKE RACK	METAL	GREEN, RAL 6009	GALVANIZED, PANGARD II POLYESTER POWDER COAT
BIKE LOCKER	FIBERGLASS	TAN	PER MANUFACTURER
RAILINGS: DECORATIVE RAILS & GUARDRAILS	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM C
HANDRAILS	METAL	-	STAINLESS STEEL
HANDRAIL BRACKETS	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM C
BASE PLATE	METAL	TNEMEC - MONTERREY TILE	COATING SYSTEM C
PEDESTRIAN BUS SHELTER	METAL	BLACK, RAL 9005	PER MANUFACTURER
CAST IN PLACE CONCRETE RETAINING WALLS AT 12ND, 112TH AND 104TH STATIONS	PAINTED CONCRETE	BROWN, FS 30450	FRACTURED FIN FORMLINER, 1/4" MAX DEPTH, 1-1/2" OC MAX FIN SPACING, EXTERIOR PAINT
OCS POLES AT STATION PLATFORM	STEEL	BLACK, RAL 9005	
YARD HYDRANT CAGE	STEEL	STAINLESS STEEL	

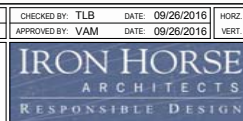
CANOPY FINISHES LEGEND

ELEMENT	FINISH	COATING SYSTEM
COLUMN	TNEMEC IRONWOOD 28BR	B
SPINE BEAM	TNEMEC IRONWOOD 28BR	B
OUTRIGGER	TNEMEC IRONWOOD 28BR	B
GLASS PANEL FRAME	TNEMEC BALSAMWOOD 13AGN	B
GUTTER	TNEMEC BALSAMWOOD 13AGN	B
DOWNSPOUT	TNEMEC IRONWOOD 28BR	B
WINDSCREEN	TNEMEC BALSAMWOOD 13AGN	B
GLASS LITES	OLDCASTLE SOLEXIA WITH ARCTIC SNOW LAMINATE	-
SEALANT	CLEAR	B
ACCESS PANEL COVER	MATCH ADJACENT STEEL FINISH	B
JUNCTION BOX	TNEMEC IRONWOOD 28BR	B
STEEL PLATE	TNEMEC IRONWOOD 28BR	B

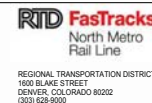


NO.	ISSUED FOR CONSTRUCTION	VAM	09/21/2016
0	ISSUED FOR CONSTRUCTION	VAM	09/21/2016
REVISIONS	BY	DATE	

DESIGNED BY: MBB	DATE: 01/05/15	CHECKED BY: TLB	DATE: 09/26/2016
DRAWN BY: T.J.G.	DATE: 01/05/15	APPROVED BY: VAM	DATE: 09/26/2016



475 17th Street Suite 720
Denver, Colorado 80202
iron-horse-architects.com



NORTH METRO RAIL LINE
ARCHITECTURAL
NORTHGLENN AT 112TH STATION
STATION ELEVATION - EAST PLATFORM WALL

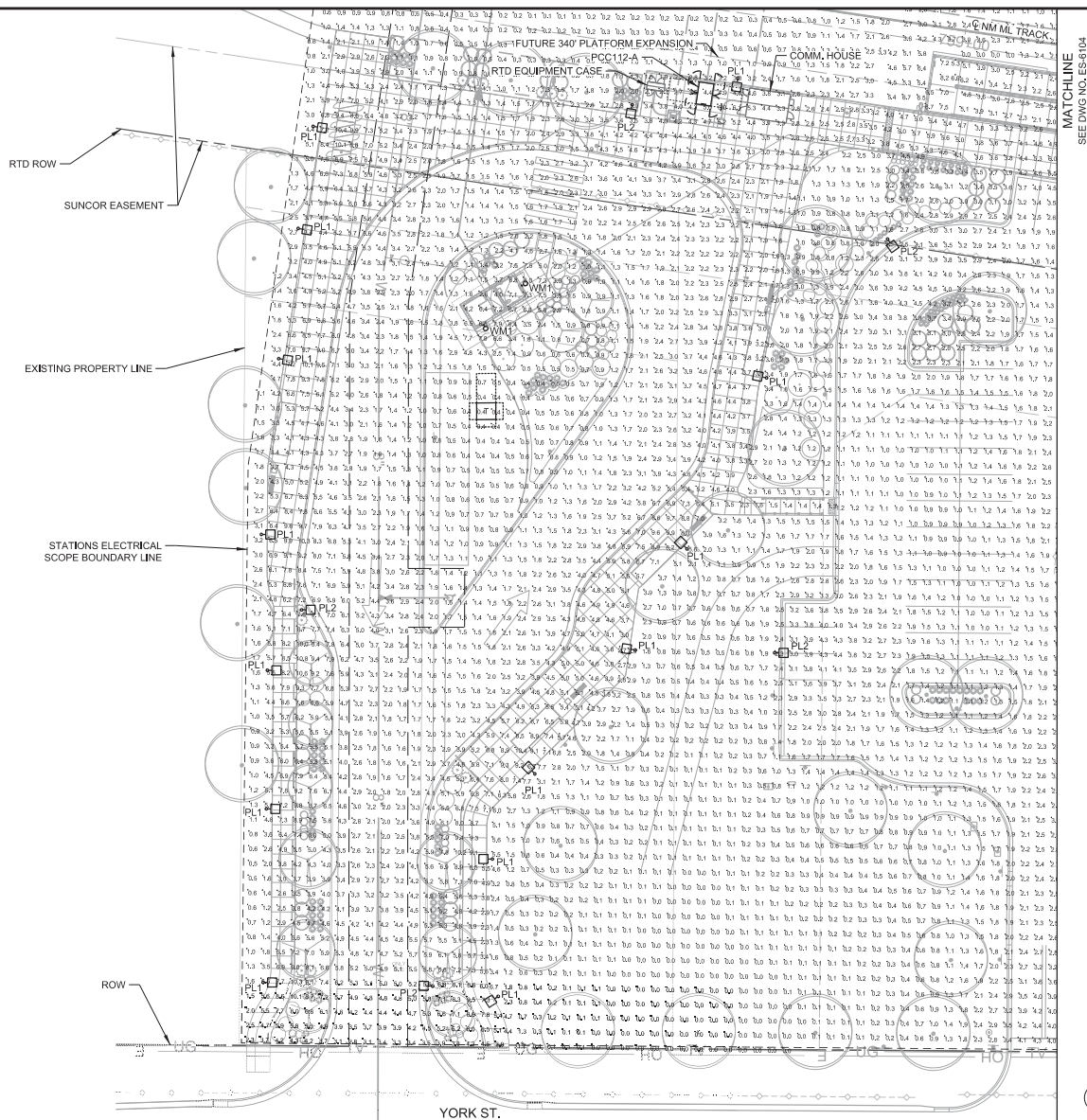
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NMLE-ES-0103-112H-SitePhotPS.dgn

348489 PM

5/12/2016

USER: Adminor



GENERAL NOTES:
1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.

1 SITE PHOTOMETRIC PLAN - SOUTH

NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: SJ/GP DATE: 05/05/2016
DRAWN BY: AR/TE DATE: 05/06/2016
CHECKED BY: KAT DATE: 05/09/2016
APPROVED BY: KAT DATE: 05/20/2016
HORIZ. SCALE: 1" = 40'
VERT. SCALE: N/A

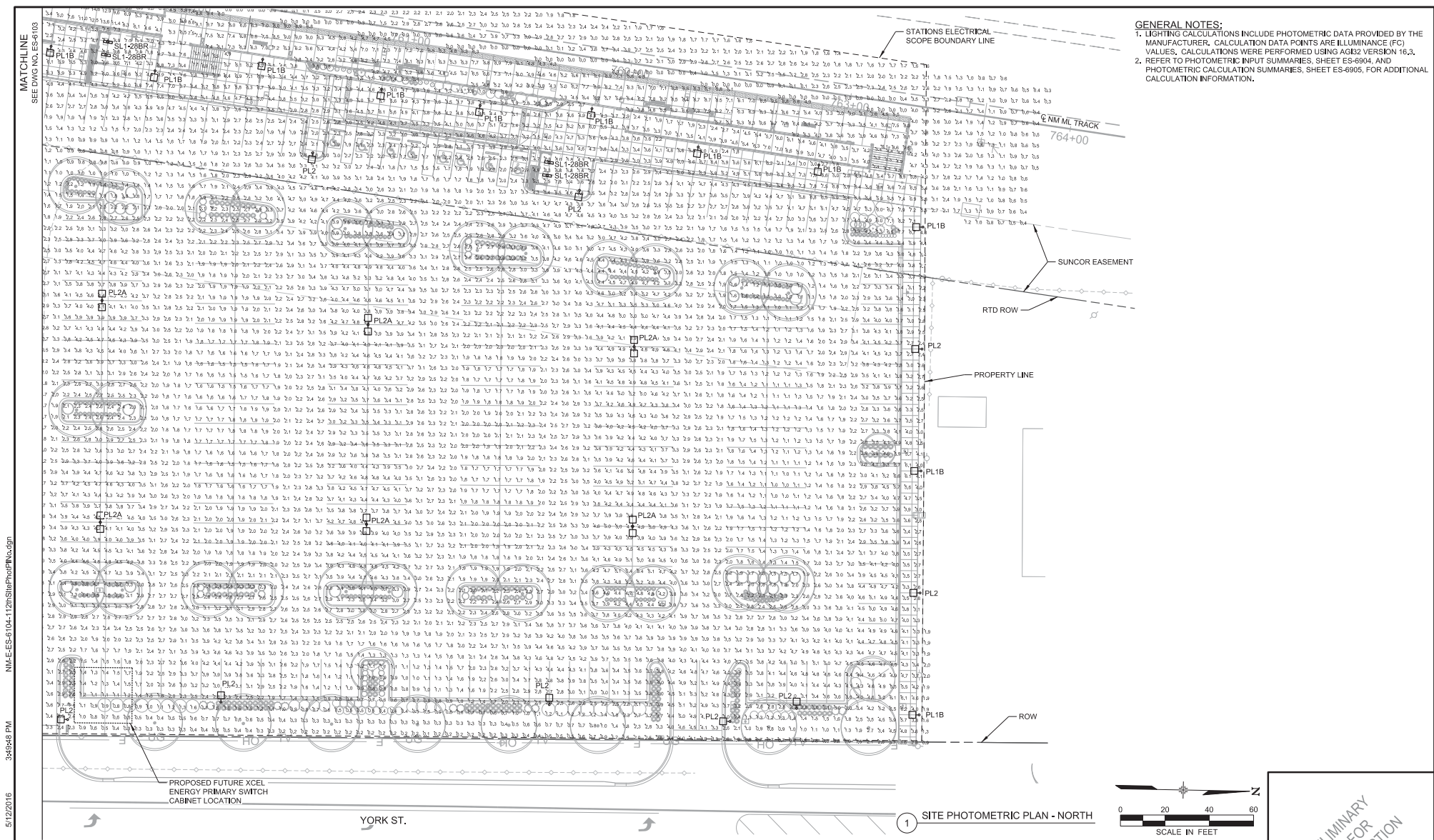
RTD FastTracks
North Metro Rail Line
REGIONAL TRANSPORTATION DISTRICT
1600 BEARS STREET
DENVER, CO 80202 80202
(303) 628-0001

SSG MEP
SSG MEP, Inc.
3025 S. Parker Road, Suite 1100
Aurora, Co 80014-2950
303.696.2602 fax 303.696.0812
www.ssgmep.com

NORTH METRO RAIL LINE
ELECTRICAL
NORTHGLENN AT 112TH STATION
SITE PHOTOMETRIC PLAN - SOUTH

PRELIMINARY
NOT FOR
CONSTRUCTION

DWG No. ES-6103



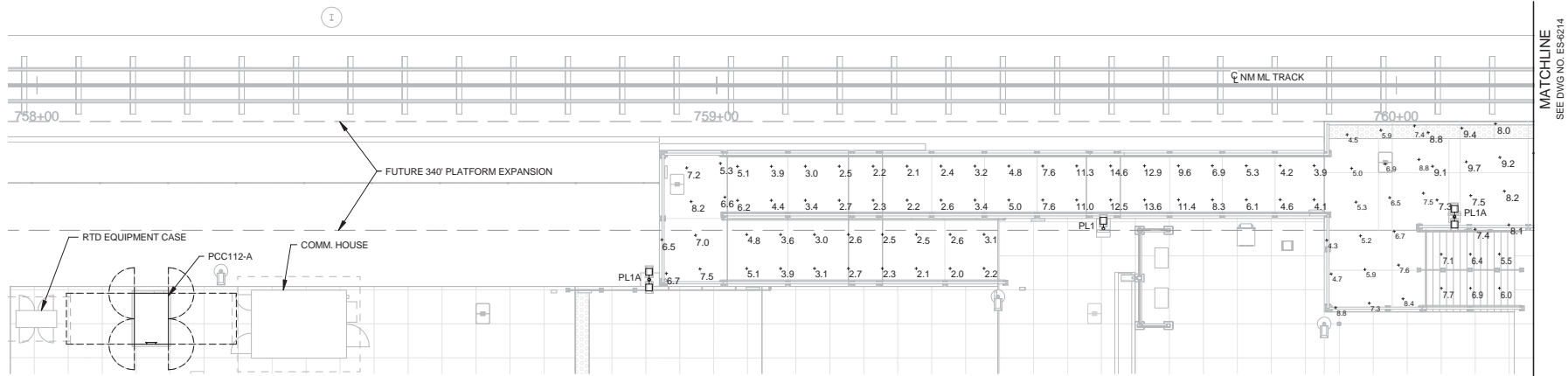
GENERAL NOTES:
1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-690A, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-690S, FOR ADDITIONAL CALCULATION INFORMATION.

USER: Amorin 5/12/2016 3:48:48 PM NMLE-ES-004-112th Station Photo.dgn	DESIGNED BY: SJ/GP DATE: 05/05/2016	CHECKED BY: KAT DATE: 05/09/2016	HORIZ. SCALE: 1" = 40'	 RTD FastTracks North Metro Rail Line REGIONAL TRANSPORTATION DISTRICT 1600 BEARS STREET DENVER, CO 80202 (303) 628-0001	NORTH METRO RAIL LINE ELECTRICAL NORTHGLENN AT 112TH STATION SITE PHOTOMETRIC PLAN - NORTH	PRELIMINARY NOT FOR CONSTRUCTION															
	DRAWN BY: AR/TE DATE: 05/06/2016	APPROVED BY: KAT DATE: 05/20/2016	VERT. SCALE: N/A																		
	 SSG MEP, Inc. 3025 S. Parker Road, Suite 1100 Aurora, CO 80014-2950 303.696.2602 fax 303.696.0812 www.ssggroupinc.com																				
	 REGIONAL RAIL PARTNERS Boulder, Fort Collins, Greeley, Longmont, Loveland, Poudre Valley, and Weld County																				
<table border="1"><thead><tr><th>NO.</th><th>REVISIONS</th><th>BY</th><th>DATE</th></tr></thead><tbody><tr><td>C</td><td>ISSUED FOR 100% SUBMITTAL, CDRL 03-038-10.06</td><td>KAT</td><td>05/20/2016</td></tr><tr><td>B</td><td>ISSUED FOR 90% SUBMITTAL, CDRL 03-037-10.06</td><td>KAT</td><td>04/29/2015</td></tr><tr><td>A</td><td>ISSUED FOR 60% SUBMITTAL, CDRL 03-036-10.06</td><td>KAT</td><td>10/17/2014</td></tr></tbody></table>		NO.	REVISIONS	BY	DATE	C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038-10.06	KAT	05/20/2016	B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037-10.06	KAT	04/29/2015	A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036-10.06	KAT	10/17/2014				
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C	ISSUED FOR 100% SUBMITTAL, CDRL 03-038-10.06	KAT	05/20/2016																		
B	ISSUED FOR 90% SUBMITTAL, CDRL 03-037-10.06	KAT	04/29/2015																		
A	ISSUED FOR 60% SUBMITTAL, CDRL 03-036-10.06	KAT	10/17/2014																		

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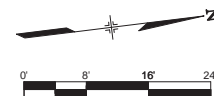
USER: Alvaro



1 PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH

GENERAL NOTES:

1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AGI32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.



NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL. CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL. CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL. CDRL 03-036.10.06	KAT	10/17/2014



NORTH METRO RAIL LINE

ELECTRICAL

NORTHGLENN AT 112TH STATION

PLATFORM PHOTOMETRIC LIGHTING PLAN - SOUTH

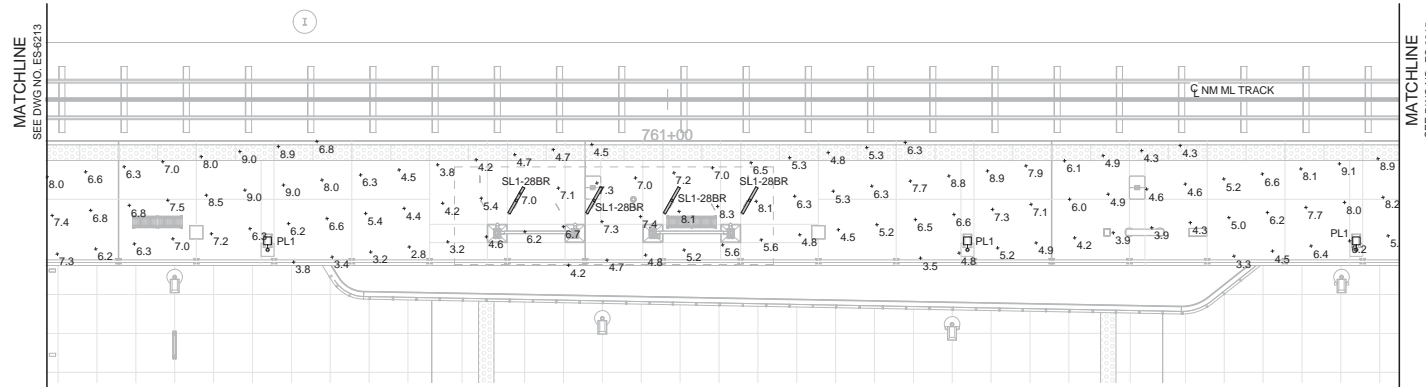
DWG No. ES-6213

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CONSTRUCTION

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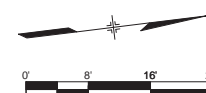
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1 PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER

GENERAL NOTES:

1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.



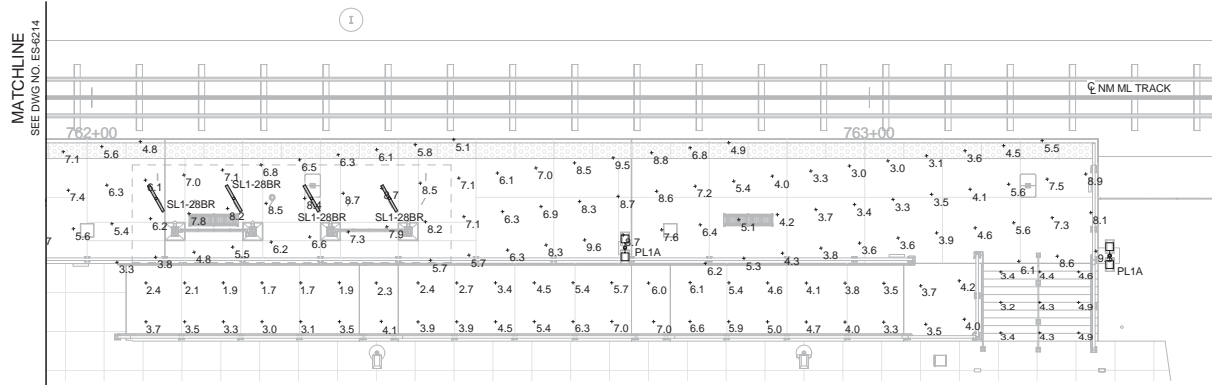
NO.	REVISIONS	BY	DATE
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B	ISSUED FOR 90% SUBMITTAL CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL CDRL 03-036.10.06	KAT	10/17/2014



NORTH METRO RAIL LINE
ELECTRICAL
NORTHGLENN AT 112TH STATION
PLATFORM PHOTOMETRIC LIGHTING PLAN - CENTER

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NOT FOR
CONSTRUCTION

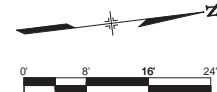
DWG No. ES-6214



1 PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH

GENERAL NOTES:

1. LIGHTING CALCULATIONS INCLUDE PHOTOMETRIC DATA PROVIDED BY THE MANUFACTURER. CALCULATION DATA POINTS ARE ILLUMINANCE (FC) VALUES. CALCULATIONS WERE PERFORMED USING AG32 VERSION 16.3.
2. REFER TO PHOTOMETRIC INPUT SUMMARIES, SHEET ES-6904, AND PHOTOMETRIC CALCULATION SUMMARIES, SHEET ES-6905, FOR ADDITIONAL CALCULATION INFORMATION.



NO.	REVISIONS	BY	DATE
C	ISSUED FOR 100% SUBMITTAL CDRL 03-038.10.06	KAT	05/20/2016
B	ISSUED FOR 90% SUBMITTAL CDRL 03-037.10.06	KAT	04/29/2015
A	ISSUED FOR 60% SUBMITTAL CDRL 03-036.10.06	KAT	10/17/2014

DESIGNED BY: SRJ DATE: 05/05/2016
 DRAWN BY: AR/TE DATE: 05/06/2016

CHECKED BY: KAT DATE: 05/09/2016
 APPROVED BY: KAT DATE: 05/20/2016

RTD REGIONAL RAIL PARTNERS
 A. Balfour Beatty / GRIFFIN J. JONES

SSG MEP, Inc.
 3025 S. Parker Road, Suite 1100
 Aurora, Co 80014-2950
 303.696.2502 fax 303.696.0812
 www.ssgroupinc.com

RTD FasTracks
 North Metro
 Rail Line

REGIONAL TRANSPORTATION DISTRICT
 1600 BLAKE STREET
 DENVER, COLORADO 80202
 (303) 628-8000








NORTH METRO RAIL LINE

ELECTRICAL
 NORTHGLENN AT 112TH STATION
 PLATFORM PHOTOMETRIC LIGHTING PLAN - NORTH

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 CONSTRUCTION

DWG No. ES-6215

NORTH METRO RAIL LUMINAIRE SCHEDULE - NORTHGLENN AT 112TH STATION

IMAGE	KEY	LAMPS					DESCRIPTION	FINISH	MOUNTING	MANUFACTURER	CATALOG NUMBER	VOLTAGE	LLF	INPUT WATTS	NOTES
		QTY	TYPE	LUMENS	CR	CCT									
	PL1		LED ARRAY	9,375 LUMENS	70+	4,000 K	SINGLE HEAD PEDESTRIAN LED LUMINAIRE, DIE-CAST ALUMINUM HOUSING WITH 9" ARM LENGTH AND INTEGRAL THERMAL CONTROL SYSTEM, VIBRATION RESISTANT 3G RATING, UL WET LOCATION, FULL CUT-OFF TYPE III OPTICS.	BLACK	7" BASE DIAMETER, 20'-0" ROUND TAPERED STEEL POLE CUT TO 14'3" WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (4) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 1 - 3 - 90LA - NW - JNV - BLP -OR APPROVED EQUAL- DS210 - R700A140 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - SL - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	89.0	1
	PL1A		LED ARRAY	9,375 LUMENS EACH LUMINAIRE	70+	4,000 K	SAME AS PL1 EXCEPT DOUBLE HEAD.	BLACK	7" BASE DIAMETER, 20'-0" ROUND TAPERED STEEL POLE CUT TO 14'3" WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (4) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 2 - 3 - 90LA - NW - JNV - BLP -OR APPROVED EQUAL- DS210 - R700A140 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - SL - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	178.0	1
	PL1B		LED ARRAY	5,330 LUMENS	70+	4,000 K	SAME AS PL1 EXCEPT LUMEN OUTPUT.	BLACK	7" BASE DIAMETER, 20'-0" ROUND TAPERED STEEL POLE CUT TO 14'3" WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (4) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 1 - 3 - 55LA - NW - JNV - BLP -OR APPROVED EQUAL- US210 - K700A140 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - SL - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	54.0	1
	PL2		LED ARRAY	16,732 LUMENS	70+	4,000 K	SINGLE HEAD PARKING LOT LED LUMINAIRE, DIE-CAST ALUMINUM HOUSING WITH 9" ARM LENGTH AND INTEGRAL THERMAL CONTROL SYSTEM, VIBRATION RESISTANT 3G RATING, UL WET LOCATION, FULL CUT-OFF TYPE III OPTICS.	BLACK	7" BASE DIAMETER, 25'-0" ROUND TAPERED STEEL POLE WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (5) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 1 - 3 - 180LA - NW - UNV - 3LP -OR APPROVED EQUAL- DS210 - R700A250 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	177.0	1
	PL2A		LED ARRAY	16,732 LUMENS EACH LUMINAIRE	70+	4,000 K	SAME AS PL2 EXCEPT DOUBLE HEAD.	BLACK	7" BASE DIAMETER, 25'-0" ROUND TAPERED STEEL POLE WITH INTERNAL VIBRATION DAMPER AS REQUIRED BY RTD, SIZED TO ACCOMMODATE (5) 1" CONDUITS, 100MPH MINIMUM SPEED RATING	PHILIPS ORDCO VALMONT POLE MILLERBERND	PUREFORM P21 - A1 - 2 - 3 - 180LA - NW - UNV - 3LP -OR APPROVED EQUAL- DS210 - R700A250 - D1/D2 - GV/FP - (F-540) ** - AB - SFBC - HH - PC - VD APPROVED EQUAL -OR APPROVED EQUAL-	120 - 277 V	0.87	354.0	1
	SL1-28BR		LED ARRAY	2,582 LUMENS	80+	4,000 K	SURFACE MOUNT LINEAR LED LUMINAIRE, 4" LENGTH X 4" WIDTH X 4-5/8" OVERALL HEIGHT, EXTRUDED ALUMINUM HOUSING WITH GASKETTED ENDS AND HIGH IMPACT ACRYLIC LENS, UL WET LOCATION, IP66.	TO MATCH CANOPY FINISH - TNE MEC IRONWOOD 28BR	SURFACE MOUNT	ALW	LIGHTPLANE 3.5 WET LOCATION LP3.5SMB-WL - 4' - HP900-400K - LED - 277 - CUSTOM COLOR - CUSTOM MOUNTING BRACKET -OR APPROVED EQUAL-	277 V	0.87	28.0	1
	WM1		LED ARRAY	3,607 LUMENS	70+	4,000 K	PERFORMANCE LED WALL SCONCE, DIE-CAST ALUMINUM HOUSING AND INTEGRAL THERMAL CONTROL SYSTEM, DIFFUSING GLASS LENS, UL WET LOCATION, FULL CUT-OFF OPTICS.	BRONZE	WALL MOUNT, 8"-C AFF TO BOTTOM OF FIXTURE	PHILIPS ORDCO	121 LED PERFORMANCE SCONCE 121 - 3 - 35LA-700 - NW - BL2 - DL -OR APPROVED EQUAL-	120 - 277 V	0.87	52.0	1

GENERAL NOTES:

1. CONTRACTOR SHALL BID BASED ON THE LUMINAIRE SCHEDULE ONLY. REFERENCE TO SPECIFICATIONS FOR SUBMITTAL, SUBSTITUTION, AND EQUIPMENT LIST REQUIREMENTS.
2. CONTRACTOR SHALL SUPPLY UNIT PRICES AT TIME OF BID. LOT PRICES ARE INACCEPTABLE.

DETAIL NOTES:

1. FINAL COLOR SELECTION SHALL BE APPROVED BY ARCHITECT AND OWNER PRIOR TO ORDERING.

DESIGNED BY: SJ/GP DATE: 05/05/2016		CHECKED BY: KAT DATE: 05/09/2016		HORIZ. SCALE: N/A	
DRAWN BY: AR/TE DATE: 05/06/2016		APPROVED BY: KAT DATE: 05/20/2016		VERT. SCALE: N/A	
					
C ISSUED FOR 100% SUBMITTAL, CDRL 03-038,10.06 B ISSUED FOR 90% SUBMITTAL, CDRL 03-037,10.06 A ISSUED FOR 60% SUBMITTAL, CDRL 03-036,10.06		KAT 05/20/2016 KAT 04/29/2015 KAT 10/17/2014		SSG MEP, Inc. 3025 S. Parker Road, Suite 1100 Aurora, Co 80014-2950 303.696.2602 fax 303.696.0812 www.ssggroupinc.com	
NO.	REVISIONS	BY	DATE		

REGIONAL TRANSPORTATION DISTRICT
1600 BEANS STREET
DENVER, CO 80202 80202
(303) 628-8000

NORTH METRO RAIL LINE

ELECTRICAL
NORTHGLENN AT 112TH STATION
LUMINAIRE SCHEDULE

PRELIMINARY
NOT FOR
CONSTRUCTION

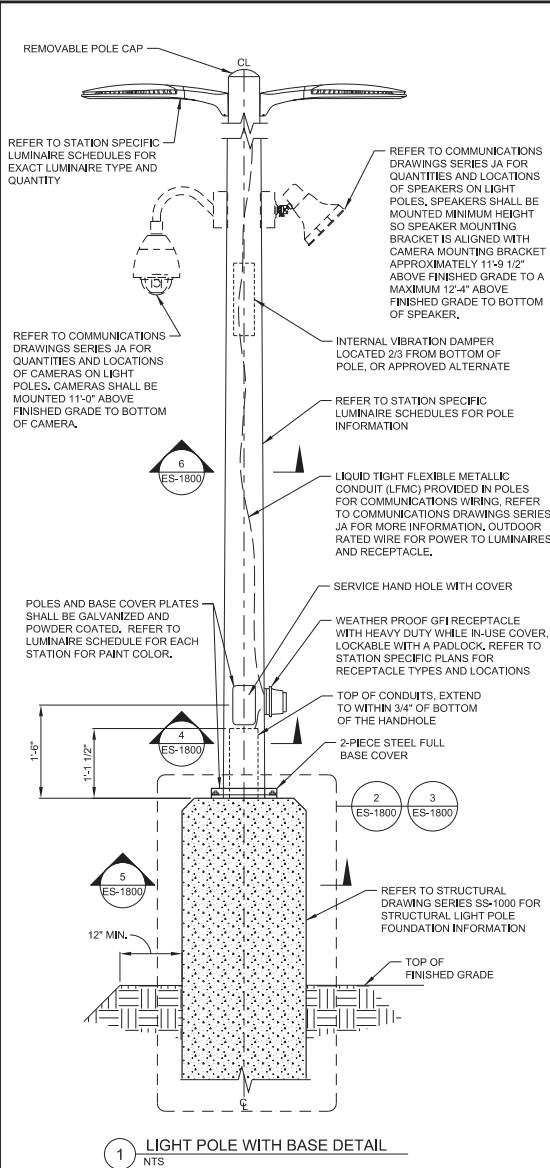
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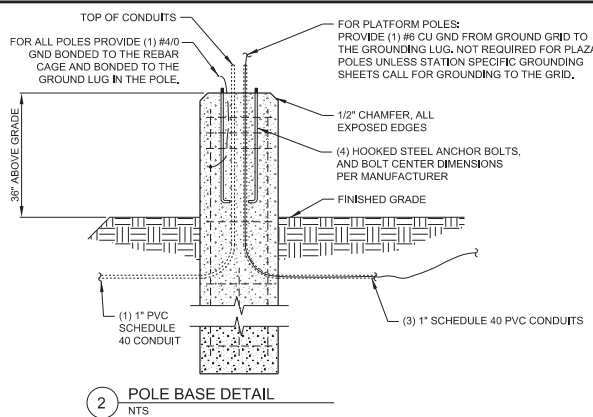
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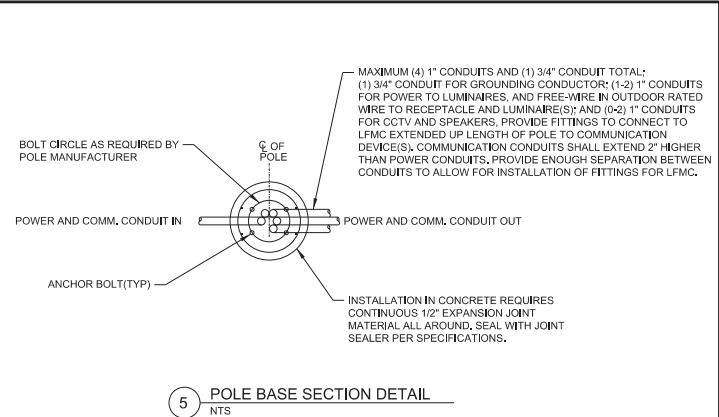
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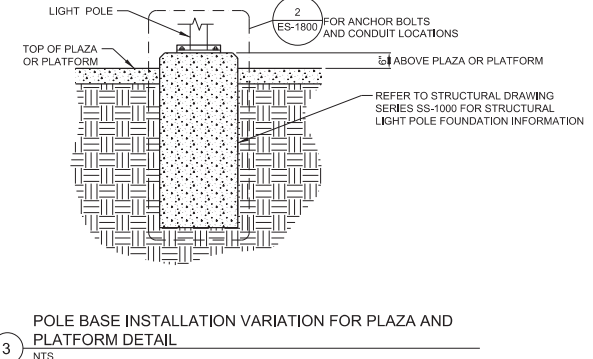
1 LIGHT POLE WITH BASE DETAIL
NTS



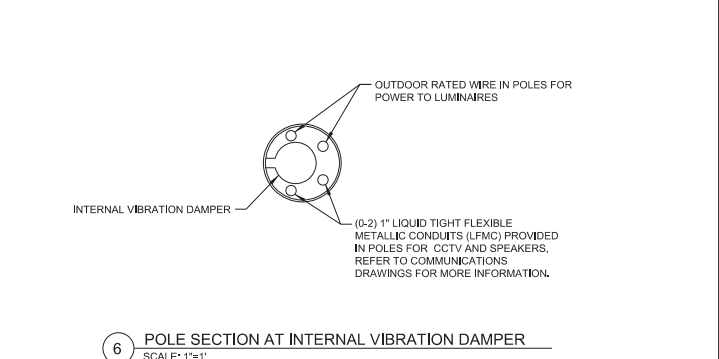
2 POLE BASE DETAIL
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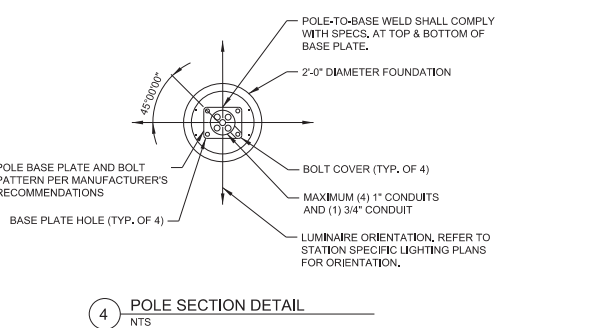
5 POLE BASE SECTION DETAIL
NTS



3 POLE BASE INSTALLATION VARIATION FOR PLAZA AND PLATFORM DETAIL
NTS



6 POLE SECTION AT INTERNAL VIBRATION DAMPER
SCALE: 1\"/>



4 POLE SECTION DETAIL
NTS

NO.	ISSUED FOR CONSTRUCTION	KAT	11/15/2016
NO.	REVISIONS	BY	DATE

DESIGNED BY: SRJ DATE: 11/02/2015
 DRAWN BY: AR/TE DATE: 11/03/2015
 APPROVED BY: KAT DATE: 11/15/2016

R-P
 REGIONAL RAIL PARTNERS
 RailAmerica / GRAHAM

SSG MEP
 SSG MEP, Inc.
 3025 S. Parker Road, Suite 1100
 Aurora, Co 80014-2950
 303.696.2602 fax 303.696.0812
 www.ssgmep.com

RTD FastTracks
 North Metro Rail Line
 REGIONAL TRANSPORTATION DISTRICT
 1600 BLAIR STREET
 DENVER, CO 80202
 (303) 628-8000

NORTH METRO RAIL LINE
 ELECTRICAL STATIONS, STANDARD DETAILS
 PEDESTRIAN LIGHT POLE DETAILS

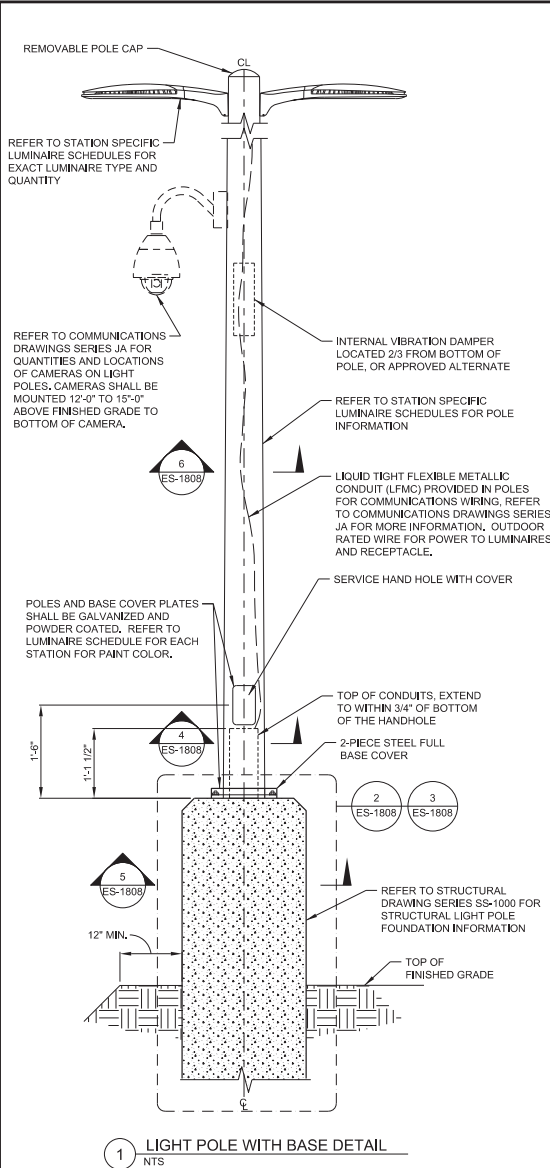
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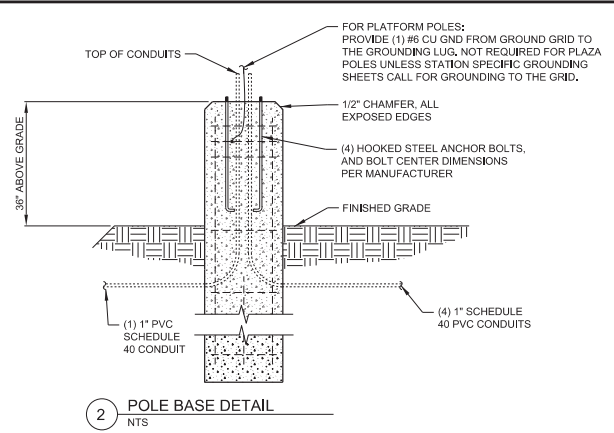
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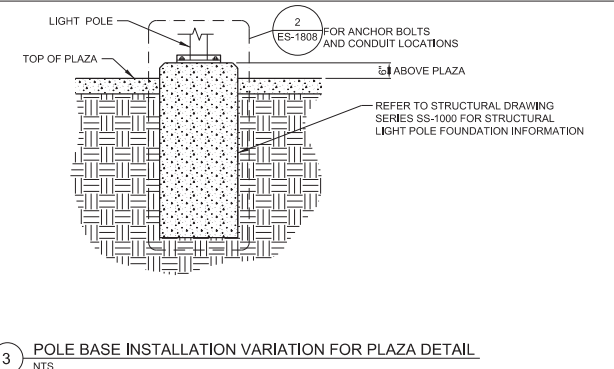
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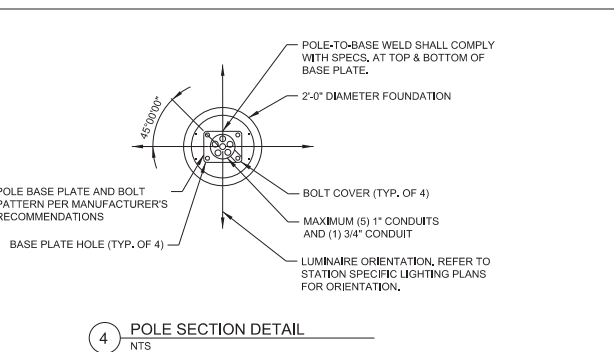
1 LIGHT POLE WITH BASE DETAIL
NTS



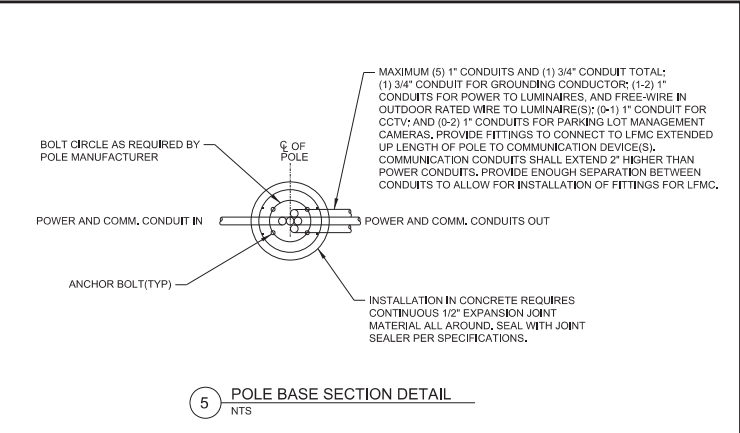
2 POLE BASE DETAIL
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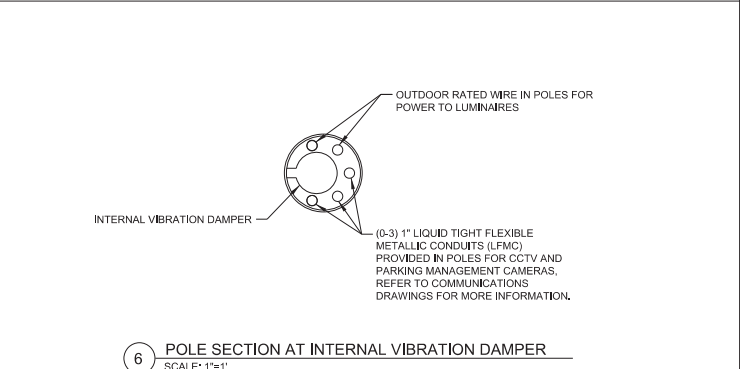
3 POLE BASE INSTALLATION VARIATION FOR PLAZA DETAIL
NTS



4 POLE SECTION DETAIL
NTS



5 POLE BASE SECTION DETAIL
NTS



6 POLE SECTION AT INTERNAL VIBRATION DAMPER
SCALE: 1"=1'

NO.	REVISIONS	BY	DATE
0	ISSUED FOR CONSTRUCTION	KAT	11/15/2016

DESIGNED BY: SRJ DATE: 11/02/2015
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SSG MEP, Inc.
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 North Metro
 Rail Line
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 DENVER, CO 80202
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NORTH METRO RAIL LINE
 ELECTRICAL
 STATIONS, STANDARD DETAILS
 PARKING LOT LIGHT POLE DETAILS

DWG No. ES-1808

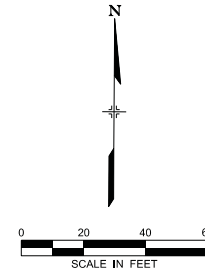
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LEGEND

- ① EDGE LINES WHITE, 6" WIDE
- ② LANE LINES DASHED WHITE, 4" WIDE
- ③ CHANNELIZED LINES WHITE, 8" WIDE
- ④ STOP LINE WHITE, 24" WIDE
- ⑤ CROSSWALK LINES WHITE, 24" WIDE
- ⑥ CENTER LINES DOUBLE YELLOW, 4" WIDE
- ⑦ YELLOW, 4" WIDE
- ⑧ LANE LINES DASHED WHITE, 8" WIDE
- ⑨ LANE DROP LINES SHORT DASHED WHITE, 8" WIDE
- ⑩ RIGHT TURN ARROW
- ⑪ LEFT TURN ARROW
- ⑫ GRADE CROSSING PAVEMENT MARKING PER DETAIL ON SHEET SD-101

TRAFFIC CONTROL NOTES:

- 1. PAVEMENT SYMBOLS SHALL BE FIELD VERIFIED, PRIOR TO INSTALLATION.
- 2. MATCH STRIPING TO EXISTING (TYP).
- 3. ALL SIGNS FIELD LOCATED BY ENGINEER AND/OR MUNICIPALITY.
- 4. RTD NMRL IMPROVEMENTS ON THE WEST SIDE SHALL MATCH INTO PROPOSED NORTHGLENN IMPROVEMENTS ALONG 112TH AVENUE.
- 5. ADDITIONAL SIGNS ON PEDESTRIAN CROSSINGS, SEE SHEET KJ-801.
- 6. "NO TRAIN HORN" PLAQUE IS TO BE INSTALLED UPON THE APPROVAL OF OF QUIET ZONE BY FRA.



FUTURE LANE WIDENING
BY CITY OF NORTHGLENN

FUTURE SIDEWALK EXTENSION
BY CITY OF NORTHGLENN

MATCHLINE SHEET 1 OF 2
MATCHLINE SHEET 2 OF 2

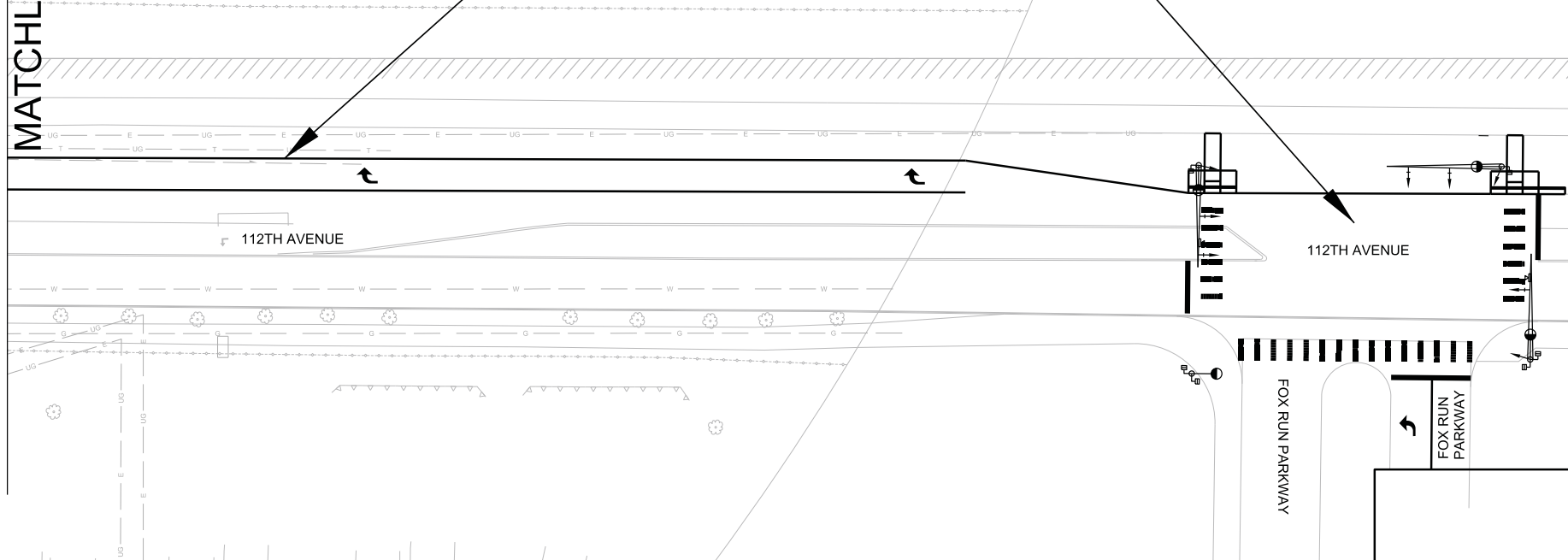
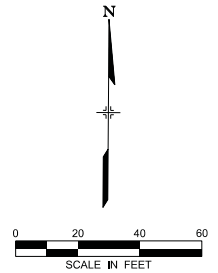
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				RTD FastTracks North Metro Rail Line <small>REGIONAL TRANSPORTATION DISTRICT 1600 BLANS STREET DENVER, COLORADO 80202 (303) 628-6000</small>		
				NORTH METRO RAIL LINE SIGNING AND STRIPING PLAN 112TH AVENUE & YORK STREET (CITY OF NORTHGLENN) SHEET 1 OF 2		
NO. _____ REVISIONS _____ BY _____ DATE _____				DWG No. X-100		

USER: h007072 10/26/2016 3:52:28 PM AMS_NM4-CR802-112th_difcpl_2012.dgn

MATCHLINE SHEET 1 OF 2
MATCHLINE SHEET 2 OF 2

FUTURE INTERSECTION IMPROVEMENTS
BY CITY OF NORTHGLENN

FUTURE LANE WIDENING
BY CITY OF NORTHGLENN



NO.	REVISIONS	BY	DATE

DESIGNED BY:	DATE:	CHECKED BY:	DATE:	HORIZ. SCALE: 1" = 40'
DRAWN BY:	DATE:	APPROVED BY:	DATE:	VERT. SCALE: N/A

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NORTH METRO RAIL LINE
SIGNING AND STRIPING PLAN
112TH AVENUE & YORK STREET (CITY OF NORTHGLENN)
SHEET 2 OF 2

DWG No. X-101